

**Location:**



**RENO TRANSPORTATION COMMISSION**  
**1105 Terminal Way, 1<sup>st</sup> Floor Great Room, Reno, NV**  
**Date/Time: 10:00 A.M., Friday, August 18, 2023**

**REGIONAL TRANSPORTATION COMMISSION**  
**OF WASHOE COUNTY**  
**BOARD MEETING AGENDA**

- I. The Regional Transportation Commission Great Room is accessible to individuals with disabilities. Requests for auxiliary aids to assist individuals with disabilities should be made with as much advance notice as possible. For those requiring hearing or speech assistance, contact Relay Nevada at 1-800-326-6868 (TTY, VCO or HCO). Requests for supporting documents and all other requests should be directed to Michelle Kraus at 775-348-0400 and you will receive a response within five business days. Supporting documents may also be found on the RTC website: [www.rtcwashoe.com](http://www.rtcwashoe.com).
- II. This meeting will be televised live and replayed on RTC's YouTube channel at: [bit.ly/RTCWashoeYouTube](https://bit.ly/RTCWashoeYouTube)
- III. Members of the public in attendance at the meeting may provide public comment (limited to three minutes) after filling out a request to speak form at the meeting. Members of the public that would like to provide presentation aids must bring eight (8) hard copies to be distributed to the Board members at the meeting. Alternatively, presentation aids may be emailed, in PDF format only, to [mkraus@rtcwashoe.com](mailto:mkraus@rtcwashoe.com) prior to 4:00 p.m. on the day preceding the meeting to be distributed to the Board members in advance of the meeting. Members of the public may also provide public comment by one of the following methods: (1) emailing comments to: [rtcpubliccomments@rtcwashoe.com](mailto:rtcpubliccomments@rtcwashoe.com); or (2) leaving a voicemail (limited to three minutes) at (775) 335-0018. Comments received prior to 4:00 p.m. on the day preceding the meeting will be entered into the record.
- IV. The Commission may combine two or more agenda items for consideration and/or may remove an item from the agenda or delay discussion relating to an item on the agenda at any time.
- V. The supporting materials for the meeting will be available at [www.rtcwashoe.com/meetings](http://www.rtcwashoe.com/meetings). In addition, a member of the public may request supporting materials electronically from Michelle Kraus at the following email address: [mkraus@rtcwashoe.com](mailto:mkraus@rtcwashoe.com).

**1. Call to Order:**

- 1.1. Roll Call
- 1.2. Pledge of Allegiance

**2. Public Comment:** *Public comment taken under this item may pertain to matters both on and off the agenda. The Chair may take public comment on a particular item on the agenda at the time it is discussed. Comments are to be made to the Board as a whole and not to individual commissioners*

**3. Approval of Agenda (For Possible Action):**

**4. Consent Items (For Possible Action):**

- 4.1. Minutes
  - 4.1.1 Approve the draft meeting minutes from the 7/21/2023 Board meeting. (For Possible Action)
- 4.2. Reports
  - 4.2.1 Acknowledge receipt of the month Procurement Activity Report. (For Possible Action)

- 4.2.2 Acknowledge receipt of the Summary Report for the Technical, Citizens Multimodal, and Regional Road Impact Fee Advisory Committees. (For Possible Action)
- 4.2.3 Acknowledge receipt of the monthly Planning Activity Report. (For Possible Action)
- 4.2.4 Acknowledge receipt of the monthly Engineering Activity Report. (For Possible Action)
- 4.2.5 Acknowledge receipt of the monthly Public Transportation and Operations Report. (For Possible Action)
- 4.2.6 Acknowledge receipt of the monthly Outreach Activities Report. (For Possible Action)

4.3. Planning Department

- 4.3.1 Approve a contract with Parametrix, Inc., for consulting services on the Regional Freight Plan, in an amount not-to-exceed \$149,296.14. (For Possible Action)

4.4. Engineering Department

- 4.4.1 Approve a contract with HDR Engineering, Inc., for design and optional construction management services for the Veterans Parkway ITS Project, in an amount not-to-exceed \$375,460. (For Possible Action)
- 4.4.2 Approve a recommendation to commence with the sale of four parcels (APN# 020-111-66; 020-11-67; 020-111-36; and 020-111-37) acquired in connection with the Peppermill Bus Rapid Transit (BRT) Station Project, by sale to adjoining property owners, sealed bids, auction, direct sale, or as otherwise authorized by law. (For Possible Action)

4.5. Public transportation/Operations Department

- 4.5.1 Approve a contract with Spika Design & Manufacturing for the purchase of a bus inspection platform, in an amount not-to-exceed \$200,790. (For Possible Action)
- 4.5.2 Approve an Interlocal Cooperative Agreement (ICA) with the Nevada Department of Transportation to provide funding to the RTC for the ED Pass Program in the amount of \$350,000. (For Possible Action)

4.6. Executive, Administrative and Finance Department

- 4.6.1 Acknowledge receipt of a report regarding quarterly progress on RTC Agency Goals and Strategic Roadmap - FY 2023 (Q4). (For Possible Action)

**5. Public Hearing:**

- 5.1. Approve a resolution adopting the Federal Fiscal Years (FFY) 2023-2027 Regional Transportation Improvement Program (RTIP). (For Possible Action)
  - a. Staff presentation
  - b. Public Hearing
  - c. Action

**6. Discussion Items and Presentations:**

- 6.1. Discuss paving and lighting on the Truckee River Path as requested by Mayor Schieve, and provide input and direction to the Executive Director. (For Possible Action).
- 6.2. Discuss the history and future direction of public transit in relation to Lake Tahoe as requested by Mayor Schieve, and provide input and direction to the Executive Director. (For Possible Action)

**7. Reports (Information Only):**

- 7.1. Monthly verbal update/messages from RTC Executive Director Bill Thomas - no action will be taken.
- 7.2. Monthly verbal update/messages from Paul Nelson, RTC Government Affairs Officer on federal matters related to the RTC - no action will be taken.
- 7.3. Monthly verbal update of activities from NDOT - no action will be taken.

**8. Commissioner Announcements and Updates:** *Announcements and updates to include requests for information or topics for future agendas. No deliberation or action will take place on this item.*

**9. Public Comment:** *Public comment taken under this item may pertain to matters both on and off the agenda. The Chair may take public comment on a particular item on the agenda at the time it is discussed. Comments are to be made to the Board as a whole and not to individual commissioners*

**10. Adjournment (For Possible Action):**

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**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.1.1

To: Regional Transportation Commission

From: Michelle Kraus, Clerk of the Board

**SUBJECT: Draft Meeting Minutes for 7/21/2023**

**RECOMMENDED ACTION**

Approve the draft meeting minutes from the 7/21/2023 Board meeting.

**BACKGROUND AND DISCUSSION**

See Attachment for Background and Discussion.

**FISCAL IMPACT**

There is no fiscal impact related to this item.

**PREVIOUS BOARD ACTION**

There has been so previous Board action taken.

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**REGIONAL TRANSPORTATION COMMISSION  
WASHOE COUNTY, NEVADA**

**FRIDAY**

**10:00 A.M.**

**July 21, 2023**

**PRESENT:**

**Alexis Hill, Vice Chair, Washoe County Commissioner  
Ed Lawson, Mayor of Sparks, Via Zoom (Joined from 10:15 a.m. to 11:07 a.m.)  
Hillary Schieve, Mayor of Reno, Via Zoom (Joined @ 10:15 a.m.)  
Mariluz Garcia, Washoe County Commissioner  
Naomi Duerr, City of Reno (Alternate)**

**Bill Thomas, RTC Executive Director  
Adam Spear, Legal Counsel  
Darin Tedford, Deputy Director of NDOT**

**ABSENT:**

**Devon Reese, Reno City Vice Mayor**

The regular monthly meeting, held in the 1<sup>st</sup> Floor Great Room at Regional Transportation Commission of Washoe County, Reno, Nevada, was called to order by Vice Chair Hill. The Board conducted the following business:

***Item 1           CALL TO ORDER***

- 1.1   Roll Call
- 1.2   Pledge of Allegiance

***Item 2           PUBLIC INPUT***

Vice Chair Hill opened the meeting to public input and called on anyone wishing to speak on topics relevant to the Regional Transportation Commission (RTC) that are not included in the current agenda.

Ms. Dora Uchel-Martinez, NV Disability Peer Action Coalition Advocate and Coalition for the Blind, discussed concerns for her safety with Paratransit driver that came to pick her up for scheduled appointment who did not introduce himself as being with Paratransit and would not tell her his name. It made her feel unsafe not knowing who exactly was there and she did not get on the bus. Customer service told her they would send an Uber, then they told her they would not because the driver had been on-time, even though she explained what had happened.

Ms. Donna Clontz, local resident with the Senior Coalition, and Lead Volunteer with Age Friendly Communities Reno, who are expanding to the County and Sparks as well. We have surveys out to our seniors for what they think would make the community, County and City of Sparks more age friendly. I wanted to thank the RTC, and also let the Board know about the FlexRIDE program in Sparks/Spanish Springs. The Sparks Senior Advisory Board received the opportunity from their City Council to decide where some of their ARPA funds would go. They decided they would create a free transportation program using FlexRIDE in region of the County for one-year for seniors. I would love it if there were a way for RTC to do a press release or get out some information to let that community know that this opportunity exists for them, to try this out and

find out about this new way to get around. The Age Friendly Groups are very interested in improving transportation. It's one of the main things the seniors say they need.

There being no one else wishing to speak, Vice Chair Hill closed public input.

### **Item 3        *APPROVAL OF AGENDA***

On motion of Commissioner Garcia to move to approve with changes, seconded by Commissioner Duerr, which motion unanimously carried, Vice Chair Hill ordered that the agenda for this meeting be approved.

### **Items 4        *CONSENT ITEMS***

#### **4.1     Minutes**

4.1.1    Approve minutes from 6/16/2023 Board meeting. (For Possible Action)

#### **4.2.    Reports**

4.2.1    Acknowledge receipt of the monthly Procurement Activity Report. (For Possible Action)

4.2.2    Acknowledge receipt of the Summary Report for the Technical, Citizens Multimodal, and Regional Road Impact Fee Advisory Committees. (For Possible Action)

4.2.3    Acknowledge receipt of the monthly Planning Activity Report. (For Possible Action)

4.2.4    Acknowledge receipt of the monthly Engineering Activity Report. (For Possible Action)

4.2.5    Acknowledge receipt of the monthly Public Transportation and Operations Activity Report. (For Possible Action)

4.2.6    Acknowledge receipt of the monthly Outreach Report from the Communications staff. (For Possible Action)

#### **4.3     Planning Department – (For Possible Action)**

4.3.1    Approve funding for Transportation Alternatives Set-Aside (TA Set-Aside) projects for the federal fiscal year 2023-2024 cycle in the amount of \$1,605,000. (For Possible Action) – *Pulled for Discussion*

#### **4.4     Engineering Department**

4.4.1    Approve a contract with DOWL, LLC to provide traffic analysis and design services related to the North Valleys North Virginia Street Capacity Project, in an amount not-to-exceed \$1,509,982. (For Possible Action)

4.4.2    Approve a Local Public Agency (LPA) Agreement with the Nevada Department of Transportation (NDOT) for the use and reimbursement of federal funds on the Pyramid Highway/Sparks Boulevard Interchange and Connector Improvements Project. (For Possible Action)

4.4.3    Approve an Interlocal Cooperative Agreement (ICA) with the City of Reno for the reimbursement of the cost of specialized multi-use path maintenance equipment, in an amount not-to-exceed \$190,000. (For Possible Action)

4.4.4    Approve an Interlocal Cooperative Agreement (ICA) with the City of Sparks for the reimbursement of the cost of specialized multi-use path maintenance equipment, in an amount not-to-exceed \$190,000. (For Possible Action)

4.4.5    Approve a contract with Kimley Horn & Associates, Inc., for design and optional construction management services for the Buck Drive Circulation Project, in an amount not-to-exceed \$511,164. (For Possible Action)

- 4.4.6 Approve a contract with CA Group, Inc., for professional services for the Lemmon Drive Traffic Improvements and Resiliency Project NEPA and design, in an amount not-to-exceed \$5,583,739. (For Possible Action)
- 4.4.7 Approve a contract with Wood Rodgers Inc., for the preliminary and final design of the Downtown West Fourth Street Project, in an amount not-to-exceed \$741,480. (For Possible Action)
- 4.4.8 Approve Amendment No. 4 to the Professional Services Agreement (PSA) with Atkins North America Inc., for additional engineering during construction services needed in connection with extending the contract performance period for the Sky Vista Parkway Rehabilitation Project, in the amount of \$40,000, for a new total not-to-exceed amount of \$2,500,173. (For Possible Action)
- 4.4.9 Authorize staff to return \$708,938.32 of federal funds to the Nevada Department of Transportation (NDOT) for certain costs that the Federal Highway Administration (FHWA) considers ineligible for federal participation on the Arlington Avenue Bridges Replacement Project. (For Possible Action)

Commissioner Duerr would like to pull Item 4.3.1 for discussion.

Vice Chair Hill asked for a motion to approve Consent items, minus 4.3.1. On motion of Commissioner Garcia to move to approve Consent items, seconded by Commissioner Duerr, which motion unanimously carried, Vice Chair Hill order that Consent Items, minus 4.3.1, be approved.

For the record Mayor Lawson and Mayor Schieve voted yes, for approval of Consent Items, minus 4.3.1.

4.3.1 Approve funding for Transportation Alternatives Set-Aside (TA Set-Aside) projects for the federal fiscal year 2023-2024 cycle in the amount of \$1,605,000. (For Possible Action)

Commissioner Duerr said she pulled this item because both Madam Vice Chair and myself sponsored one of these items, which is to address sidewalk issues along Plumas and Urban, near the Washoe County Golf Course. I wanted to thank the RTC Staff Review Committee for prioritizing this item. This is the kind of project we all love to see, where we are enhancing pedestrian safety in particular.

On motion of Commissioner Duerr to move to approve Item 4.3.1, seconded by Commissioner Garcia, which motion unanimously carried, Vice Chair Hill ordered that Item 4.3.1 be approved.

For the record Mayor Lawson and Mayor Schieve voted yes, for approval of Item 4.3.1.

## ***Item 5           DISCUSSION ITEMS AND PRESENTATIONS***

5.1. Presentation regarding final summary of discussions and actions at the 82nd (2023) Session of the Nevada Legislature. (No Action Required)

Michael Hillerby with Kaempfer Crowell gave a summary of the Legislative Session and stated he would be happy to answer any questions after the summary.

The Legislature completed their 120-day regular session and promptly when into two special sessions, one would become the shortest session on record to pass the Capital Improvement Project bill that had failed on the last night of the session. The bill ultimately passed that talked about the State Funding portion of what would be a public stadium for a Major League Baseball team, in this case the Oakland A's.

In terms of RTC, first I want to thank the Board for the opportunity to continue to represent you. It is a job that I enjoy a great deal and our firm is very pleased with, so thank you and thank you to your staff, Paul Nelson, Amber Bowsmith, all of the Director's and Bill Thomas. Appreciate the regular meetings and quick responses we received from everyone.

Bill AB93 would have allowed school zone speed enforcement cameras, which was a bill the Board decided to support. This bill didn't do very well and was never heard. That remains a subject that is really difficult for the Legislature.

AB14 was sponsored by Assemblywoman Natha Anderson and co-sponsored by Senator Skip Daly, both from Sparks. That bill was brought up on behalf of the Teamsters and really dealt with the relationship between Keolis, the Teamsters and us on a handful of issues.

We spent a lot of time on that bill and I'd like to acknowledge the work that Assemblywoman Anderson did. She worked very hard and we had a number of meetings with Mr. Thomas, myself, staff with representatives of a variety of Labor Locals here, including the Teamsters. I think those went well and spoke well of what I hope is a good future for that relationship. In the end, counties with more than 700,000 in population, which now applies to Washoe County, needs to have a Bench, Bus Stop and Shelter Advisory Committee. Two members of that committee would be appointed by you. Those need to be employees of the Contractor that runs the Transit System. That can either be a stand-alone committee or the law allows those duties to be assigned to another Advisory Committee that you may have. This is something to consider as the effective date if October 1, 2023, and I know staff is working on some recommendations, so we can be ready to meet the deadline.

The bill also has some specific language about retention of video events that happen on the buses and how that can be accessed by representatives of employees and the time limit involved. As newer buses come into the freight, I think it will be easier to do that.

Another bill, AB184, was by Assemblyman Howard Watts. That bill provides an incentive program for hydrogen and electric medium and heavy-duty vehicles. RTC is listed as one of the organizations that would qualify for those. We are also one of the organizations that would get a bump on the incentive of an extra 10% for vehicles that qualified.

The gas tax holiday bill, to get some money back to Nevadan's, became a bit of a political football and it did not go through.

I want to thank your staff who are great and provide considerable assistance in reviewing these bills and getting us information and guidance from those of you on the Legislative Committee at the Board level makes our representation much easier.

Commissioner Duerr asked about AB93, the automated enforcement and speed cameras. One of the biggest complaints I get as a Councilmember is people speeding. A number of people have written in asking if we can have camera automated enforcement? What do you think would be the future for this? Do you think this is dead in the water for a decade, or do you think there is any interest in moving forward with that?

Michael Hillerby, I'll talk a little bit about the past and to the extent that would be something that guides on our thoughts for the future. One of the things that lead to this proposal from the Traffic Safety Advisory Committee and others was; in a number of locales including Washoe County, we think we've done just about everything we can do to make those better. We've re-engineered them,



we've moved things around, we've increased signage and patrols. You see law enforcement working with the school district. Given the limited resources to have patrols out in front of every school all the time, we think we've done everything we can to make those areas as safe as possible, given the resources and was certainly something that this Board was supportive of. The history has been that there has always been a concern that those cameras might inordinately target minority communities and that is what has killed those bills in the past.

Bill Thomas, RTC Executive Director wanted to take this opportunity to specifically thank Michael and his firm. They did a really good job of representing us this year. Great guidance, definitely dedicated and committed and responsive, so I wanted to thank you publicly on behalf of our staff and the Board.

5.2. Quarterly report from Keolis Transit Services, LLC regarding fixed-route operations and maintenance performance measures. (No Action Required)

Phil Pumphrey, General Manager for Keolis and Karin Pinkerton, Customer Service Manager for Keolis, presented a slide show and gave the quarterly update on Keolis Transit Services.

5.3. Approve the Verdi Area Multimodal Transportation Study. (For Possible Action)

Bill Thomas, Executive Director, started by giving some background on this item. This particular study was initiated by the former Chair, so this is an example of situations where the Board members have the opportunity to collectively agree on providing us direction on areas where we need to do some analysis. A lot of people, including NDOT, participated in this study, and we're hoping this can provide guidance going forward for the community and RTC.

Xuan Wang, RTC Senior Technical Planner, presented a PowerPoint presentation and discussed the Verdi Area Multimodal Transportation Study.

We focused on the Regional Roads and also coordinated with NDOT to look into the freeways and interchanges. This study is to engage the public stakeholders and our partner agencies to identify a list of potential improvements.

The first step in the Planning stage is to identify needs and after this study is completed. The recommended improvements will be prioritized in RTC's Regional Transportation Plan and NDOT's One Nevada Plan. When the improvements are ready to go, they will go through environmental analysis, design and construction.

We had two public meetings and multiple Technical Advisory Committee meetings to discuss the study with local agencies and NDOT. We also had an online survey, which received 273 responses.

Based upon the analysis and public input, we developed recommendations. The slide shows improvements for freeways, which are divided into three (3) time frames and are already working on the near-term improvements to install wildlife fencing and the removal of the rumble strips, because of the noise complaints. The near-term improvements include median cable barrier installation and the improvements for several bridges. The long-term improvements include the widening of I-80 to accommodate the growth and the construction of the Gold Ranch interchange.

For Regional Roads, community members expressed their concerns about South Verdi Road. This road is currently two-lanes with steep slopes on both sides and it poses safety concerns when the road is icy. A portion of the road is within the UPRR Right-of-Way and the current volume is about 420

vehicles per day, making the benefit cost ratio low. During the study, we looked at different options for improving the road. As I said, this study is to identify the needs, so once we are ready to move forward with the improvements, we will do a more focused public outreach and a more detailed investigation to identify the best improvement concept.

We also recommended improvements for bike and pedestrians, which include the improvement at the Edinburgh roundabout and also to provide shared use paths along 3<sup>rd</sup> Street and Bridge Avenue. We recommended installing shared use paths under I-80 along Crystal Park Road to improve the sight distance.

Other areas we looked at concerned truck parking during winter weather when I-80 is closed, as semi-trucks park along local roads. We would work continuously with NDOT on the State Freight Plan to address truck parking and advanced notification along I-80. Also, identifying private parcels near Gold Ranch, Mogul and 4<sup>th</sup> Street for additional truck parking.

The RTC is also moving forward with the Regional Freight Study, which will address freight planning in the region. We also recommended Park and Ride locations near the Gold Ranch Interchange and partnership with the Boomtown Casino to develop Park and Ride lots. The community also requested transit. The RTC already has the FlexRIDE On Demand Transit Services in Verdi, but many residents don't know about this service. During our public meetings we demonstrated how to use the service and they were very interested in it. We still need to continue the educational effort on this.

The study included a speed analysis using Connected Vehicle Data through UNR's research. The speed data showed that the compliances are generally good in Verdi, but if in the future we need to do speed management, we can consider reducing the lanes on 3<sup>rd</sup> Street to 11' and also work with the law enforcement to reinforce the speed.

The last slide shows a summary of all recommended improvements, and we also have the Draft Report on our website with all of the details.

Chad Anson, Project Manager with CA Group Inc., is our consultant who is also here, and we are happy to answer any questions.

Commissioner Duerr said she is so pleased we are doing this study and is looking forward to see the road improvements and bike and pedestrian safety improvements.

Vice Chair Hill is also looking forward to seeing all of the improvements for roads and safety.

Vice Chair Hill called for public comment.

Ms. Dee Ann Radcliffe, long time Verdi resident, wanted to talk a little about the roads and brought along pictures that were passed out to all Board members. Wrong way crashes, Garson Interchange, Garson Overpass, safety issues for bikes and pedestrians. Traffic increases in the area, narrow roads for trucks, no striping.

On motion of Commissioner Garcia to approve Item 5.3, seconded by Commissioner Duerr, which motion carried unanimously.

**6.1    RTC Executive Director Report**

1. We are extremely happy to receive two very important federal grants. The Federal Highway Administration has awarded a 25-million-dollar RAISE Grant to the RTC for the Lemmon Drive Improvements and Resiliency Project. The project makes some key environmental and safety improvements to 3.7 miles of Lemmon Drive. This is one of the projects that the board identified as a federal priority.
2. The other grant is the Low or No Emission Vehicle Program from the Federal Transit Agency. The RTC will receive 8.78 million dollars to support the purchase of six additional hydrogen fuel cell buses and the expansion of hydrogen fueling infrastructure. It will pay for an innovative virtual reality program to train bus mechanics to use this cutting-edge technology. As you know, 100 percent of our fleet uses alternative fuel. This grant will help increase sustainability.
3. I would like to welcome our two newest employees. Eliseo Garcia Cruz is our Engineering Intern who started on June 5<sup>th</sup>. James Okorochoa is our new Planning Intern. He started on June 23<sup>rd</sup>.
4. Xuan Wang celebrated her 10-year anniversary with RTC, July 8<sup>th</sup>. Judy Tortelli and Mark Schlador both celebrated their five-year anniversary, July 9. John Ponzo celebrates his 25<sup>th</sup> anniversary, July 27. Congratulations to all four of you and thank you for your hard work here.
5. Congratulations to Amanda Callegari on her promotion to Engineering Manager. Amanda has proven to be a very valuable asset to the RTC, so this is very well-deserved. She is replacing Doug Maloy who retired this month.
6. The Hubbard Way bus station is substantially complete and in operation across the street from the Peppermill. The new Virginia Line RAPID station is one of the busiest stops along the corridor. I want to thank Ian Chamberlain for his leadership on this project, along with Kimley-Horn and Associates – and Q & D Construction. The 1.7 million-dollar investment makes this BRT stop much safer and more comfortable for our customers. Congratulations to everyone who worked so hard on this important project.
7. For the second year in a row, the RTC Finance Department has been awarded the Certificate of Achievement for Excellence in Financial Reporting. The award comes from the Government Finance Officers Association of the United States and Canada. This is for our annual financial report for Fiscal Year '22. This is the highest form of recognition for governmental accounting and financial reporting. Congratulations to Christian Schonlau and his team for a job well-done.
8. Once again, the RTC will provide free bus rides on our RAPID Virginia and Lincoln Lines – Routes one and 11 – and ACCESS for Hot August Nights. The free service will be all day, each day from August 1<sup>st</sup> through August 6<sup>th</sup>.
9. Congratulations to the Keolis Driver of the month – Timothy Radley. Tim has worked as a bus operator for RTC RIDE since January of 2002. Tim is known as a gentle giant and as the kindest individual. He treats the passengers with respect and dignity. Tim is such a good employee that sometimes we forget he works there. He's also quite the car enthusiast. His pride and joy in his '67 Volkswagen Beetle.
10. Please join us in congratulating Byron Mac-chi-etto as the MTM Employee of the Month. Byron has been an employee since 2005, back when it was Citilift. He's known by his co-workers as Bronco because of his love for the Denver Broncos. He appreciates his co-workers, gets along well with passengers, and gets a lot of positive remarks from both. Outside of work he is the family repair man with 8 kids and 12 grandkids. He keeps busy with toys and appliances alike, and he enjoys bowling. Bronco is a great example of MTM Transit's core values – and embodies our vision that every trip is important.

## **6.2 RTC Federal Report**

Paul Nelson, RTC Government Affairs Office, the Senate THUD bill passed in committee with no opposition. There is \$2.5 million for the Sun Valley Boulevard project, and that is one of the priorities that the Board laid out for us last December. Regionally, there is also funding for Pyramid Way and SR28 at Lake Tahoe. There are also several other projects throughout the State that are included in this bill. The bill provides additional funding for the Low No Program and Bus and Bus Facilities Grants and it provides \$16.8 billion for the FTA.

The House of Appropriations THUD bill looks quite a bit different. The good news is the Transit Formula Funds match the IJA reauthorization levels of \$13.9 billion, so most agencies are very unlikely to see any kind of significant loss of formula funding, especially those who have fewer miles traveled and a slower population growth. The bill would cut transportation funding for RAISE, Mega, Capital Investment Grants and Bus and Bus Facilities Grants. This is just the starting point for the negotiations between the House and Senate, so we have quite a way to go. Talking to our partners in Washington, D.C., they say that it's pretty unlikely that any of our projects would be affected by the negotiations, as it's more of a policy.

We also submitted our application for the Safe Streets and Roads For All Grant a couple weeks ago and we're hoping for some good news. We're asking for \$9 million. We learned a lot from the last go around where we asked for money for several projects, so we narrowed this one down to 6<sup>th</sup> Street in Reno.

## **6.3 NDOT Director Report**

NDOT Deputy Director Darin Tedford started off with a couple of informal responses to questions that were raised during the meeting.

In the Bipartisan Infrastructural Law Funding, there was specific funding designated for structures. That is a huge increase to what we would typically get for Federal Funding, which amounts to about \$45 million per year. Part of that money goes to off system bridges, which we are working on with RTC for several bridges in Washoe County. The bulk of that money will be used on State maintained bridges and we've identified two sections. Garson Road is one of the bridges in the seven-bridge package. The rest of the bridges come back towards Keystone Avenue, and we're going to put out a design build project to replace those. There will be between \$100 million and \$200 million design build project coming up in the next couple of years.

We are setting up the Garson Bridge to be replaced and widened in the best manner possible. We are working with RTC on this now.

There was legislation that was issued for a \$5 million earmark to work towards the State match for Federal Grants for Wildlife Crossings. So, along with that legislation comes the requirement for an implementation of a plan, appointment of Board members for making those decisions and where they should be done. NDOT has done a lot of work Statewide looking at where we have issues because we track every type of crash that happens in the State, including crashes with wildlife. So, we have that data and the ability to inform the decision on priorities of crossings and we're going to go after Grants. The \$5 million typically would end up being 5% of a Grant match, it depends on the grant, but that \$5 million could turn into \$100 million easily and let us put wildlife crossings where they are needed and prioritized. We will be working on that in the future.

For our official report, we will start with our safety improvements update. The RTC is working with NDOT to install a roundabout at Woodland and Mesa Park to improve safety on 4<sup>th</sup> Street. That will improve safety for not just the traffic traveling on 4<sup>th</sup> Street, but coming in from both of those access roads. We are also working on concepts for roundabouts to improve safety further to the east on 4<sup>th</sup> Street.

As far as the Covid-19 funding recession update, we did not lose any of those funds that were offered to the States previously.

Project updates on Mt. Rose Highway Safety Improvements, two of the main things is drainage going along Mt. Rose Highway to come down from about Thomas Creek down about a mile where there is an open ditch. We're putting some of that drainage in pipes, so we can have a multi-use path that goes from Thomas Creek down to the neighborhood that is less than ¼ mile down that leads to the school. Those improvements are expected to be completed in the fall.

We will also be doing some lane closures on I-80 West for emergency surface repairs.

Our last major Reno I-580 marathon weekend will be this weekend. Starting this evening, July 21<sup>st</sup>, traffic control is already being set out and the message boards are up all the way down to Washoe Valley. We put 5 million pounds of new polymer concrete on redoing the surface on the bridge via duct over the top of the RTC bus station.

Commissioner Duerr had a follow up question on the Garson overpass. Are you going to keep it one lane each way instead of two lanes? Also, if it will be one lane each way, can you add the pedestrian bike lane now?

Darin Tedford, correct that it will be one lane each way. We believe we can add the bike lane, because that's not adding capacity in terms of vehicles, but we're double checking and we realize that is a valuable component to have.

***Item 7            COMMISSIONER ANNOUNCEMENTS AND UPDATES***

Commissioner Schieve would like to have conversations with the Board and have the following items put on the next Board agenda:

- Paving and lighting the Truckee River.
- 4<sup>th</sup> Street safety, lighting and crosswalk flashers.
- Street lights in general and replacing them on a regular schedule, rather than only when a project comes up in an area.
- Pilot for transportation up to Lake Tahoe.

***Item 8            PUBLIC INPUT***

Vice Chair Hill opened the meeting to public input and called on anyone wishing to speak on topics relevant to the Regional Transportation Commission (RTC) that are not included in the current agenda.

Ms. Dora Uchel-Martinez, NV Disability Peer Action Coalition Advocate and Coalition for the Blind, wanted everyone to know that July is Disability Awareness Month. I wanted to thank Bill Thomas, Mark Maloney and Susi Trinidad for all of their help. Mayor Schieve and everyone for their continued work in helping make our area ADA safe.

There being no one else wishing to speak, the Vice Chair Hill closed public input.

*Item 9           ADJOURNMENT*

There being no further business to come before the Board, the meeting adjourned at 11:28 a.m.

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ALEXIS HILL, Vice Chair  
Regional Transportation Commission

**\*\*Copies of all presentations are available by contacting Michelle Kraus at [mkraus@rtcwashoe.com](mailto:mkraus@rtcwashoe.com).**



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.2.1

To: Regional Transportation Commission

From: Christian Schonlau, Director of Finance/CFO

**SUBJECT: Procurement Activity Report**

**RECOMMENDED ACTION**

Acknowledge receipt of the month Procurement Activity Report. (For Possible Action)

**BACKGROUND AND DISCUSSION**

See Attachment A for Background and Discussion.

**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

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## ATTACHMENT A

### PROJECTS CURRENTLY ADVERTISED

<b><u>Invitations for Bids (IFB)</u></b>	
<b>Project</b>	<b>Due Date</b>
2023 RTC Corrective Maintenance	August 3, 2023
<b><u>Request for Proposals (RFP)</u></b>	
<b>Project</b>	<b>Due Date</b>
RTC Regional Travel Characteristics Study	August 21, 2023
HVAC Preventive Maintenance	August 23, 2023
Snow and Ice Removal	August 31, 2023
Employee Classification, Compensation and Fringe Benefit Study	September 9, 2023

### REPORT ON INVITATION FOR BID (IFB) AWARDS

*Per NRS 332, NRS 338 and RTC's Management Policy P-13 "Purchasing," the Executive Director has authority to negotiate and execute a contract with the lowest responsive and responsible bidder on an Invitation for Bid (IFB) without Commission approval.*

<b>Project</b>	<b>Contractor</b>	<b>Award Date</b>	<b>Contract Amount</b>
Las Brisas & Los Altos Resurfacing	Sierra Nevada Construction	7/28/2023	\$4,448,007
McCarran Boulevard & Pyramid Way Fiber Connectivity	Quanta Telecom Services	7/12/2023	\$1,337,514

### PROFESSIONAL SERVICES/CONSULTING AGREEMENTS

*Per RTC's Management Policy P-13 Executive Director has authority to approve contracts greater than \$25,000 and less than (or equal to) \$100,000.*

<b>Project</b>	<b>Contractor</b>	<b>Contract Amount</b>
Specialized Legal Services	Zev E. Kaplan, Ltd.	\$100,000

### CHANGE ORDERS AND CONTRACT AMENDMENTS WITHIN EXECUTIVE DIRECTOR'S RTC'S P-13 PURCHASING POLICY AUTHORITY

<b>Project</b>	<b>Contractor</b>	<b>Approval Date</b>	<b>CO / Amend. Number</b>	<b>CO / Amend. Amount</b>	<b>Revised Total Contract Amount</b>
Sky Vista Widening and Rehabilitation	Atkins North America	7/25/2023	Amend 4	\$40,000	\$2,500,173
ITS Traffic Management Phase 4	Kimley-Horn and Associates	7/26/2023	Amend 1	\$13,115	\$410,722





**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.2.2

To: Regional Transportation Commission

From: Dan Doenges, PTP, RSP, Director of Planning

**SUBJECT: Advisory Committees Report**

**RECOMMENDED ACTION**

Acknowledge receipt of the Summary Report for the Technical, Citizens Multimodal, and Regional Road Impact Fee Advisory Committees.

**BACKGROUND AND DISCUSSION**

The RTC has three advisory committees that provide input on a wide range of policy and planning issues as well as key planning documents and the RTC Budget. The committees include:

- The Citizens Multimodal Advisory Committee (CMAC), which includes three individuals who use RTC RIDE, two individuals who use RTC ACCESS, five individuals who represent bicyclists/pedestrians, and five individuals who represent general multimodal transportation. The RTC Board approves appointments to this advisory committee.
- The Technical Advisory Committee (TAC), which includes local public works directors, community development directors, and staff from other key agencies.
- The Regional Road Impact Fee Technical Advisory Committee (RRIF TAC), which was created to oversee and advise the local governments regarding land use classification assumptions and the Capital Improvements Plan (CIP) used in the impact fee program. The RRIF TAC consists of three representatives from each local entity, two RTC representatives, and four private sector members who are appointed by the RTC Board.

The CMAC and the TAC met on August 2, 2023 and August 3, 2023, respectively. Each committee recommend approval of the Proposed FFY 2023-2027 Regional Transportation Improvement Program (RTIP).

There has not been a RRIF TAC meeting since the Board previously met.

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**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.2.3

To: Regional Transportation Commission

From: Daniel Doenges, Director of Planning

**SUBJECT: Planning Activity Report**

**RECOMMENDED ACTION**

Acknowledge receipt of the monthly Planning Activity Report.

**BACKGROUND AND DISCUSSION**

See Attachment A for Background and Discussion.

**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

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## PLANNING STUDIES

<b>Virginia Street Transit Oriented Development (TOD) Planning Study</b>	
Graham Dollarhide, Project Manager	<a href="https://www.rtcwashoe.com/mpo-corridor-plan/south-virginia-street-transit-oriented-development-tod-study/">https://www.rtcwashoe.com/mpo-corridor-plan/south-virginia-street-transit-oriented-development-tod-study/</a>
<i>Status: Meeting with City of Reno held July 13. Survey results and transit model alternatives scheduled to be shared with Project TAC by the end of August, ahead of second set of public workshops and draft study recommendations.</i>	
<b>Active Transportation Plan</b>	
Dan Doenges, Project Manager	<a href="https://www.rtcwashoe.com/mpo-reports/active-transportation-plan/">https://www.rtcwashoe.com/mpo-reports/active-transportation-plan/</a>
<i>Status: Public outreach results and next steps shared with Project TAC on July 18. Agency Working Group meeting 1 held August 7 with local jurisdictions to discuss potential standards, maintenance issues, and implementation guidelines.</i>	
<b>Regional Freight Study</b>	
Marquis Williams, Project Manager	N/A
<i>Status: Proposals received; selection consensus meeting held July 24. Contract scheduled to be awarded September.</i>	
<b>RTC Website Update</b>	
RTC Graphics Team	<a href="https://www.rtcwashoe.com/">https://www.rtcwashoe.com/</a>
<i>Status: Working with website maintenance contractor to refine web layout/structure.</i>	
<b>ONGOING PROGRAMS</b>	
<b>Data Collection Program</b>	
James Weston, Project Manager	N/A
<i>Status: Field survey conducted to determine ideal data collection conditions at identified locations.</i>	
<ul style="list-style-type: none"><li>• <i>Data collection paused for summer and will resume after school is back in session.</i></li><li>• <i>Summary reports received for initial collection sites.</i></li></ul>	
<b>Bicycle and Pedestrian Planning</b>	
RTC Planning and Engineering Staff	<a href="https://www.rtcwashoe.com/metropolitan-planning/">https://www.rtcwashoe.com/metropolitan-planning/</a>
<i>Status: Ongoing collaboration with partner agencies on several initiatives to improve bicycle and pedestrian safety &amp; facilities:</i>	
<ul style="list-style-type: none"><li>• <i>Safe Streets and Roads for All (SS4A) application for East 6<sup>th</sup> Street improvements submitted.</i></li><li>• <i>Coordinating with Engineering to develop design details on roadway network concepts and outreach activities.</i></li></ul>	
<b>Vision Zero Truckee Meadows</b>	
RTC Planning Staff	<a href="https://visionzerotruckeemeadows.com/">https://visionzerotruckeemeadows.com/</a>
<i>Status: Meeting held August 14<sup>th</sup> and items discussed include a Smart &amp; Connected Communities grant being pursued by UNR, the recently submitted SS4A grant application, an Active Transportation Plan update, and Downtown Reno Micromobility Project update.</i>	



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.2.4

To: Regional Transportation Commission

From: Dale Keller, Director of Engineering

**SUBJECT: Engineering Activity Report**

**RECOMMENDED ACTION**

Acknowledge receipt of the monthly Engineering Activity Report.

**BACKGROUND AND DISCUSSION**

See Attachment A for Background and Discussion.

**FISCAL IMPACT**

Funding for this item is included in the approved FY 2024 budget.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

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## BICYCLE AND PEDESTRIAN IMPROVEMENTS

<b>Center Street Multimodal Improvements</b>	
Maria Paz Fernandez, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/center-street-multimodal-improvements-project/">https://www.rtcwashoe.com/engineering-project/center-street-multimodal-improvements-project/</a>
<i>Status: Thirty percent (30%) design plans are produced. The project is being evaluated as part of the Downtown Micro Mobility Network Phase 1.</i>	

## CAPACITY/CONGESTION RELIEF PROJECTS

<b>4<sup>th</sup> Street/Woodland Avenue Roundabout</b>	
Garrett Rodgers, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/4th-street-and-woodland-avenue-roundabout/">https://www.rtcwashoe.com/engineering-project/4th-street-and-woodland-avenue-roundabout/</a>
<i>Status: The roundabout construction began in March with the construction of the retaining wall. Construction will continue throughout the summer.</i>	

<b>Buck Drive Circulation</b>	
Maria Paz Fernandez, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/buck-drive-circulation/">https://www.rtcwashoe.com/engineering-project/buck-drive-circulation/</a>
<i>Status: Kimley Horn is the selected firm for the design and construction engineering services. Preliminary design started at the end of last month. Ongoing coordination with City of Reno staff.</i>	

<b>Geiger Grade Realignment</b>	
Kim Diegle, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/geiger-grade-road-realignment/">https://www.rtcwashoe.com/engineering-project/geiger-grade-road-realignment/</a>
<i>Status: Access and Circulation study is underway. RTC is in preliminary analysis stage of restarting the project. Evaluation of the procurement method for traffic, design, and environmental services is underway.</i>	

<b>Kietzke Lane ITS</b>	
Garrett Rodgers, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/kietzke-lane-its-project/">https://www.rtcwashoe.com/engineering-project/kietzke-lane-its-project/</a>
<i>Status: The consultant began design of the project in June 2023.</i>	

<b>Military Road Capacity &amp; Safety</b>	
Kim Diegle, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/military-road-capacity-and-safety/">https://www.rtcwashoe.com/engineering-project/military-road-capacity-and-safety/</a>
<i>Status: Project kick-off has occurred with City of Reno. The consultant design team is proceeding with topographic survey, investigation of existing conditions, and traffic analysis.</i>	

<b>N McCarran Boulevard &amp; Pyramid Hwy Fiber</b>	
Alex Wolfson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/mccarran-pyramid-fiber/">https://www.rtcwashoe.com/engineering-project/mccarran-pyramid-fiber/</a>
<i>Status: Construction contract has been awarded to Quanta Telecommunication Solutions. Construction is expected to begin in September.</i>	

<b>North Valleys North Virginia Street Capacity</b>	
Garrett Rodgers, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/north-valleys-north-virginia-street-capacity/">https://www.rtcwashoe.com/engineering-project/north-valleys-north-virginia-street-capacity/</a>
<i>Status: The project is underway with early scoping and schedule items.</i>	
<b>Pembroke Drive Capacity &amp; Safety</b>	
Maria Paz Fernandez, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/pembroke-drive-capacity-and-safety/">https://www.rtcwashoe.com/engineering-project/pembroke-drive-capacity-and-safety/</a>
<i>Status: Nichols Consulting Engineers (NCE) was the selected design consultant and the team has started the preliminary design. Construction is tentatively scheduled for spring 2025.</i>	
<b>Pyramid Highway Intelligent Corridor</b>	
Alex Wolfson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/pyramid-highway-intelligent-corridor/">https://www.rtcwashoe.com/engineering-project/pyramid-highway-intelligent-corridor/</a>
<i>Status: The consultant is in the process of procuring necessary software for system functionality. Fiber installation design is nearing 90%. Testing of software is expected to occur in the fall.</i>	
<b>Pyramid Way, Sparks Boulevard, Highland Ranch Interchange</b>	
Amanda Callegari, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/pyramid-highway-us-395-connection-project/">https://www.rtcwashoe.com/engineering-project/pyramid-highway-us-395-connection-project/</a>
<i>Status: Finalizing scope of services and beginning fee negotiations with consultant for the preliminary design and engineering services associated with the Pyramid Way/Sparks Blvd/Highland Ranch Interchange and Connector Improvements.</i>	
<b>South Meadows Traffic Enhancements</b>	
Sara Going, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/south-meadows-traffic-enhancements/">https://www.rtcwashoe.com/engineering-project/south-meadows-traffic-enhancements/</a>
<i>Status: The project is obtaining final approvals in preparation for advertising for construction bids.</i>	
<b>South Virginia Street &amp; I-580 Exit 29 Capacity &amp; Safety</b>	
Maria Paz Fernandez, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/south-virginia-street-nb-lane-widening/">https://www.rtcwashoe.com/engineering-project/south-virginia-street-nb-lane-widening/</a>
<i>Status: Ongoing coordination with NDOT and City of Reno. Ninety percent (90%) design plans were submitted. Right-of-way process and Public/Stakeholder meetings are underway. Construction is tentatively scheduled to start in spring 2024.</i>	
<b>Sparks Boulevard – North Phase</b>	
Garrett Rodgers, Project Manager	<a href="https://SparksBLVDproject.com">SparksBLVDproject.com</a> .
<i>Status: Project design will continue to advance with final design anticipated for late summer 2023. Utility relocations are being evaluated to ensure project improvements are not in conflict.</i>	

<b>Steamboat Parkway Improvement</b>	
Amanda Callegari, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/steamboat-pkwy-improvement/">https://www.rtcwashoe.com/engineering-project/steamboat-pkwy-improvement/</a>
<i>Status: The utility coordination and right-of-way acquisition process is ongoing. Construction is anticipated to start in spring 2024.</i>	
<b>Traffic Management – ITS Phase 4</b>	
Alex Wolfson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/its-traffic-management-phase-4/">https://www.rtcwashoe.com/engineering-project/its-traffic-management-phase-4/</a>
<i>Status: Project construction is substantially complete. Minor punch list and construction closeout tasks are still in progress.</i>	
<b>Traffic Signal Installations 23-01</b>	
Alex Wolfson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/traffic-signal-installations-23-01/">https://www.rtcwashoe.com/engineering-project/traffic-signal-installations-23-01/</a>
<i>Status: 100% design plans are currently being reviewed. Contract bid opening is expected in late September.</i>	
<b>Traffic Signal Modifications 23-01</b>	
Sara Going, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/traffic-signal-modifications-23-01/">https://www.rtcwashoe.com/engineering-project/traffic-signal-modifications-23-01/</a>
<i>Status: The final plans and specifications are being prepared to advertise for construction bids in August.</i>	
<b>Traffic Signal Modifications 24-01</b>	
Sara Going, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/traffic-signal-modifications-24-01/">https://www.rtcwashoe.com/engineering-project/traffic-signal-modifications-24-01/</a>
<i>Status: The project team has completed data collection and field review for the following sites: N. McCarran &amp; 7th Street, S. Virginia St &amp; Center Street, Proctor R. Hug High School, and Outlets at Legends/ Lincoln Way.</i>	
<b>Traffic Signal Timing 7</b>	
Alex Wolfson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/traffic-signal-timing-7-project/">https://www.rtcwashoe.com/engineering-project/traffic-signal-timing-7-project/</a>
<i>Status: The interlocal cooperative agreements have been approved and executed by Reno, Sparks, UNR, and RTC. The notices to proceed were issued on July 3, 2023. This project is now underway.</i>	
<b>Veterans Parkway ITS</b>	
Alex Wolfson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/veterans-parkway-its/">https://www.rtcwashoe.com/engineering-project/veterans-parkway-its/</a>
<i>Status: HDR has been selected as the design consultant for this project. The Professional Services Agreement will be considered by the RTC Board in August.</i>	



<b>Veterans Roundabout Modifications</b>	
Jessica Dover, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/veterans-roundabout-modifications/">https://www.rtcwashoe.com/engineering-project/veterans-roundabout-modifications/</a>
<i>Status: Project Kick-off was conducted in June 2023; Preliminary Engineering efforts are currently underway.</i>	

<b>Vista Boulevard/Prater Way ITS</b>	
Garrett Rodgers, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/vista-boulevard-prater-way-its/">https://www.rtcwashoe.com/engineering-project/vista-boulevard-prater-way-its/</a>
<i>Status: Preliminary design is ongoing through fall 2023.</i>	

## CORRIDOR IMPROVEMENT PROJECTS

<b>Arlington Avenue Bridges</b>	
Judy Tortelli, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/arlington-avenue-bridges-project/">https://www.rtcwashoe.com/engineering-project/arlington-avenue-bridges-project/</a>
<i>Status: The 60% design is complete. Coordination with utility companies is on-going. A Section 408 permit for geotechnical borings was submitted to Carson Truckee Water Conservancy District. Coordination with USACE, FHWA, and NDOT continues.</i>	

<b>Keystone Bridge Replacement</b>	
Sara Going, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/keystone-avenue-bridge-replacement/">https://www.rtcwashoe.com/engineering-project/keystone-avenue-bridge-replacement/</a>
<i>Status: The project team has begun collecting data and performing field reviews for the project. In August, the team will develop the screening criteria for assessing project alternatives and begin evaluation of environmental constraints and opportunities.</i>	

<b>Lemmon Drive Traffic Improvements and Resiliency</b>	
Amanda Callegari, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/lemmon-drive-segment-2/">https://www.rtcwashoe.com/engineering-project/lemmon-drive-segment-2/</a>
<i>Status: Environmental services and final design is underway. The RTC Board approved a PSA with CA Group at the July 2023 Board Meeting.</i>	

<b>Mill Street Capacity &amp; Safety (Kietzke Lane to Terminal Way)</b>	
Kimberly Diegle, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/mill-st-widening-kietzke-to-terminal/">https://www.rtcwashoe.com/engineering-project/mill-st-widening-kietzke-to-terminal/</a>
<i>Status: 90% design submittal is due in October. The right-of-way acquisition process has begun. Coordination with utility companies is on-going. Outreach with adjacent business owners continues. Presented at Reno Sparks Indian Colony Tribal Council who provided approval for permission to construct. Construction anticipated to start early 2025.</i>	

<b>Oddie/Wells Multimodal Improvements</b>	
Maria Paz Fernandez, Project Manager	<a href="http://oddiewellsproject.com/">http://oddiewellsproject.com/</a>
<p><i>Status:</i></p> <p><i>Finalizing landscape installation on Phase 1 (Pyramid Way to Sullivan Lane in Sparks).</i></p> <p><i>Ongoing construction activities on the southside of Oddie Blvd on Phase 2 (Silverada Blvd to east of US 395 in Reno) and are expected to be completed by the end of September.</i></p> <p><i>Construction activities on Phase 3 and 4 were resumed.</i></p> <p><i>One lane on each direction is maintained along Oddie Boulevard.</i></p> <p><i>Overall construction, including the remaining phases, is anticipated to continue over the next construction seasons and be complete by the third quarter of 2024.</i></p>	

<b>Sierra Street Bridge Replacement</b>	
Amanda Callegari, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/mill-st-widening-kietzke-to-terminal/">https://www.rtcwashoe.com/engineering-project/mill-st-widening-kietzke-to-terminal/</a>
<p><i>Status: Coordination with utility companies has begun. A kick-off meeting occurred in July.</i></p>	

<b>Sky Vista Parkway Widening Rehabilitation</b>	
Judy Tortelli, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/sky-vista-widening-rehabilitation-project/">https://www.rtcwashoe.com/engineering-project/sky-vista-widening-rehabilitation-project/</a>
<p><i>Status: Construction activities are ongoing. This project is anticipated to be complete this fall.</i></p>	

<b>West Fourth Street Downtown</b>	
Scott Gibson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/west-fourth-street-downtown/">https://www.rtcwashoe.com/engineering-project/west-fourth-street-downtown/</a>
<p><i>Status: The PSA with Wood Rodgers has been executed and initial design and project kickoff activities have begun.</i></p>	

<b>West Fourth Street Safety</b>	
Scott Gibson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/west-fourth-street-safety/">https://www.rtcwashoe.com/engineering-project/west-fourth-street-safety/</a>
<p><i>Status: Design is underway and coordination with NDOT environmental continues. Contact with Union Pacific Railroad for potential right-of-way impacts continues.</i></p>	

<b>Truckee River Shared Use Path</b>	
Scott Gibson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/truckee-river-shared-use-path-project/">https://www.rtcwashoe.com/engineering-project/truckee-river-shared-use-path-project/</a>
<i>Status: The RTC is continuing to coordinate with the Reno Sparks Indian Colony (RSIC) for the necessary property in which the pathway will traverse. There has been no progress on this in recent months.</i>	

## PAVEMENT PRESERVATION PROJECTS

<b>2023 Bridge Maintenance</b>	
Scott Gibson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/2023-bridge-maintenance/">https://www.rtcwashoe.com/engineering-project/2023-bridge-maintenance/</a>
<i>Status: 90% design plans have been reviewed by the city of Reno with minor comments. Plan to advertise by late August with an October construction start.</i>	

<b>2023 Corrective Maintenance (Neil Road &amp; the Greg Street Intersection)</b>	
Scott Gibson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/2023-corrective-maintenance/">https://www.rtcwashoe.com/engineering-project/2023-corrective-maintenance/</a>
<i>Status: Bids have been opened and the project was awarded to Sierra Nevada Construction.</i>	

<b>2023 Preventive Maintenance Program</b>	
Scott Gibson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/2023-preventive-maintenance-program/">https://www.rtcwashoe.com/engineering-project/2023-preventive-maintenance-program/</a>
<i>Status: The project is well underway. Weather had some impact early on, but the contractor is now on schedule and making progress.</i>	

<b>4<sup>th</sup> Street (Sparks) Reconstruction</b>	
Judy Tortelli, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/lemmon-dr-segment-1/">https://www.rtcwashoe.com/engineering-project/lemmon-dr-segment-1/</a>
<i>Status: Construction continues throughout the summer.</i>	

<b>Arrowcreek Parkway Rehabilitation</b>	
Kimberly Diegle, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/arrowcreek-pkwy-rubblestone-to-virginia/">https://www.rtcwashoe.com/engineering-project/arrowcreek-pkwy-rubblestone-to-virginia/</a>
<i>Status: Construction began on June 12, 2023, and will be complete in mid to late August. Paving operations are complete.</i>	

<b>California Avenue at Newlands Terrace Rehabilitation</b>	
Scott Gibson, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/california-avenue-at-newlands-terrace-rehabilitation/">https://www.rtcwashoe.com/engineering-project/california-avenue-at-newlands-terrace-rehabilitation/</a>
<i>Status: The project is well under way with the contractor making good progress. The south side of the road has been paved and after the Hot August Nights shut down, the north side of the road will be completed before school starts.</i>	
<b>Holcomb Avenue Rehabilitation</b>	
Amanda Callegari, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/holcomb-avenue-rehabilitation/">https://www.rtcwashoe.com/engineering-project/holcomb-avenue-rehabilitation/</a>
<i>Status: Construction is anticipated to be complete in September.</i>	
<b>Las Brisas and Los Altos Resurfacing</b>	
Jessica Dover, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/las-brisas-and-los-altos-resurfacing/">https://www.rtcwashoe.com/engineering-project/las-brisas-and-los-altos-resurfacing/</a>
<i>Status: Bids were opened July 26, 2023. Construction is scheduled to commence in late August/early September 2023, with an allocated 45 working days under the contract.</i>	
<b>N Virginia Street University Rehabilitation</b>	
Amanda Callegari, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/north-virginia-street-university-rehabilitation/">https://www.rtcwashoe.com/engineering-project/north-virginia-street-university-rehabilitation/</a>
<i>Status: Design has reached the 50% milestone with 90% design anticipated in late August.</i>	
<b>Raleigh Heights Rehabilitation</b>	
Judy Tortelli, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/raleigh-heights-rehabilitation/">https://www.rtcwashoe.com/engineering-project/raleigh-heights-rehabilitation/</a>
<i>Status: Lumos and Associates, Inc., begins preliminary design and thirty percent design is scheduled to be submitted in March 2023. Construction is tentatively scheduled for March through August 2024.</i>	
<b>Selmi Drive Rehabilitation</b>	
Maria Paz Fernandez, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/selmi-drive-rehabilitation/">https://www.rtcwashoe.com/engineering-project/selmi-drive-rehabilitation/</a>
<i>Status: DOWL (formerly Farr West Engineering) is the selected team for the design. Ninety percent (90%) design plans were submitted to the City last month. Construction is tentatively scheduled for spring 2024.</i>	
<b>Stanford Way Rehabilitation</b>	
Kimberly Diegle, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/stanford-way-rehabilitation/">https://www.rtcwashoe.com/engineering-project/stanford-way-rehabilitation/</a>
<i>Status: 90% design will be submitted in August. Coordination with the City of Sparks and affected utility companies continues. Construction is anticipated for spring 2024.</i>	

## OTHER PROJECTS

<b>4<sup>th</sup> Street Station Expansion</b>	
Jeff Wilbrecht, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/4th-street-station-expansion/">https://www.rtcwashoe.com/engineering-project/4th-street-station-expansion/</a>
<i>Status: This project is on hold due as a result of ongoing coordination with City of Reno.</i>	

<b>Peppermill BRT Station</b>	
Ian Chamberlain, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/peppermill-brt/">https://www.rtcwashoe.com/engineering-project/peppermill-brt/</a>
<i>Status: The construction is complete.</i>	

<b>Virginia Line BRT Improvements</b>	
Kimberly Diegle, Project Manager	<a href="https://www.rtcwashoe.com/engineering-project/virginia-line-brt-improvements/">https://www.rtcwashoe.com/engineering-project/virginia-line-brt-improvements/</a>
<i>Status: Thirty percent (30%) design is complete. Coordination with the City of Reno, FTA, and affected utility companies continues in advance of the 60% design submitted in August. NEPA re-evaluation of the original Virginia Street Bus RAPID Transit Extension project is underway, including Tribal consultation.</i>	

## REPORT ON NEGOTIATED SETTLEMENT AGREEMENTS FOR THE ACQUISITION OF PROPERTY

<b>Project</b>	<b>Property Owner</b>	<b>Purchase Amount</b>	<b>Amount Over Appraisal</b>
Steamboat Parkway Improvement	DD-1, LLC	\$1,475.00	\$0
Steamboat Parkway Improvement	Mountain America Federal Credit Union	\$78,840.00	\$0
Steamboat Parkway Improvement	Nevada Tri Partners	\$294,456.00	\$0
Steamboat Parkway Improvement	Trade Center Maintenance Association	\$29,085.00	\$0
Steamboat Parkway Improvement	Damonte Ranch Landscape Maintenance Association	\$57,398.00	\$0
Las Brisas & Los Altos Resurfacing	DH Minis, LLC	\$1,659.00	\$0
Traffic Signal Modifications 22-01	Syufy Enterprises	\$1,920.00	\$0
Traffic Signal Modifications 22-01	Sanders Family Trust	\$7,575.00	\$15,000.00

**CONTRACTS UP TO \$100,000**

<b>Project</b>	<b>Vender</b>	<b>Scope</b>	<b>Amount</b>
N/A			



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.2.5

To: Regional Transportation Commission

From: Mark Maloney, Director of Public Transportation and Operations

**SUBJECT: Public Transportation and Operations Activity Report**

**RECOMMENDED ACTION**

Acknowledge receipt of the monthly Public Transportation and Operations Report.

**BACKGROUND AND DISCUSSION**

See Attachment A for background and discussion.

**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

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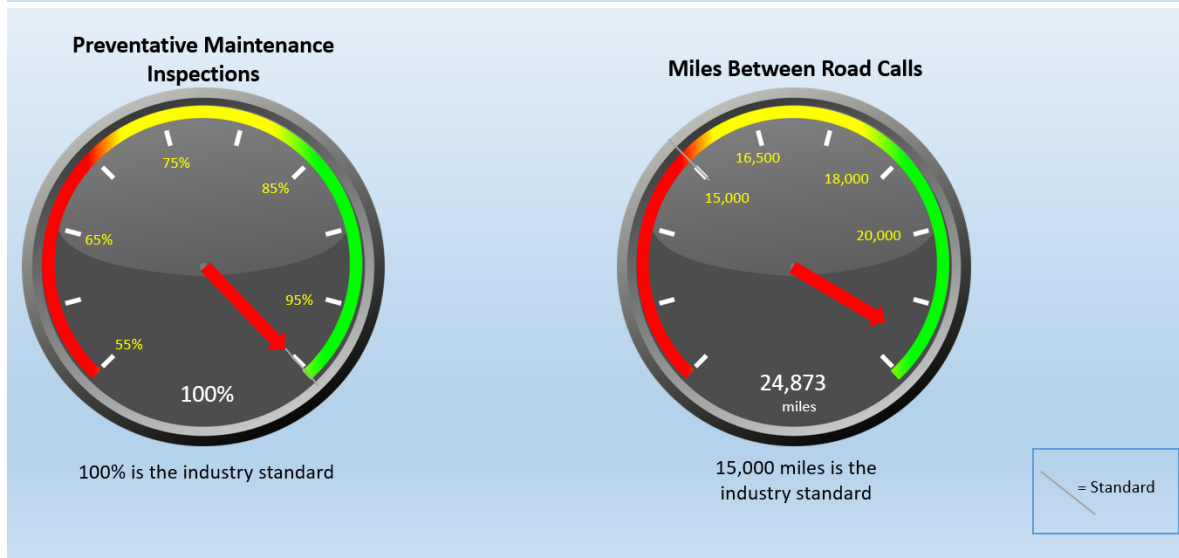
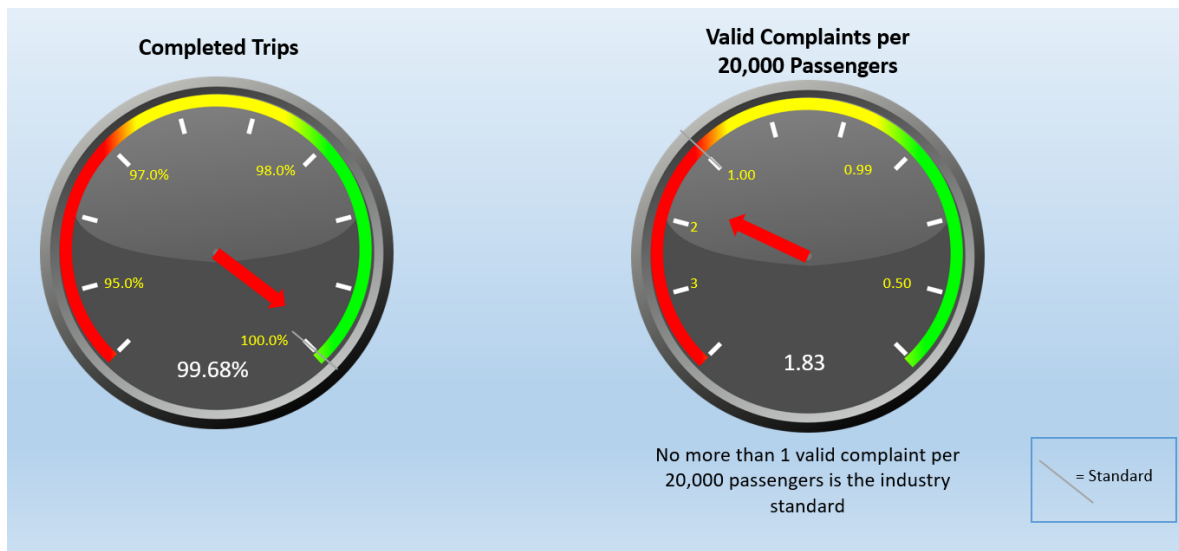


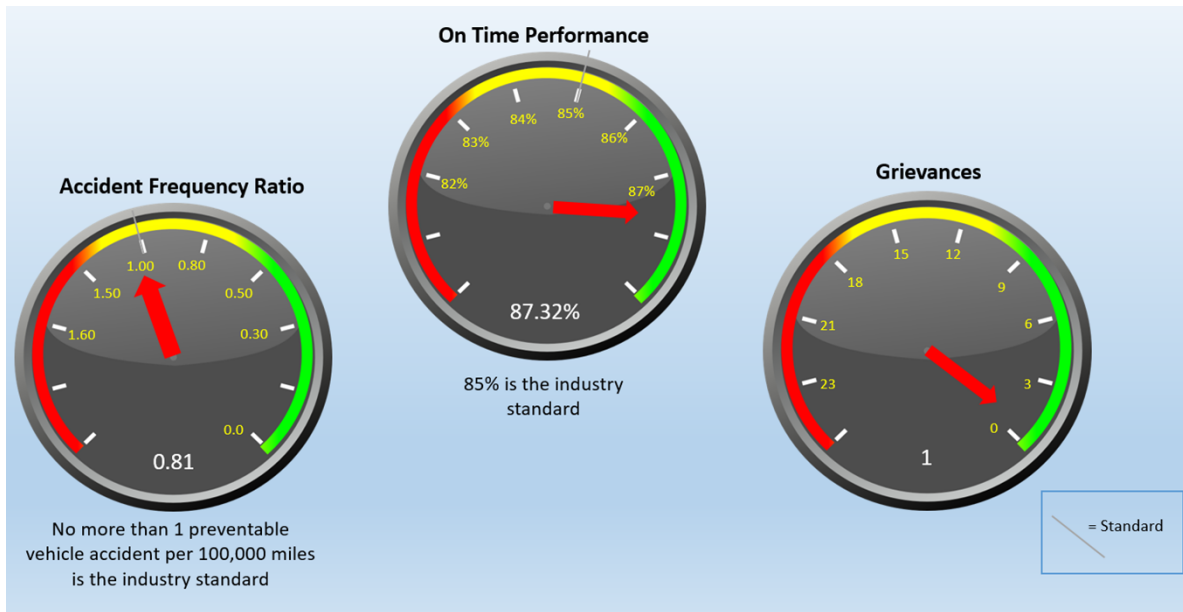


**Keolis represented staffing headcount as of July 31, 2023:**

Position	Total Employed	#Needed
Coach Operator Trainees	2	0
Coach Operators	156	9
Dispatchers	5	1
Road Supervisors	4	0
Manager On Duty	4	0
Mechanic A	5	1
Mechanic B	4	1
Mechanic C	4	0
EV Technician	1	0
Maintenance Supervisor	3	0
Electronics Tech	2	0
Body Technician	1	0

**RTC RIDE Contract Compliance for June**





**RTC ACCESS Key Highlights – July**

**MTM Classes Held:** July 11, 2023 – 4 of 7 made it through training.  
 July 25, 2023 – 3 currently in training

**Safety:**

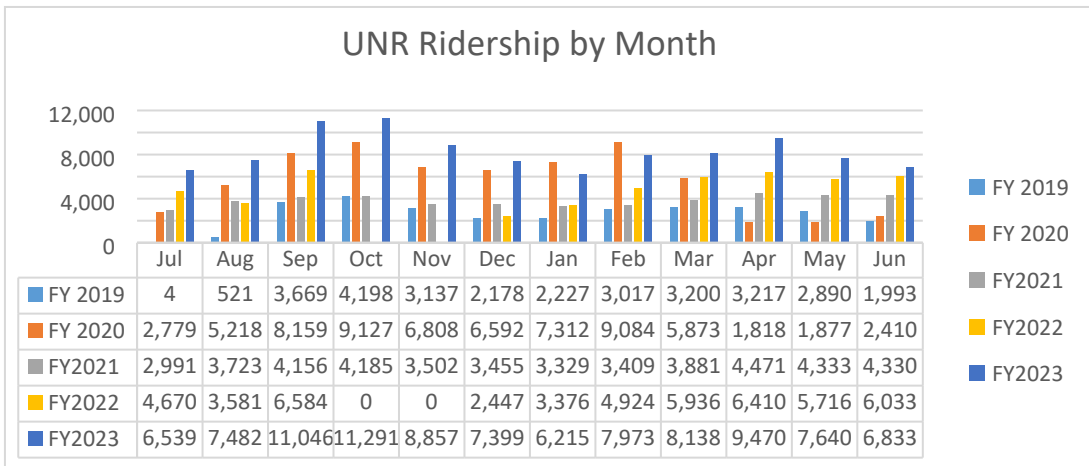
- **Accidents:**
  - 0 Preventable
  - 3 Non Preventable
- **Incidents**
  - 1 Preventable.
- **Injuries:**
  - 1
- **YTD Preventable Accident Count: 5**
- **YTD Injury Count: 7**
- **July Safety Blitz**
  - Backing and footwear advise.
- **July Safety Meeting**
  - FTA drug awareness video

**MTM represented staffing headcount as of July 31, 2023:**

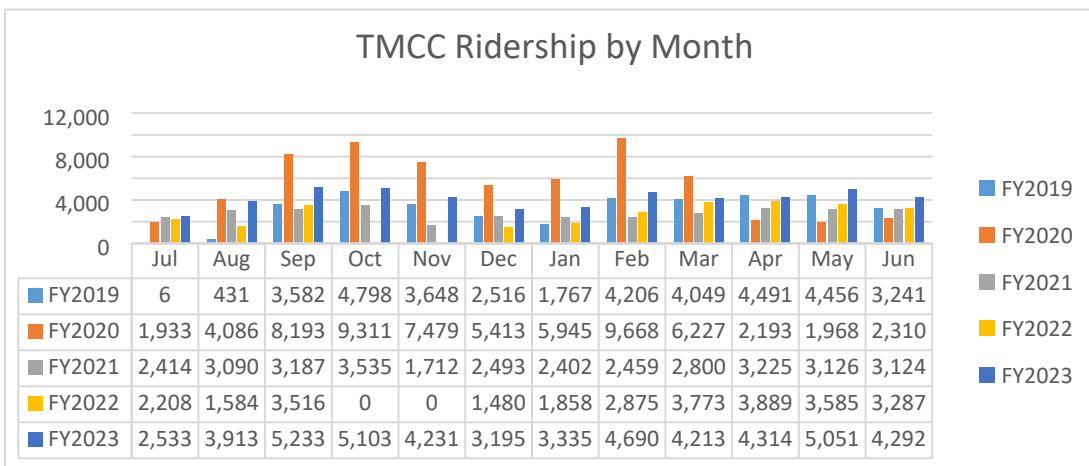
Position	Total Employed	#Needed
Drivers	52FT – 8PT	3 FT – 0 PT
Dispatchers	4 FT	0
Reservationists	4.5 FTE's	0
Mechanic A	3 FT	0
Utility Worker	1	0
Facility Technician	1	0

**TRANSIT DEMAND MANAGEMENT (TDM) Update**

- Vanpools dropped to 325. Staff continues to work with residents of Lake Tahoe region to start more vanpools. Both the Truckee North Tahoe TMA (Transportation Management Association) and South Shore TMA are looking into grants to get further subsidies to help support vanpools.
- Staff continues to work on filling the board for the Northern Nevada Transportation Management Association. Prospective board members met via zoom on July 19. Another meeting is planned for mid-August.
- Staff presented at the Green Energy Business training at the Chamber of Commerce on July 18.
- Staff presented RTC’s vanpool program to the South Shore TMA on July 21.
- Staff maned a table at the Reno Aces game on July 25.
- Staff will be attending the Association of Commuter Transportation International conference in Seattle from July 30 – August 2.
- Ridership numbers from the ED Pass Program through the month of June 2023:



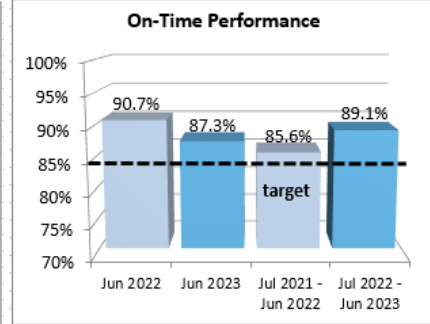
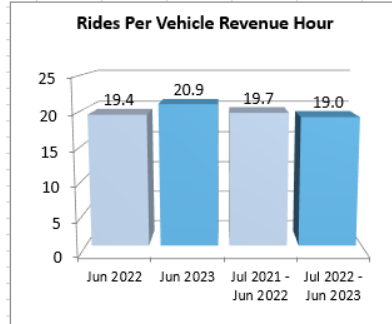
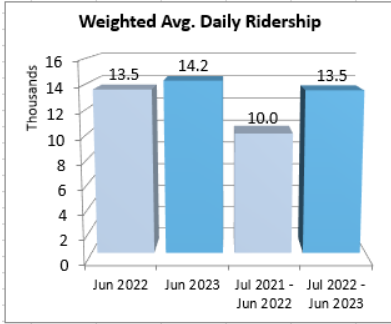
***Ridership at UNR this year has surpassed the combined totals for the last 2 years.***



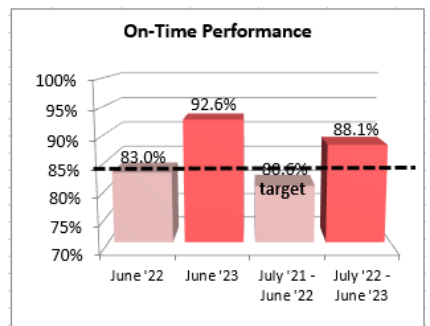
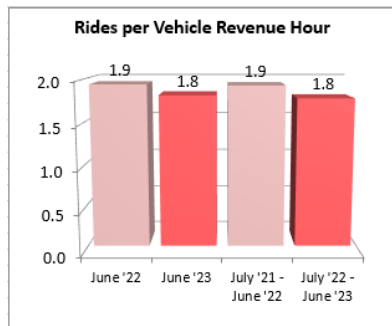
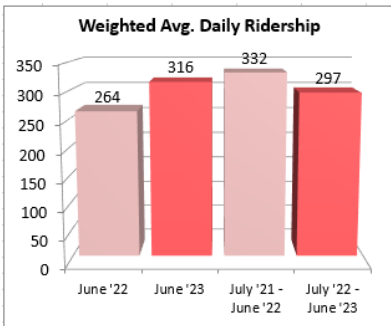
***Ridership numbers in October & November of 2021 were affected by the driver strikes.***

# JUNE 2023 TRANSIT PERFORMANCE

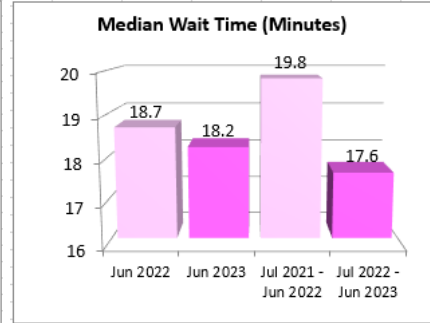
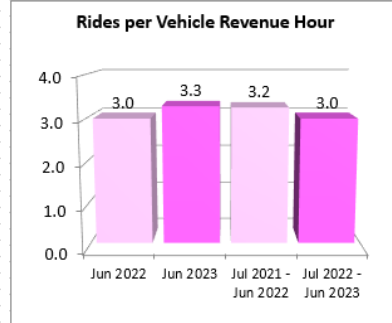
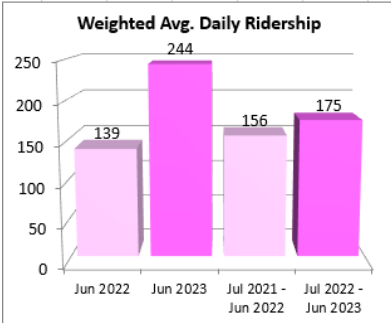
## RTC RIDE



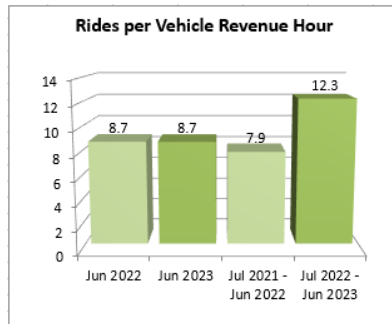
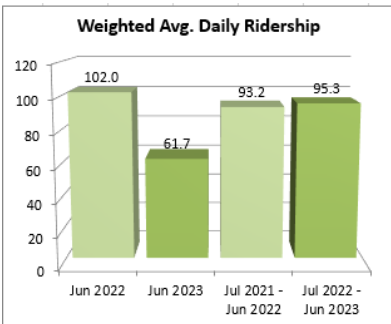
## RTC ACCESS



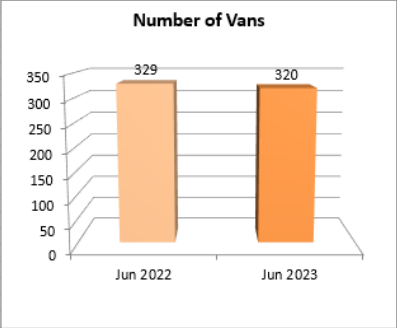
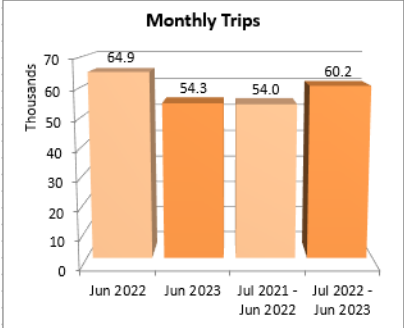
## RTC FlexRIDE



## TART



# RTC VANPOOL





**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.2.6

To: Regional Transportation Commission

From: Paul Nelson, Government Affairs Officer

**SUBJECT: Community Outreach Report**

**RECOMMENDED ACTION**

Acknowledge receipt of the monthly Outreach Activities Report.

**BACKGROUND AND DISCUSSION**

Each month, the RTC Communications staff compiles a list of outreach activities for all RTC departments. This report covers the month of July 2023.

**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

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## COMMUNITY AND MEDIA OUTREACH ACTIVITIES

<b>Outreach Activities</b>
Paul Nelson, Project Manager
<i>Status: RTC staff conducted the following outreach activities from June 1-June 30</i> <i>July 4 – Star Spangled Sparks Fireworks Event/Free Transit</i> <i>July 5 – RTC Citizens Multimodal Advisory Committee Meeting</i> <i>July 6 – RTC Technical Advisory Committee</i> <i>July 12 – Reno Sparks Indian Colony Tribal Council Meeting</i> <i>July 25 – Micheladas de Reno, Reno Aces</i>
<b>Media Relations &amp; Social Media</b>
Paul Nelson, Project Manager
<i>Status: The RTC issued eight news releases and received four media inquiries regarding BRT Digital Boards, RTIP Public Comment Period, the Midtown Roundabout Property Dispute, and the Midtown Median.</i>  <i>Social media was used to promote and provide information about Sutro Street/9<sup>th</sup> Street intersection closure, community feedback about the Regional Connector, the Arrowcreek Parkway Rehabilitation Project, Construction Completion on Sutro Street, Hard Closure and detour of 4<sup>th</sup> Street in Sparks, Public Input of the FY23-27 RTIP, Detour of California Avenue, Oddie Wells Project Detour, Zolezzi Lane Closure at Arrowcreek Parkway, Free Transportation on RAPID Virginia and Lincoln Lines, Route 1 and Route 11 for Hot August Nights, and Medicaid Coverage Renewal.</i>  <i>Social media metrics for the month of July: 13,382 impressions on Facebook, Twitter, YouTube, and Instagram.</i>
<b>Informational Materials and Video Production</b>
Paul Nelson, Project Manager
<i>Status: Four topics were broadcast on KOLO-TV for The Road Ahead with RTC. Segments included information about the Midtown’s Dancing in the Streets Event, Arrowcreek Parkway Project, Micheladas de Reno, and the Holcomb Avenue Project.</i>



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.3.1

To: Regional Transportation Commission

From: Marquis Williams, Senior Technical Planner

**SUBJECT: Regional Freight Plan**

**RECOMMENDED ACTION**

Approve a contract with Parametrix, Inc., for consulting services on the Regional Freight Plan, in an amount not-to-exceed \$149,296.14.

**BACKGROUND AND DISCUSSION**

The purpose of the Regional Freight Plan is to provide a detailed understanding of the freight network and goods movements patterns in the Reno-Sparks area and to develop a corresponding needs assessment and prioritization process. The Plan will build upon the Nevada Statewide Freight Plan completed by the Nevada Department of Transportation (NDOT) in 2022.

Proposals were received in response to the Request for Proposals of the Regional Freight Plan. The technical evaluation of the proposals was conducted by an Evaluation Committee, composed of three individuals from the RTC and NDOT. All members of the Evaluation Committee evaluated and scored the technical proposals independently in accordance with the evaluation criteria set forth in the solicitation. Based on final scoring and discussion, the proposal from Parametrix, Inc., received the highest weighted score and is being recommended for award of the PSA.

**FISCAL IMPACT**

Funding for this item has been budgeted for in the FY 2024 – FY 2025 Unified Planning Work Program (UPWP).

**PREVIOUS BOARD ACTION**

4/21/2023      Approved the FY 2024 – FY 2025 UPWP.

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**AGREEMENT  
FOR  
PROFESSIONAL SERVICES**

This agreement (this “Agreement”) is dated and effective as of \_\_\_\_\_, 2023, by and between the Regional Transportation Commission of Washoe County (“RTC”) and Parametrix, Inc. (“CONSULTANT”).

WITNESSETH:

[WHEREAS, RTC issued a Request for Proposals for interested persons and firms to perform a study in connection with the Regional Freight Plan; and

WHEREAS, CONSULTANT submitted a proposal (the “Proposal”) and was selected to perform the study.

NOW, THEREFORE, RTC and CONSULTANT, in consideration of the mutual covenants and other consideration set forth herein, do hereby agree as follows:

ARTICLE 1 – TERM AND ENGAGEMENT

- 1.1. The term of this Agreement shall be from the date first written above through February 29, 2024, unless terminated at an earlier date, or extended to a later date, pursuant to the provisions herein.
- 1.2. CONSULTANT will perform the work using the project team identified in the Proposal. Any changes to the project team must be approved by RTC’s Project Manager.
- 1.3. CONSULTANT will promptly, diligently and faithfully execute the work to completion in accordance with applicable professional standards subject to any delays due to strikes, acts of God, act of any government, civil disturbances, or any other cause beyond the reasonable control of CONSULTANT.
- 1.4. CONSULTANT shall not proceed with work until both parties have executed this Agreement and a purchase order has been issued to CONSULTANT. If CONSULTANT violates that prohibition, CONSULTANT forfeits any and all right to reimbursement and payment for that work and waives any and all claims against RTC, its employees, agents, and affiliates, including but not limited to monetary damages, and any other remedy available at law or in equity arising under the terms of this Agreement. Furthermore, prior to execution and issuance of a purchase order, CONSULTANT shall not rely on the terms of this Agreement in any way, including but not limited to any written or oral representations, assurances or warranties made by RTC or any of its agents, employees or affiliates, or on any dates of performance, deadlines, indemnities, or any term contained in this Agreement or otherwise.

## ARTICLE 2 - SERVICES OF CONSULTANT

### 2.1. SCOPE OF SERVICES

The scope of services consist of the tasks set forth in Exhibit A.

### 2.2. SCHEDULE OF SERVICES

Tasks and subtasks shall be completed in accordance with the schedule in Exhibit A. Any change(s) to the schedule must be approved by RTC's Project Manager.

### 2.3. CONTINGENCY

Contingency line items identified in the scope of services are for miscellaneous increases within the scope of work. Prior to the use of any contingency amounts, CONSULTANT shall provide a letter to RTC's Project Manager detailing the need, scope, and not-to-exceed budget for the proposed work. Work to be paid for out of contingency shall proceed only with the RTC Project Manager's written approval.

### 2.4. OPTIONS

RTC shall have the right to exercise its option(s) for all or any part of the optional tasks or subtasks identified in Exhibit A. CONSULTANT will prepare and submit a detailed scope of services reflecting the specific optional services requested, a schedule for such services, and a cost proposal. RTC will review and approve the scope of services and RTC and CONSULTANT will discuss and agree upon compensation and a schedule. CONSULTANT shall undertake no work on any optional task without written notice to proceed with the performance of said task. RTC, at its sole option and discretion, may select another individual or firm to perform the optional tasks or subtasks identified in Exhibit A.

### 2.5. ADDITIONAL SERVICES

CONSULTANT will provide additional services when agreed to in writing by RTC and CONSULTANT.

### 2.6. PERFORMANCE REQUIREMENTS

Any and all design and engineering work furnished by CONSULTANT shall be performed by or under the supervision of persons licensed to practice architecture, engineering, or surveying (as applicable) in the State of Nevada, by personnel who are careful, skilled, experienced and competent in their respective trades or professions, who are professionally qualified to perform the work, and who shall assume professional responsibility for the accuracy and completeness of documents prepared or checked by them, in accordance with appropriate prevailing professional standards. Notwithstanding the provision of any drawings, technical specifications, or other data by RTC, CONSULTANT shall have the

responsibility of supplying all items and details required for the deliverables required hereunder.

Any sampling and materials testing shall be performed by an approved testing laboratory accredited by AASHTO or other ASTM recognized accrediting organization in the applicable test methods. If any geotechnical or materials testing is performed by a sub-consultant, that laboratory shall maintain the required certification. Proof of certification shall be provided to RTC with this Agreement. If certification expires or is removed during the term of this Agreement, CONSULTANT shall notify RTC immediately, and propose a remedy. If an acceptable remedy cannot be agreed upon by both parties, RTC may terminate this Agreement for default.

CONSULTANT shall provide only Nevada Alliance for Quality Transportation Construction (NAQTC) qualified personnel to perform field and laboratory sampling and testing during the term of this Agreement. All test reports shall be signed by a licensed NAQTC tester and notated with his/her license number.

2.7. ERRORS AND OMISSIONS

CONSULTANT shall, without additional compensation, correct or revise any deficiencies, errors, or omissions caused by CONSULTANT in its analysis, reports, and services. CONSULTANT also agrees that if any error or omission is found, CONSULTANT will expeditiously make the necessary correction, at no expense to RTC. If an error or omission was directly caused by RTC, and not by CONSULTANT and RTC requires that such error or omission be corrected, CONSULTANT may be compensated for such additional work.

ARTICLE 3 - COMPENSATION

3.1. CONSULTANT shall be paid for hours worked at the hourly rates and rates for testing in Exhibit B. RTC shall not be responsible for any other costs or expenses except as provided in Exhibit B.

3.2. The maximum amount payable to CONSULTANT to complete each task is equal to the not-to-exceed amounts identified in Exhibit B. CONSULTANT can request in writing that RTC's Project Manager reallocate not-to-exceed amounts between tasks. A request to reallocate not-to-exceed amounts must be accompanied with a revised fee schedule, and must be approved in writing by RTC's Project Manager prior to performance of the work. In no case shall CONSULTANT be compensated in excess of the following not-to exceed amounts:

Total Services (Tasks 1 to 9)	\$149,296.14
<hr/>	
Total Not-to-Exceed Amount	\$149,296.14

3.3. For any work authorized under Section 2.5, "Additional Services," RTC and CONSULTANT will negotiate not-to-exceed amounts based on the standard hourly rates

and rates for testing in Exhibit B. Any work authorized under Section 2.5, "Additional Services," when performed by persons who are not employees or individuals employed by affiliates of CONSULTANT, will be billed at a mutually agreed upon rate for such services, but not more than 105% of the amounts billed to CONSULTANT for such services.

- 3.4. CONSULTANT shall receive compensation for preparing for and/or appearing in any litigation at the request of RTC, except: (1) if such litigation costs are incurred by CONSULTANT in defending its work or services or those of any of its sub-consultants; or (2) as may be required by CONSULTANT's indemnification obligations. Compensation for litigation services requested by RTC shall be paid at a mutually agreed upon rate and/or at a reasonable rate for such services.

#### ARTICLE 4 - INVOICING

- 4.1. CONSULTANT shall submit monthly invoices in the format specified by RTC. Invoices must be submitted to [accountspayable@rtcwashoe.com](mailto:accountspayable@rtcwashoe.com). RTC's payment terms are 30 days after the receipt of the invoice. Simple interest will be paid at the rate of half a percent (0.5%) per month on all invoices approved by RTC that are not paid within thirty (30) days of receipt of the invoice.
- 4.2. RTC shall notify CONSULTANT of any disagreement with any submitted invoice for consulting services within thirty (30) days of receipt of an invoice. Any amounts not in dispute shall be promptly paid by RTC.
- 4.3. CONSULTANT shall maintain complete records supporting every request for payment that may become due. Upon request, CONSULTANT shall produce all or a portion of its records and RTC shall have the right to inspect and copy such records.

#### ARTICLE 5 - ACCESS TO INFORMATION AND PROPERTY

- 5.1. Upon request and without cost to CONSULTANT, RTC will provide all pertinent information that is reasonably available to RTC including surveys, reports and any other data relative to design and construction.
- 5.2. RTC will provide access to and make all provisions for CONSULTANT to enter upon RTC facilities and public lands, as required for CONSULTANT to perform its work under this Agreement.

#### ARTICLE 6 - OWNERSHIP OF WORK

- 6.1. Plans, reports, studies, tracings, maps, software, electronic files, licenses, programs, equipment manuals, and databases and other documents or instruments of service prepared or obtained by CONSULTANT in the course of performing work under this Agreement, shall be delivered to and become the property of RTC. Software already developed and purchased by CONSULTANT prior to the Agreement is excluded from this requirement.

CONSULTANT and its sub-consultants shall convey and transfer all copyrightable interests, trademarks, licenses, and other intellectual property rights in such materials to RTC upon completion of all services under this Agreement and upon payment in full of all compensation due to CONSULTANT in accordance with the terms of this Agreement. Basic survey notes, sketches, charts, computations and similar data prepared or obtained by CONSULTANT under this Agreement shall, upon request, also be provided to RTC.

- 6.2. CONSULTANT represents that it has secured all necessary licenses, consents, or approvals to use the components of any intellectual property, including computer software, used in providing services under this Agreement, that it has full legal title to and the right to reproduce such materials, and that it has the right to convey such title and other necessary rights and interests to RTC.
- 6.3. CONSULTANT shall bear all costs arising from the use of patented, copyrighted, trade secret, or trademarked materials, equipment, devices, or processes used on or incorporated in the services and materials produced under this Agreement.
- 6.4. CONSULTANT agrees that all reports, communications, electronic files, databases, documents, and information that it obtains or prepares in connection with performing this Agreement shall be treated as confidential material and shall not be released or published without the prior written consent of RTC; provided, however, that CONSULTANT may refer to this scope of work in connection with its promotional literature in a professional and commercially reasonable manner. The provisions of this subsection shall not apply to information in whatever form that comes into the public domain. The provisions of this paragraph also shall not restrict CONSULTANT from giving notices required by law or complying with an order to provide information or data when such order is issued by a court, administrative agency, or other entity with proper jurisdiction, or if it is reasonably necessary for CONSULTANT to defend itself from any suit or claim.

## ARTICLE 7 - TERMINATION

### 7.1. CONTRACT TERMINATION FOR DEFAULT

If CONSULTANT fails to perform services in the manner called for in this Agreement or if CONSULTANT fails to comply with any other provisions of this Agreement, RTC may terminate this Agreement for default. Termination shall be effected by serving a notice of termination on CONSULTANT setting forth the manner in which CONSULTANT is in default. CONSULTANT will only be paid the contract price for services delivered and accepted, or services performed in accordance with the manner of performance set forth in this Agreement.

If it is later determined by RTC that CONSULTANT had an excusable reason for not performing, such as a fire, flood, or events which are not the fault of or are beyond the control of CONSULTANT, RTC, after setting up a new performance schedule, may allow CONSULTANT to continue work, or treat the termination as a termination for convenience.

7.2. CONTRACT TERMINATION FOR CONVENIENCE

RTC may terminate this Agreement, in whole or in part, at any time by written notice to CONSULTANT when it is in RTC's best interest. CONSULTANT shall be paid its costs, including contract closeout costs, and profit on work performed up to the time of termination. CONSULTANT shall promptly submit its termination claim to RTC to be paid CONSULTANT. If CONSULTANT has any property in its possession belonging to RTC, CONSULTANT will account for the same, and dispose of it in the manner RTC directs.

ARTICLE 8 - INSURANCE

- 8.1. CONSULTANT shall not commence any work or permit any employee/agent to commence any work until satisfactory proof has been submitted to RTC that all insurance requirements have been met.
- 8.2. In conjunction with the performance of the services/work required by the terms of this Agreement, CONSULTANT shall obtain all types and amounts of insurance set forth in Exhibit C, and shall comply with all provisions set forth therein.

ARTICLE 9 - HOLD HARMLESS

- 9.1. CONSULTANT's obligation under this provision is as set forth in Exhibit C. Said obligation would also extend to any liability of RTC resulting from any action to clear any lien and/or to recover for damage to RTC property.

ARTICLE 10 - EQUAL EMPLOYMENT OPPORTUNITY

- 10.1. During the performance of this Agreement, CONSULTANT agrees not to discriminate against any employee or applicant for employment because of race, color, religion, sex, age, disability, or national origin. CONSULTANT will take affirmative action to ensure that applicants are employed, and that employees are treated fairly during employment, without regard to their race, color, religion, sex, age, disability, or national origin. Such action shall include, but not be limited to, the following: employment, upgrading, demotion, or transfer; recruitment or recruitment advertising; layoff or termination; rates of pay or other forms of compensation; and selection for training, including apprenticeship. CONSULTANT agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided by RTC setting forth the provisions of this nondiscrimination clause.
- 10.2. CONSULTANT will, in all solicitations or advertisements for employees placed by or on behalf of CONSULTANT, state that well qualified applicants will receive consideration of employment without regard to race, color, religion, sex, age, disability, or national origin.

- 10.3. CONSULTANT will cause the foregoing provisions to be inserted in all sub-agreements for any work covered by this Agreement so that such provisions will be binding upon each sub-consultant.

## ARTICLE 11 - RESOLUTION OF CLAIMS AND DISPUTES

### 11.1. NEGOTIATED RESOLUTION

In the event that any dispute or claim arises under this Agreement, the parties shall timely cooperate and negotiate in good faith to resolve any such dispute or claim. Such cooperation shall include providing the other party with all information in order to properly evaluate the dispute or claim and making available the necessary personnel to discuss and make decisions relative to the dispute or claim.

### 11.2. MEDIATION

If the parties have been unable to reach an informal negotiated resolution to the dispute or claim within thirty (30) days following submission in writing of the dispute or claim to the other party, or such longer period of time as the parties may agree to in writing, either party may then request, in writing, that the dispute or claim be submitted to mediation (the "Mediation Notice"). After the other party's receipt or deemed receipt of the Mediation Notice, the parties shall endeavor to agree upon a mutually acceptable mediator, but if the parties have been unable to agree upon a mediator within ten (10) days following receipt of the Mediation Notice, then each party shall select a mediator and those two selected mediators shall select the mediator. A mediator selected by the parties' designated mediators shall meet the qualification set forth in as provided in Rule 4 of Part C., "Nevada Mediation Rules" of the "Rules Governing Alternative Dispute Resolutions adopted by the Nevada Supreme Court." Unless otherwise agreed to by the parties, in writing, the mediator shall have complete discretion over the conduct of the mediation proceeding. Unless otherwise agreed to by the parties, in writing, the mediation proceeding must take place within thirty (30) days following appointment of the mediator. The parties shall share the mediator's fee and any filing fees equally. The mediation shall be held in Washoe County, Nevada, unless otherwise agreed to by the parties, in writing. Agreements reached in mediation shall be enforceable as settlement agreements in any court having jurisdiction thereof.

### 11.3. LITIGATION

In the event that the parties are unable to settle and/or resolve the dispute or claim as provided above, then either party may proceed with litigation in the Second Judicial District Court of the State of Nevada, County of Washoe.

11.4. CONTINUING CONTRACT PERFORMANCE

During the pendency of any dispute or claim the parties shall proceed diligently with performance of this Agreement and such dispute or claim shall not constitute an excuse or defense for a party's nonperformance or delay.

ARTICLE 12 – PROJECT MANAGERS

- 12.1. RTC's Project Manager is Marquis Williams or such other person as is later designated in writing by RTC. RTC's Project Manager has authority to act as RTC's representative with respect to the performance of this Agreement.
- 12.2. CONSULTANT' Project Manager is Amy Cummings, AICP, LEED AP or such other person as is later designated in writing by CONSULTANT. CONSULTANT's Project Manager has authority to act as CONSULTANT's representative with respect to the performance of this Agreement.

ARTICLE 13 - NOTICE

- 13.1. Notices required under this Agreement shall be given as follows:

RTC: Bill Thomas, AICP  
Executive Director  
Marquis Williams  
RTC Project Manager  
Regional Transportation Commission  
1105 Terminal Way  
Reno, Nevada 89502  
Email:  
(775) 332-0174

CONSULTANT: Amy Cummings, AICP, LEED AP  
Project Manager  
Parametrix, Inc.  
9190 Double Diamond Parkway  
Reno, NV 89521  
acummings@parametrix.com  
(725) 240-1902

ARTICLE 14 - DELAYS IN PERFORMANCE

- 14.1. TIME IS OF THE ESSENCE

It is understood and agreed that all times stated and referred to herein are of the essence. The period for performance may be extended by RTC's Executive Director pursuant to the



process specified herein. No extension of time shall be valid unless reduced to writing and signed by RTC's Executive Director.

14.2. UNAVOIDABLE DELAYS

If the timely completion of the services under this Agreement should be unavoidably delayed, RTC may extend the time for completion of this Agreement for not less than the number of days CONSULTANT was excusably delayed. A delay is unavoidable only if the delay is not reasonably expected to occur in connection with or during CONSULTANT's performance, is not caused directly or substantially by acts, omissions, negligence or mistakes of CONSULTANT, is substantial and in fact causes CONSULTANT to miss specified completion dates, and cannot adequately be guarded against by contractual or legal means.

14.3. NOTIFICATION OF DELAYS

CONSULTANT shall notify RTC as soon as CONSULTANT has knowledge that an event has occurred or otherwise becomes aware that CONSULTANT will be delayed in the completion of the work. Within ten (10) working days thereafter, CONSULTANT shall provide such notice to RTC, in writing, furnishing as much detail on the delay as possible and requesting an extension of time.

14.4. REQUEST FOR EXTENSION

Any request by CONSULTANT for an extension of time to complete the work under this Agreement shall be made in writing to RTC. CONSULTANT shall supply to RTC documentation to substantiate and justify the additional time needed to complete the work and shall provide a revised schedule. RTC shall provide CONSULTANT with notice of its decision within a reasonable time after receipt of a request.

ARTICLE 15 - GENERAL PROVISIONS

15.1. SUCCESSORS AND ASSIGNS

RTC and CONSULTANT bind themselves and their successors and assigns to the other party and to the successors and assigns of such party, with respect to the performance of all covenants of this Agreement. Except as set forth herein, neither RTC nor CONSULTANT shall assign or transfer interest in this Agreement without the written consent of the other. Nothing herein shall be construed as creating a personal liability on the part of any officer or agent or any public body which may be a party hereto, nor shall it be construed as giving any rights or benefits hereunder to anyone other than RTC and CONSULTANT.

15.2. NON TRANSFERABILITY

This Agreement is for CONSULTANT's professional services, and CONSULTANT's rights and obligations hereunder may not be assigned without the prior written consent of RTC.

15.3. SEVERABILITY

If any part, term, article, or provision of this Agreement is, by a court of competent jurisdiction, held to be illegal, void, or unenforceable, or to be in conflict with any law of the State of Nevada, the validity of the remaining provisions or portions of this Agreement are not affected, and the rights and obligations of the parties shall be construed and enforced as if this Agreement did not contain the particular part, term, or provision held invalid.

15.4. RELATIONSHIP OF PARTIES

CONSULTANT is an independent contractor to RTC under this Agreement. Accordingly, CONSULTANT is not entitled to participate in any retirement, deferred compensation, health insurance plans or other benefits RTC provides to its employees. CONSULTANT shall be free to contract to provide similar services for others while it is under contract to RTC, so long as said services and advocacy are not in direct conflict, as determined by RTC, with services being provided by CONSULTANT to RTC.

15.5. WAIVER/BREACH

Any waiver or breach of a provision in this Agreement shall not be deemed a waiver of any other provision in this Agreement and no waiver is valid unless in writing and executed by the waiving party. An extension of the time for performance of any obligation or act shall not be deemed an extension of time for the performance of any other obligation or act. This Agreement inures to the benefit of and is binding upon the parties to this Agreement and their respective heirs, successors and assigns.

15.6. REGULATORY COMPLIANCE

A. CONSULTANT shall comply with all applicable federal, state and local government laws, regulations and ordinances. CONSULTANT shall be responsible for obtaining all necessary permits and licenses for performance of services under this Agreement. Upon request of RTC, CONSULTANT shall furnish RTC certificates of compliance with all such laws, orders and regulations.

B. CONSULTANT represents and warrants that none of the services to be rendered pursuant to this Agreement constitute the performance of public work, as that term is defined by Section 338.010(17) of the Nevada Revised Statutes. To the extent CONSULTANT does engage in such public work, CONSULTANT shall be responsible for paying the prevailing wage as required by Chapter 338 of the Nevada Revised Statutes.

15.7. EXCLUSIVE AGREEMENT

There are no verbal agreements, representations or understandings affecting this Agreement, and all negotiations, representations and undertakings are set forth herein with the understanding that this Agreement constitutes the entire understanding by and between the parties.

15.8. AMENDMENTS

No alteration, amendment or modification of this Agreement shall be effective unless it is in writing and signed by both parties.

15.9. CONTINUING OBLIGATION

CONSULTANT agrees that if, because of death or any other occurrence it becomes impossible for any principal or employee of CONSULTANT to render the services required under this Agreement, neither CONSULTANT nor the surviving principals shall be relieved of any obligation to render complete performance. However, in such event, RTC may terminate this Agreement if it considers the death or incapacity of such principal or employee to be a loss of such magnitude as to affect CONSULTANT's ability to satisfactorily complete the performance of this Agreement.

15.10. APPLICABLE LAW AND VENUE

The provisions of this Agreement shall be governed and construed in accordance with the laws of the State of Nevada. The exclusive venue and court for all lawsuits concerning this Agreement shall be the Second Judicial District Court of the State of Nevada, County of Washoe, and the parties hereto submit to the jurisdiction of that District Court.

15.11. ATTORNEYS' FEES

In the event of a dispute between the parties result in a proceeding in any Court of Nevada having jurisdiction, the prevailing party shall be entitled to an award of costs and any reasonable attorneys' fees.

15.12. CERTIFICATION REQUIRED BY NEVADA SENATE BILL 27 (2017)

CONSULTANT expressly certifies and agrees, as a material part of this Agreement, that it is not currently engaged in a boycott of Israel. CONSULTANT further agrees, as a material part of this Agreement, it will not engage in a boycott of Israel for the duration of this Agreement. If, at any time during the formation or duration of this Agreement, CONSULTANT is engaged or engages in a boycott of Israel, it will constitute a material breach of this Agreement.

\*\*\*ARTICLE 16 - FEDERAL FORMS AND CLAUSES\*\*\*

- 16.1. CONSULTANT has completed and signed the following: (1) Affidavit of Non-Collusion; (2) Certification Regarding Debarment, Suspension, Other Ineligibility and Voluntary Exclusion; (3) Certification Required by 31 U.S.C. § 1352, Restrictions on Lobbying Using Federal Appropriated Funds, and “Instructions for Completion of SF-LLL, Disclosure of Lobbying Activities”. CONSULTANT affirms that such certifications remain valid and shall immediately notify RTC if circumstances change that affect the validity of these certifications.
- 16.2. This Agreement is funded in whole or in part with money administered by the Nevada Department of Transportation on behalf of the Federal Highway Administration. As a condition for receiving payment under this Agreement, CONSULTANT agrees to comply with the federally required clauses set forth in Exhibits D, E and F.

IN WITNESS WHEREOF, the parties hereto have made and executed this Agreement the day and year first above written.

REGIONAL TRANSPORTATION COMMISSION  
OF WASHOE COUNTY

By: \_\_\_\_\_  
Bill Thomas, AICP, Executive Director

Parametrix, Inc.

By: \_\_\_\_\_  
Amy Cummings, AICP, LEED AP, Project Manager

**Attachment A**  
**Scope of Work**  
**RTC Regional Freight Study**

**Objective**

The Regional Transportation Commission of Washoe County (RTC) is seeking proposals to conduct a Regional Freight Study. The Reno-Sparks area has experienced rapid economic and population growth over the past decade, and the trend is expected to continue. The county boasts a dynamic economic landscape driven by diverse industries, including technology, logistics, manufacturing, and tourism. This growth has resulted in an increased need for a reliable and efficient freight transportation system tailored to the region's unique requirements. Leveraging the groundwork laid by the 2017 Nevada Department of Transportation (NDOT) State Freight Plan, the RTC Regional Freight Plan will offer a regional perspective addressing the area's specific freight transportation needs and challenges. This plan will concentrate on improving safety, efficiency, and sustainability by documenting the existing conditions from a freight perspective and identifying goals, strategies, policies, and projects focused on improving goods movement in Washoe County.

This comprehensive study will document current conditions related to goods movement that can be used to pursue funding opportunities. Additionally, it is anticipated that the Plan will support the pending long-range Regional Transportation Plan (RTP) update leading to cost-effective, comprehensive decision-making. The concurrent development of the RTP update should ensure a unified approach to addressing transportation challenges in the area.

The proposed work tasks outlined below are aimed at achieving the goal of developing a comprehensive regional freight plan. The contractor may suggest alternative approaches that deviate from these tasks if they can demonstrate that their proposed approaches can produce results of equal or superior quality.

**Project Tasks**

**1.0 Project Management**

- 1.1 Invoicing and Progress Reports – Prepare monthly progress reports, invoices, and resolve other billing and logistical issues.
- 1.2 Coordination – An initial meeting with RTC Washoe staff will be organized to establish study objectives and goals. Coordination with the RTC project management team will be ongoing throughout the project. It is anticipated that the consultant will agendize and record monthly scheduled meetings with the project management team. These meetings may be conducted virtually.
- 1.3 Management Plan – Identify a team organizational structure and anticipated steps and processes required to complete the study. This will include a project schedule and budget for each task with corresponding deliverables and progress reports.

Deliverables:

- Invoices and progress reports
- Meeting agendas and meeting minutes
- Management Plan

## **2.0 Existing Conditions Analysis**

- 2.1 Use existing data, model outputs, plans, and studies along with consultation from government staff and freight industry stakeholders to identify and map economically significant freight patterns, movements, and origins/destinations (domestic and international) across all modes. Identify the categories of goods and their corresponding characteristics (hazardous, perishable, etc.). Summarize all commodity flow data by tonnage and by dollar value.
- 2.2 Identify and map economically significant freight infrastructure such as roads, bridges, intermodal connectors, storage and transfer facilities, railroads, airports, etc. using existing map data. Rank roads into three tiers based on importance to the overall freight network.

Deliverables:

- Tables, maps, and narratives summarizing the freight movements, commodities, and infrastructure in Washoe County.
- Lists containing most significant trade partners, freight corridors (by tier), and categories of commodities with brief descriptions of each item.

## **3.0 Public and Agency Involvement**

- 3.1 Work with RTC staff to develop and maintain a stakeholder list comprised of representatives from governments, economic development groups, trucking companies, distribution companies, railroad operators, military representatives, and relevant research groups. Host up to four (4) engagement sessions with the stakeholder group to solicit input, ideas, perceptions, and concerns and to identify key issues related to freight and goods movement in the region. Meetings can be held virtually or in person.
- 3.2 Develop, distribute, and collect surveys from the public concerning freight issues. The intent of these surveys is to identify recurring issues and concerns within the network and support the final recommendations of this Plan. Work with RTC staff to distribute materials to the public through standard procedure.

Deliverables:

- List of parties who were contacted and who participated in the stakeholder feedback group.
- Summary of input received both from the stakeholder feedback group and from the public survey.

#### **4.0 Freight Operations and Performance**

- 4.1 Describe key operational aspects of the region’s freight system, including but not limited to emergency and road closure procedures, alternate routing, communication systems, “last mile” considerations, and other operational features.
- 4.2 Using available data, identify inefficiencies specific to goods movement such as persistent bottlenecks, pavement maintenance concerns, areas with high crash rates, etc.
- 4.3 Review present and future “last mile” considerations, as well as examples of best practices appropriate for the region. The “last mile problem” contributes significantly to congestion and safety problems in urban areas, and can account for a large portion of total shipping costs.
- 4.4 Analyze and discuss the current state of urban parking for freight vehicles within the County, identifying existing parking facilities, areas with high demand for freight parking, and constraints regarding the provision of that parking, within the context of both normal operations and non-recurring events such as road closures and delays due to crashes, inclement weather, etc. Provide recommendations for potential parking locations, high-level cost estimates, and steps for implementation.

Deliverables:

- Description (including maps) of existing operational conditions and inefficiencies as described above.
- Report detailing “last mile” considerations along with best practices suited for Washoe County. This report should address urban parking for freight vehicles, among other issues.

#### **5.0 Strengths, Weaknesses, Opportunities, Threats (SWOT) Analysis**

- 5.1 Competitive economic, logistical, strategic, and other advantages of Washoe County from a freight perspective.
- 5.2 Competitive weaknesses/disadvantages of Washoe County from a freight perspective.
- 5.3 Strategies that could be applied to capitalize on freight-related strengths.
- 5.4 Threats to increased development of freight activity in the County. (Population stagnation, traffic, infrastructure, etc.).

Deliverables:

- Analysis of the strengths, weaknesses, opportunities, and threats from a freight perspective in Washoe County.

#### **6.0 Focus Projects and Corridors**

- 6.1 List and map all current project pursuits as identified in the RTP that will have a significant impact on freight or that could help improve freight connectivity and efficiency.

- 6.2 Identify, list, and map corridors for improvement within the region, categorizing corridors into three levels based on priority. Priority should be determined using a combination of stakeholder data, SWOT analysis, and existing conditions identified in 1.0.
- 6.3 Provide a list of the most applicable funding opportunities for the projects identified and for the types of projects likely to be most advantageous based on the results of this study.

Deliverables:

- List of freight-related RTP projects.
- List of significant freight corridors with priority level; description of prioritization methods used.
- Relevant funding opportunities to be potentially explored by RTC Staff.

## **7.0 Future of Urban Freight**

- 7.1 Produce a technical memo summarizing the macro trends in freight as they relate to the County's transportation system. This memo will describe new and developing technologies and trends such as automation, e-commerce, hydrogen, and micro-freight.

Deliverables:

- Technical memo as outlined in 7.1.

## **8.0 Recommendations**

- 8.1 Truck Parking Toolbox and memo with site-specific recommendations.
- 8.2 Strategic Growth/Development Guidelines.
- 8.3 Comprehensive map summarizing current conditions/concerns.
  - 8.3.1 Bottlenecks.
  - 8.3.2 Critical routes.
  - 8.3.3 Pavement concerns.
  - 8.3.4 Safety concerns.
- 8.4 Human element
  - 8.4.1 Transit needs of warehouse workers.
  - 8.4.2 Perspective of truckers and others with hands-on experience in region.
- 8.5 Potential freight-related project recommendations and policies not already identified in existing plans.

## **9.0 Report Preparation**

- 9.1 Admin Draft – Outline of all deliverables with maps and tables for staff and TAC review.
- 9.2 Public Draft – Near-final draft for review by RTC Board and public.
- 9.3 Final Draft – Final draft incorporating Board/public comments.



**Regional Freight Plan - RTC Washoe  
Estimated Budget Detail**

<b>Parametrix</b>
Regional Freight Plan

Parametrix									
Amy Cummings	Bardia Nezhati	Jennifer Valentine	Jackie Kuechenmeister	Eric Nielsen	Heather McLaughlin-Kolb	Amir Almotahari	Jeff Hale	Marriah McCreery	

Task	SubTask	Description	Labor Dollars	Labor Hours	Amy Cummings	Bardia Nezhati	Jennifer Valentine	Jackie Kuechenmeister	Eric Nielsen	Heather McLaughlin-Kolb	Amir Almotahari	Jeff Hale	Marriah McCreery	
<b>01</b>		<b>Project Management</b>	<b>\$14,356.26</b>	<b>72</b>	<b>28</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10</b>	<b>0</b>	<b>10</b>	<b>0</b>	<b>24</b>	<b>\$14,356.26</b>
	01	Project Management Meetings	\$8,082.50	38	18				10		10			
	02	Monthly Progress Reports	\$6,273.76	34	10								24	
<b>02</b>		<b>Existing Conditions Analysis</b>	<b>\$15,441.40</b>	<b>108</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>40</b>	<b>0</b>	<b>60</b>	<b>0</b>	<b>0</b>	<b>\$15,441.40</b>
	01	Existing Conditions	\$15,441.40	108	8				40		60			
<b>03</b>		<b>Public and Agency Involvement</b>	<b>\$17,285.36</b>	<b>80</b>	<b>40</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>24</b>	<b>8</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>\$17,285.36</b>
	01	Public and Agency Involvement	\$17,285.36	80	40				24	8	8			
<b>04</b>		<b>Freight Operations and Performance</b>	<b>\$0.00</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>\$0.00</b>
	01	Performance Analysis	\$0.00	0										
<b>05</b>		<b>SWOT Analysis</b>	<b>\$3,600.08</b>	<b>16</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>\$3,600.08</b>
	01	SWOT Analysis	\$3,600.08	16	8						8			
<b>06</b>		<b>Focus Projects and Corridors</b>	<b>\$13,829.44</b>	<b>72</b>	<b>16</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>16</b>	<b>0</b>	<b>32</b>	<b>8</b>	<b>0</b>	<b>\$13,829.44</b>
	01	Develop Corridor and Project Priorities	\$13,829.44	72	16				16		32	8		
<b>07</b>		<b>Future of Urban Freight</b>	<b>\$2,885.36</b>	<b>18</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>16</b>	<b>0</b>	<b>0</b>	<b>\$2,885.36</b>
	01	Develop Future of Freight Report	\$2,885.36	18	2						16			
<b>08</b>		<b>Recommendations</b>	<b>\$8,299.76</b>	<b>36</b>	<b>12</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>8</b>	<b>8</b>	<b>0</b>	<b>\$8,299.76</b>
	01	Develop Recommendations	\$8,299.76	36	12				8		8	8		
<b>09</b>		<b>Report Preparation</b>	<b>\$20,098.48</b>	<b>112</b>	<b>16</b>	<b>4</b>	<b>8</b>	<b>4</b>	<b>16</b>	<b>40</b>	<b>24</b>	<b>0</b>	<b>0</b>	<b>\$20,098.48</b>
	01	Report Preparation	\$20,098.48	112	16	4	8	4	16	40	24			
		<b>Total:</b>	<b>\$95,796.14</b>	<b>514</b>	<b>130</b>	<b>4</b>	<b>8</b>	<b>4</b>	<b>114</b>	<b>48</b>	<b>166</b>	<b>16</b>	<b>24</b>	<b>\$95,796.14</b>

Total by Company (base tasks only)	Total Hours	Labor Total	Expense Total	Total	%
Parametrix	514	\$95,796	\$3,500	\$99,296	67%
Cambridge Systematics				\$50,000	33%
<b>Base Tasks Total:</b>	<b>514</b>	<b>\$95,796</b>	<b>\$3,500</b>	<b>\$149,296</b>	
<b>Estimated Total Budget:</b>		<b>\$149,296.14</b>			

## ATTACHMENT C

### **INDEMNIFICATION AND INSURANCE REQUIREMENTS FOR PROFESSIONAL SERVICE AGREEMENTS [NRS 338 DESIGN PROFESSIONAL]**

#### **1. INTRODUCTION**

IT IS HIGHLY RECOMMENDED THAT CONSULTANTS CONFER WITH THEIR INSURANCE CARRIERS OR BROKERS TO DETERMINE THE AVAILABILITY OF THESE INSURANCE CERTIFICATES AND ENDORSEMENTS IN ADVANCE OF PROPOSAL SUBMISSION. IF THERE ARE ANY QUESTIONS REGARDING THESE INSURANCE REQUIREMENTS, IT IS RECOMMENDED THAT THE AGENT/BROKER CONTACT RTC'S FINANCE DIRECTOR AT (775) 335-1845.

#### **2. INDEMNIFICATION**

CONSULTANT agrees, subject to the limitations in Nevada Revised Statutes Section 338.155, to save and hold harmless and fully indemnify RTC, Washoe County, City of Reno and City of Sparks including their elected officials, officers, employees, and agents (hereafter, "Indemnitees") from and against any and all claims, proceedings, actions, liability and damages, including reasonable attorneys' fees and defense costs incurred in any action or proceeding (collectively "Damages") arising out of the:

- A. Negligence, errors, omissions, recklessness or intentional misconduct of CONSULTANT or CONSULTANT's agents, employees, officers, directors, subconsultants, or anyone else for whom CONSULTANT may be legally responsible, which are based upon or arising out of the professional services of CONSULTANT; and
- B. Violation of law or any contractual provisions or any infringement related to trade names, licenses, franchises, patents or other means of protecting interests in products or inventions resulting from the use by the Indemnitees of any materials, devices, processes, equipment, or other deliverable (including software) supplied by CONSULTANT under or as a result of this Agreement, but excluding any violation or infringement resulting from the modification or alteration by the Indemnitees of any materials, devices, processes, equipment, or other deliverable (including software) not consented to by CONSULTANT.

CONSULTANT further agrees to defend, save and hold harmless and fully indemnify the Indemnitees from and against any and all Damages arising out the negligence, errors, omissions, recklessness or intentional misconduct of CONSULTANT or CONSULTANT's agents, employees, officers, directors, subconsultants, or anyone else for whom CONSULTANT may be legally responsible, which are not based upon or arising out of the professional services of CONSULTANT.

The Damages shall include, but are not limited to, those resulting from personal injury to any person, including bodily injury, sickness, disease or death and injury to real property or personal

property, tangible or intangible, and the loss of use of any of that property, whether or not it is physically injured.

If the Indemnitees are involved in defending actions of CONSULTANT or anyone else for whom CONSULTANT is legally responsible, CONSULTANT shall reimburse the Indemnitees for the time spent by such personnel at the rate of the Indemnitees pay or compensation for such services.

If an Indemnitee is found to be liable in the proceeding, then CONSULTANT'S obligation hereunder shall be limited to the proportional share of the liability attributed to CONSULTANT.

In determining whether a claim is subject to indemnification, the incident underlying the claim shall determine the nature of the claim.

In the event of a violation or an infringement under paragraph 2.B above and the use is enjoined, CONSULTANT, at its sole expense, shall either (1) secure for the Indemnitees the right to continue using the materials by suspension of any injunction or by procuring a license or licenses for the Indemnitees; or (2) modify the materials so that they become non-infringing. This covenant shall survive the termination of the Professional Services Agreement.

The provisions of this Agreement are separate and severable and it is the intent of the Parties hereto that in the event any provision of this Agreement should be determined by any court of competent jurisdiction to be void, voidable or too restrictive for any reason whatsoever, the remaining provisions of this Agreement shall remain valid and binding upon said Parties. It is also understood and agreed that in the event any provision should be considered, by any court of competent jurisdiction, to be void because it imposes a greater obligation on CONSULTANT than is permitted by law, such court may reduce and reform such provisions to limitations which are deemed reasonable and enforceable by said court.

### **3. GENERAL REQUIREMENTS**

Prior to the start of any work on a RTC project, CONSULTANT shall purchase and maintain insurance of the types and limits as described below insuring against claims for injuries to persons or damages to property which may arise from or in connection with the performance of the work hereunder by CONSULTANT, its subconsultants, or their employees, agents, or representatives. The cost of all such insurance shall be borne by CONSULTANT.

### **4. VERIFICATION OF COVERAGE**

CONSULTANT shall furnish RTC with a certificate(s) of insurance, executed by a duly authorized representative of each insurer, showing compliance with the insurance requirements set forth herein, on forms acceptable to RTC. All deductibles and self-insured retentions requiring RTC approval shall be shown on the certificate. All certificates and endorsements are to be addressed to RTC's Finance Director and be received by RTC before work commences. Upon request, CONSULTANT agrees that RTC has the right to review CONSULTANT'S and the Sub's insurance policies, or certified copies of the policies. Copies of applicable policy forms or endorsements confirming required additional insured, waiver of subrogation and notice of

cancellation provisions are required to be provided with any certificate(s) evidencing the required coverage.

## **5. NOTICE OF CANCELLATION**

CONSULTANT or its insurers shall provide at least thirty (30) days' prior written notice to RTC prior to the cancellation or non-renewal of any insurance required under this Agreement. An exception may be included to provide at least ten (10) days' written notice if cancellation is due to non-payment of premium. CONSULTANT shall be responsible to provide prior written notice to RTC as soon as practicable upon receipt of any notice of cancellation, non-renewal, reduction in required limits or other material change in the insurance required under this Agreement.

## **6. SUBCONSULTANTS & SUBCONTRACTORS**

CONSULTANT shall include all Subcontractors and Subconsultants (referred to collectively as "Subs") as insureds under its liability policies OR shall cause Subs employed by CONSULTANT to purchase and maintain separate liability coverages and limits of the types specified herein. If any Subs maintain separate liability coverages and limits, each shall include the RTC, Washoe County, City of Reno and City of Sparks as additional insureds under its commercial general liability policy, subject to the same requirements stated herein, without requiring a written contract or agreement between each of the additional insureds and any sub-consultant or sub-contractor. Any separate coverage limits of liability maintained by Subs shall be at least **\$1,000,000** per occurrence and at least **\$2,000,000** for any applicable coverage aggregates or the amount customarily carried by the Sub, whichever is GREATER. If any Subs provide their own insurance with limits less than required of the Contractor, Contractor shall include Subs in their coverage up to the full limits required of the Contractor. When requested by RTC, CONSULTANT shall furnish copies of certificates of insurance evidencing coverage for each subconsultant. CONSULTANT need not require its non-design subcontractors to carry Professional Errors and Omissions Liability insurance.

## **7. DEDUCTIBLES AND SELF-INSURED RETENTIONS**

Any deductibles or self-insured retentions that exceed \$25,000 per occurrence or claim must be declared to RTC's Finance Director prior to signing this Agreement. RTC is entitled to request and receive additional documentation, financial or otherwise, prior to giving its approval of the deductibles and self-insured retentions. Any changes to the deductibles or self-insured retentions made during the term of this Agreement or during the term of any policy must be declared to RTC's Finance Director prior to the change taking effect.

## **8. ACCEPTABILITY OF INSURERS**

Required insurance is to be placed with insurers with a Best's rating of no less than A-VII and acceptable to RTC. RTC may accept coverage with carriers having lower Best's ratings upon review of financial information concerning CONSULTANT and the insurance carrier. RTC reserves the right to require that CONSULTANT'S insurer(s) be licensed and admitted in the State

of Nevada or meet any applicable state and federal laws and regulations for non-admitted insurance placements.

## **9. OTHER CONDITIONS**

- A. Failure to furnish the required certificate(s) or failure to maintain the required insurance may result in termination of this Agreement at RTC's option.
- B. If CONSULTANT fails to furnish the required certificate or fails to maintain the required insurance as set forth herein, RTC shall have the right, but not the obligation, to purchase said insurance at CONSULTANT's expense.
- C. Any waiver of CONSULTANT's obligation to furnish such certificate or maintain such insurance must be in writing and signed by an authorized representative of RTC. Failure of RTC to demand such certificate or other evidence of full compliance with these insurance requirements or failure of RTC to identify a deficiency from evidence that is provided shall not be construed as a waiver of CONSULTANT's obligation to maintain such insurance, or as a waiver as to the enforcement of any of these provisions at a later date.
- D. By requiring insurance herein, RTC does not represent that coverage and limits will necessarily be adequate to protect CONSULTANT, and such coverage and limits shall not be deemed as a limitation on CONSULTANT's liability under the indemnities granted to RTC in this contract.
- E. If CONSULTANT'S liability policies do not contain the standard ISO separation of insureds condition, or a substantially similar clause, they shall be endorsed to provide cross-liability coverage.

## **10. COMMERCIAL GENERAL LIABILITY**

CONSULTANT shall maintain commercial general liability (CGL) and, if necessary, commercial umbrella insurance with a limit of not less than **\$2,000,000** each occurrence. If such CGL insurance contains a general aggregate limit, it shall be increased to equal twice the required occurrence limit or revised to apply separately to this project.

CGL insurance shall be written on ISO occurrence form CG 00 01 04 13 (or a substitute form providing equivalent coverage) and shall cover liability arising from premises, operations, products-completed operations, personal and advertising injury, and liability assumed under an insured contract (including the tort liability of another assumed in a business contract).

RTC and any other Indemnitees listed in Section 2. INDEMNIFICATION of this Agreement shall be included as an additional insured under the CGL, using ISO additional insured endorsement CG 20 10 07/04 or CG 20 33 07/04 or a substitute providing equivalent coverage, and under the commercial umbrella, if any.

This insurance shall apply as primary insurance with respect to any other insurance or self-insurance programs afforded to RTC or any other Indemnitees under this Agreement. CONSULTANT waives all rights against RTC and any other Indemnitees listed in section 2. INDEMNIFICATION of this Agreement for recovery of damages to the extent these damages are covered by the commercial general liability or commercial umbrella liability insurance maintained pursuant to this agreement. CONSULTANT's insurer shall endorse CGL policy to waive subrogation against RTC with respect to any loss paid under the policy.

## **11. COMMERCIAL AUTOMOBILE LIABILITY**

CONSULTANT shall maintain automobile liability and, if necessary, commercial umbrella liability insurance with a limit of not less than **\$1,000,000** each accident. Such insurance shall cover liability arising out of any auto (including owned, hired, and non-owned autos).

Coverage shall be written on ISO form CA 00 01, CA 00 05, CA 00 25, or a substitute form providing equivalent liability coverage for all owned, leased, hired (rented) and non-owned vehicles (as applicable). RTC may agree to accept auto liability for non-owned and hired (rented) vehicles under the CGL if CONSULTANT does not own or operate any owned or leased vehicles.

CONSULTANT waives all rights against RTC, its officers, employees and volunteers for recovery of damages to the extent these damages are covered by the automobile liability or commercial umbrella liability insurance obtained by CONSULTANT pursuant to this Agreement.

## **12. INDUSTRIAL (WORKER'S COMPENSATION AND EMPLOYER'S LIABILITY) INSURANCE**

It is understood and agreed that there shall be no Industrial (Worker's Compensation and Employer's Liability) Insurance coverage provided for CONSULTANT or any subconsultants by RTC. CONSULTANT, and any subconsultants, shall procure, pay for and maintain the required coverages.

CONSULTANT shall maintain workers' compensation and employer's liability insurance meeting the statutory requirements of the State of Nevada, including but not limited to NRS 616B.627 and NRS 617.210. The employer's liability limits shall not be less than **\$1,000,000** each accident for bodily injury by accident or **\$1,000,000** each employee for bodily injury by disease.

CONSULTANT shall provide a Final Certificate for itself and each subconsultant evidencing that CONSULTANT and each subconsultant maintained workers' compensation and employer's liability insurance throughout the entire course of the project.

If CONSULTANT, or any subconsultant is a sole proprietor, coverage for the sole proprietor must be purchased and evidence of coverage must appear on the Certificate of Insurance and Final Certificate.

CONSULTANT waives all rights against RTC, its elected officials, officers, employees and agents for recovery of damages to the extent these damages are covered by the workers compensation and employer's liability or commercial umbrella liability insurance obtained by Tenant pursuant to this agreement. CONSULTANT shall obtain an endorsement equivalent to WC 00 03 13 to affect this waiver.

### **13. PROFESSIONAL ERRORS AND OMISSIONS LIABILITY**

CONSULTANT shall maintain professional liability insurance applying to liability for a professional error, omission, or negligent act arising out of the scope of CONSULTANT'S services provided under this Agreement with a limit of not less than **\$1,000,000** each claim and annual aggregate. CONSULTANT shall maintain professional liability insurance during the term of this Agreement and, if coverage is provided on a "claims made" or "claims made and reported" basis, shall maintain coverage or purchase an extended reporting period for a period of at least three (3) years following the termination of this Agreement.

## Exhibit D

### Federally Required Clauses

#### 1. PROMPT PAYMENT PROVISION

CONSULTANT must pay all subconsultants for satisfactory performance of their contracts no later than thirty (30) days from the receipt of payment made to CONSULTANT by RTC. Prompt return of retainage payments from CONSULTANT to the subconsultants will be made within fifteen (15) days after each subconsultant's work is satisfactorily completed. Any delay or postponement of payment among the parties may take place only for good cause and with RTC's prior written approval. If CONSULTANT determines the work of the subconsultant to be unsatisfactory, it must notify RTC's project manager immediately in writing and state the reasons. The failure by CONSULTANT to comply with this requirement will be construed to be a breach of the Contract and may be subject to sanctions as specified in the Contract or any other options listed in 49 C.F.R. 26.29.

#### 2. NONDISCRIMINATION

During the performance of this Contract, CONSULTANT, for itself, its assignees, and successors in interest, agrees as follows:

A. Compliance with Regulations. CONSULTANT shall comply with the regulations relative to nondiscrimination in DOT-assisted programs, 49 C.F.R. Part 21, as they may be amended from time to time (referred to in this section as the "Regulations"), which are herein incorporated by reference and made a part of this Contract.

B. Nondiscrimination. CONSULTANT shall not discriminate on the grounds of age, race, color, sex, or national origin in the selection and retention of subconsultants, including procurement of materials and leases of equipment. CONSULTANT shall not participate, either directly or indirectly, in the discrimination prohibited by Section 21.5 of the Regulations, including employment practices when the Contract covers a program set forth in Appendix B of the Regulations.

C. Solicitations for Subcontracts, including Procurement of Materials and Equipment. In all solicitations, whether by competitive proposing or negotiation made by CONSULTANT for work to be performed under a subcontract, including procurement of materials or leases of equipment, each potential subconsultant or supplier must be notified by CONSULTANT of CONSULTANT's obligations under this Contract and the Regulations relative to nondiscrimination on the grounds of age, race, color, sex, or national origin.

D. Information and Reports. CONSULTANT must provide all information and reports required by the Regulations or directives issued pursuant thereto, and must permit access to its books, records, accounts, other sources of information, and its facilities as may be determined by RTC to be pertinent to ascertain compliance with such Regulations, orders, and instructions.



Where any information is required, or the information is in the exclusive possession of another who fails or refuses to furnish this information, CONSULTANT must so certify to RTC, and must set forth what efforts it has made to obtain the information.

E. Sanctions for Noncompliance. In the event of CONSULTANT's noncompliance with the nondiscrimination provisions of this Contract, RTC shall impose such contract sanctions as it may determine to be appropriate, including, but not limited to: (1) withholding of payments to CONSULTANT under the Contract until CONSULTANT complies, and/or (2) cancellation, termination, or suspension of the Contract, in whole or in part.

CONSULTANT shall include the provisions of this clause in every subcontract. CONSULTANT must take such action with respect to any subcontract or procurement as RTC may direct as a means of enforcing those provisions, including sanctions for noncompliance. However, if CONSULTANT becomes involved in or is threatened with litigation with a subconsultant as a result of such direction, CONSULTANT may request RTC to enter into the litigation to protect the interests of RTC.

### **3. AFFIRMATIVE ACTION IN EMPLOYMENT**

CONSULTANT shall comply with the provisions of Section 503 of the Rehabilitation Act of 1973 (the "Rehabilitation Act").

A. CONSULTANT will not discriminate against any employee or applicant for employment because of physical or mental handicap in regard to any position for which the employee or applicant for employment is qualified. CONSULTANT agrees to take affirmative action to employ, advance in employment and otherwise treat qualified handicapped individuals without discrimination based upon their physical or mental handicap in all employment practices such as the following: employment, upgrading, demotion or transfer, recruitment, advertising, layoff or termination, rates of pay or other forms of compensation, and selection for training, including apprenticeship.

B. CONSULTANT agrees to comply with the rules, regulations, and relevant orders of the Secretary of Labor pursuant to the Rehabilitation Act.

C. In the event of CONSULTANT's noncompliance with the requirements of this clause, actions for noncompliance may be taken in accordance with the rules, regulations, and relevant orders of the Secretary of Labor pursuant to the Rehabilitation Act.

D. CONSULTANT agrees to post in conspicuous places, available to employees and applicants for employment, notices in a form to be prescribed by the director, provided by or through the contracting officer. Such notices shall state CONSULTANT's obligation under the law to take affirmative action to employ and advance in employment qualified handicapped employees and applicants for employment, and the rights of applicants and employees.

E. CONSULTANT shall include the provisions of this clause in every subcontract or

purchase order of \$2,500 or more unless exempted by rules, regulations, or orders of the Secretary of Transportation issued pursuant to Section 503 of the Rehabilitation Act, so that such provisions will be binding upon each subconsultant or vendor. CONSULTANT will take such action with respect to any subcontract or purchase order as the director of the Office of Federal Contract Compliance Programs may direct to enforce such provisions, including action for noncompliance (41 C.F.R. 60-741.4.4).

#### **4. INTEREST OF MEMBERS OF, OR DELEGATES TO, CONGRESS**

In accordance with 18 U.S.C. 431, no member of, or delegate to, the Congress of the United States shall be admitted to any share or part of this Contract or to any benefit arising therefrom.

#### **5. INTEREST OF PUBLIC OFFICIALS**

No member, officer, or employee of any public body, during his tenure, or for one (1) year thereafter, shall have any interest, direct or indirect, in this Contract or the benefits thereof.

#### **6. CIVIL RIGHTS**

The following requirements apply to the underlying Contract:

A. Nondiscrimination. In accordance with Title VI of the Civil Rights Act, as amended, 42 U.S.C. 2000d, section 303 of the Age Discrimination Act of 1975, as amended, 42 U.S.C. 6102, section 202 of the Americans with Disabilities Act of 1990, 42 U.S.C. 12132, and Federal transit law at 49 U.S.C. 5332, CONSULTANT agrees that it will not discriminate against any employee or applicant for employment because of race, color, creed, national origin, sex, age or disability.

B. Equal Employment Opportunity. The following equal employment opportunity requirements apply to the underlying contract:

- (1) Race, Color, Creed, National Origin, Sex. In accordance with Title VII of the Civil Rights Act, as amended, 42 U.S.C. 2000e, and Federal transit laws at 49 U.S.C. 5332, CONSULTANT agrees to comply with all applicable equal employment opportunity requirements of U.S. Department of Labor (U.S. DOL) regulations, "Office of Federal Contract Compliance Programs, Equal Employment Opportunity, Department of Labor", 41 C.F.R. Parts 60 et seq., (which implement Executive Order No. 11246, Equal Employment Opportunity", as amended by Executive Order No. 11375, "Amending Executive Order 11246 Relating to Equal Employment Opportunity", 42 U.S.C. 2000e note), and with any applicable Federal statutes, executive orders, regulations, and Federal policies that may in the future affect construction activities undertaken in the course of the Project. CONSULTANT agrees to take affirmative action to ensure that applicants are employed, and that employees are treated equally during employment, without regard to their race, color, creed,

national origin, sex, or age. Such action must include, but not be limited to, the following: employment, upgrading, demotion or transfer, recruitment or recruitment advertising, layoff or termination; rates of pay or other forms of compensation; and selection for training, including apprenticeship.

- (2) Age. In accordance with section 4 of the Age Discrimination in Employment Act of 1967, as amended, 29 U.S.C. 623 and Federal transit law at 49 U.S.C. 5332, CONSULTANT agrees to refrain from discrimination against present and prospective employees for reason of age.
- (3) Disabilities. In accordance with section 102 of the Americans with Disabilities Act, as amended, 42 U.S.C. 12112, CONSULTANT agrees that it will comply with the requirements of U.S. Equal Employment Opportunity Commission, "Regulations to Implement the Equal Employment Provisions of the Americans with Disabilities Act", 29 C.F.R. Part 1630, pertaining to employment of persons with disabilities.

C. CONSULTANT also agrees to include these requirements in each subcontract.

## 7. **INELIGIBLE CONSULTANTS**

In the event CONSULTANT is on the Comptroller General's List of Ineligible Consultants for Federally financed or assisted projects, this contract may be canceled, terminated, or suspended by RTC.

## 8. **NOTICE OF FEDERAL REQUIREMENTS**

New Federal laws, regulations, policies, and administrative practices may be established after the date of this Contract, which may apply to this Contract. If Federal requirements change, the changed requirements will apply to the Contract or the performance of work under the Contract as required. All standards or limits set forth in this Contract to be observed in the performance of the work are minimum requirements.

## 9. **THIRD-PARTY RIGHTS**

Notwithstanding anything herein to the contrary, the services provided under this Agreement shall not give rise to, nor shall be deemed to or construed so as to confer any rights on any other party, as a third-party beneficiary or otherwise.

## 10. **RECORDS RETENTION; AUDIT AND INSPECTION OF RECORDS**

A. CONSULTANT shall permit the authorized representatives of RTC, FHWA, the U.S. Department of Transportation's Inspector General, NDOT, and the Comptroller General of the United States, or any of their duly authorized representatives to inspect and audit all data and records of CONSULTANT relating to its performance under the contract until

the expiration of three (3) years after final payment under this Contract.

B. CONSULTANT further agrees to include in all subcontracts hereunder a provision to the effect that the subconsultant agrees that RTC, FHWA, the U.S. Department of Transportation's Inspector General, NDOT, and the Comptroller General of the United States, or any of their duly authorized representatives shall, until the expiration of three (3) years after final payment under the subcontract, have access to and the right to examine any books, documents, papers, and records of the subconsultant directly pertinent to this contract. The term "subcontract" as used in this clause excludes (1) purchase orders not exceeding \$10,000 and (2) subcontracts or purchase orders for public utility services at rates established for uniform applicability to the general public.

C. The periods of access and examination described above, for records which relate to (1) appeals under the dispute clause of this Contract, (2) litigation or the settlement of claims arising out of the performance of this Contract, or (3) costs and expenses of this Contract to which an exception has been taken by the U.S. Comptroller General or any of his duly authorized representatives, shall continue until such appeals, litigation, claims or exceptions have been disposed of.

#### **11. NO FEDERAL GOVERNMENT OBLIGATION TO THIRD PARTIES**

A. RTC and CONSULTANT acknowledge and agree that, notwithstanding any concurrence by the Federal Government in or approval of the solicitation or award of the underlying Contract, absent the express written consent by the Federal Government, the Federal Government is not a party to this Contract and shall not be subject to any obligations or liabilities to RTC, Consultant, or any other party (whether or not a party to that Contract) pertaining to any matter resulting from the underlying Contract.

B. CONSULTANT agrees to include the above clause in each subcontract. It is further agreed that the clause shall not be modified, except to identify the subconsultant who will be subject to its provisions.

#### **12. DEBARMENT, SUSPENSION, OTHER INELIGIBILITY AND VOLUNTARY EXCLUSION**

A. This Contract is a covered transaction for purposes of 2 C.F.R. Part 1200 and 2 C.F.R. Part 180. As such, CONSULTANT is required to verify that none of CONSULTANT, its principals, as defined at 2 C.F.R. 180.995, or affiliates, as defined at 2 C.F.R. 180.905, are excluded or disqualified as defined at 2 C.F.R. 180.940 and 180.945.

B. CONSULTANT is required to comply with 2 C.F.R. 180, Subpart C, and must include the requirement to comply with 2 C.F.R. 180, Subpart C, in all contracts for lower-tier transactions over \$25,000 and in all solicitations for lower tier contracts.

C. CONSULTANT agrees that it shall not knowingly enter into any lower-tier covered

transaction with a person or firm who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this contract.

### **13. COMPLIANCE WITH FEDERAL LOBBYING POLICY**

Section 1352 of Title 31, United States Code, provides in part that no appropriated funds may be expended by the recipient of a federal contract, grant, loan, or cooperative agreement to pay any person by influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with any of the following covered Federal actions: the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement.

Consultants who apply or bid for an award of \$100,000 or more shall file the certification required by 49 C.F.R. Part 20, "New Restrictions on Lobbying." Each tier certifies to the tier above that it will not and has not used federal appropriated funds to pay any person or organization for influencing or attempting to influence an officer or employee of any Federal Agency, a member of Congress, an officer or employee of Congress, or an employee of a member of Congress in connection with obtaining any federal Contract, grant or any other award covered by 31 U.S.C. 1352. Each tier shall also disclose the name of any registrant under the Lobbying Disclosure Act of 1995 who has made lobbying contacts on its behalf with non-federal funds with respect to that federal Contract, grant or award covered by 31 U.S.C. 1352. Such disclosures are forwarded from tier to tier up to the recipient.

CONSULTANT also agrees to include these requirements in each subcontract exceeding \$100,000 financed in whole or in part with Federal assistance.

### **14. REPORTING REQUIREMENTS**

CONSULTANT shall provide all information and reports required by the Regulations, or directives issued pursuant shall provide all information and reports required by the Regulations, or directives issued pursuant thereto, and shall permit access to its facilities as may be determined by RTC or the Federal Highway Administration (FHWA) to be pertinent to ascertain compliance with such Regulations or directives. Where any information required of a Consultant is in the exclusive possession of another who fails or refuses to furnish this information, CONSULTANT shall so certify to RTC, or the FHWA as appropriate, and shall set forth what efforts it has made to obtain the information.

## Exhibit E

During the performance of this contract, CONTRACTOR, for itself, its assignees, and successors in interest, agrees as follows:

1. **Compliance with Regulations:** The Consultant (hereinafter includes subconsultants) will comply with the Acts and the Regulations relative to Non-discrimination in Federally-assisted programs of the U.S. Department of Transportation, Federal Highway Administration (FHWA), as they may be amended from time to time, which are herein incorporated by reference and made a part of this contract.
2. **Non-discrimination:** The Consultant, with regard to the work performed by it during the contract, will not discriminate on the grounds of race, color, or national origin in the selection and retention of subconsultants, including procurements of materials and leases of equipment. The Consultant will not participate directly or indirectly in the discrimination prohibited by the Acts and the Regulations, including employment practices when the contract covers any activity, project, or program set forth in Appendix B of 49 C.F.R. Part 21.
3. **Solicitations for Subcontracts, Including Procurements of Materials and Equipment:** In all solicitations, either by competitive bidding, or negotiation made by the Consultant for work to be performed under a subcontract, including procurements of materials, or leases of equipment, each potential subconsultant or supplier will be notified by the Consultant of the Consultant's obligations under this contract and the Acts and the Regulations relative to Non-discrimination on the grounds of race, color, or national origin.
4. **Information and Reports:** The Consultant will provide all information and reports required by the Acts, the Regulations, and directives issued pursuant thereto and will permit access to its books, records, accounts, other sources of information, and its facilities as may be determined by the Recipient or the FHWA to be pertinent to ascertain compliance with such Acts, Regulations, and instructions. Where any information required of a Consultant is in the exclusive possession of another who fails or refuses to furnish the information, the Consultant will so certify to the Recipient or the FHWA, as appropriate, and will set forth what efforts it has made to obtain the information.
5. **Sanctions for Noncompliance:** In the event of a Consultant's noncompliance with the Non-discrimination provisions of this contract, the Recipient will impose such contract sanctions as it or the FHWA may determine to be appropriate, including, but not limited to:
  - a. withholding payments to the Consultant under the contract until the Consultant complies; and/or
  - b. cancelling, terminating, or suspending a contract, in whole or in part.
6. **Incorporation of Provisions:** The Consultant will include the provisions of paragraphs one through six in every subcontract, including procurements of materials and leases of

equipment, unless exempt by the Acts, the Regulations and directives issued pursuant thereto. The Consultant will take action with respect to any subcontract or procurement as the Recipient or the FHWA may direct as a means of enforcing such provisions including sanctions for noncompliance. Provided, that if the Consultant becomes involved in, or is threatened with litigation by a subconsultant, or supplier because of such direction, the Consultant may request the Recipient to enter into any litigation to protect the interests of the Recipient. In addition, the Consultant may request the United States to enter into the litigation to protect the interests of the United States.

## **Exhibit F**

During the performance of this contract, CONSULTANT, for itself, its assignees, and successors in interest, agrees to comply with the following non-discrimination statutes and authorities; including but not limited to:

### **Pertinent Non-Discrimination Authorities:**

- Title VI of the Civil Rights Act of 1964 (42 U.S.C. § 2000d *et seq.*, 78 stat. 252), (prohibits discrimination on the basis of race, color, national origin); and 49 C.F.R. Part 21.
- The Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, (42 U.S.C. § 4601), (prohibits unfair treatment of persons displaced or whose property has been acquired because of Federal or Federal-aid programs and projects);
- Federal-Aid Highway Act of 1973, (23 U.S.C. § 324 *et seq.*), (prohibits discrimination on the basis of sex);
- Section 504 of the Rehabilitation Act of 1973, (29 U.S.C. § 794 *et seq.*), as amended, (prohibits discrimination on the basis of disability), and 49 C.F.R. Part 27;
- The Age Discrimination Act of 1975, as amended, (42 U.S.C. § 6101 *et seq.*), (prohibits discrimination on the basis of age);
- Airport and Airway Improvement Act of 1982, (49 U.S.C. § 471, Section 47123), as amended (prohibits discrimination based on race, creed, color, national origin, or sex);
- The Civil Rights Restoration Act of 1987, (PL 100-209), (Broadened the scope, coverage and applicability of Title VI of the Civil Rights Act of 1964, The Age Discrimination Act of 1975 and Section 504 of the Rehabilitation Act of 1973, by expanding the definition of the terms "programs or activities" to include all of the programs or activities of the Federal-aid recipients, sub-recipients and Consultants, whether such programs or activities are Federally funded or not);
- Titles II and III of the Americans with Disabilities Act, which prohibit discrimination on the basis of disability in the operation of public entities, public and private transportation systems, places of public accommodation, and certain testing entities (42 U.S.C. §§ 12131 – 12189) as implemented by Department of Transportation regulations at 49 C.F.R. Parts 37 and 38;
- The Federal Aviation Administration's Non-discrimination statute (49 U.S.C. § 47123) (prohibits discrimination on the basis of race, color, national origin, and sex);
- Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations, which ensures nondiscrimination against



minority populations by discouraging programs, policies, and activities with disproportionately high and adverse human health or environmental effects on minority and low-income populations;

- Executive Order 13166, Improving Access to Services for Persons with Limited English Proficiency, and resulting agency guidance, national origin discrimination includes discrimination because of limited English proficiency (LEP). To ensure compliance with Title VI, you must take reasonable steps to ensure that LEP persons have meaningful access to your programs (70 Fed. Reg. at 74087 to 74100);
- Title IX of the Education Amendments of 1972, as amended, which prohibits you from discriminating because of sex in education programs or activities (20 U.S.C. § 1681 et seq).



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.4.1

To: Regional Transportation Commission

From: Alex Wolfson, Project Manager

**SUBJECT: Veterans Parkway ITS Project PSA with HDR Engineering, Inc.**

**RECOMMENDED ACTION**

Approve a contract with HDR Engineering, Inc., for design and optional construction management services for the Veterans Parkway ITS Project, in an amount not-to-exceed \$375,460.

**BACKGROUND AND DISCUSSION**

This Professional Services Agreement (PSA) with HDR Engineering, Inc., is for professional design services for the Veterans Parkway ITS Project in the amount of \$194,990, and optional engineering during construction services (EDC) in the amount of \$155,470. Project contingency in the amount of \$25,000 is also included in the agreement. The project includes the design of Intelligent Transportation Systems (ITS) infrastructure along Veterans Parkway between Long Meadow Drive and Steamboat Parkway. This ITS infrastructure includes conduit, fiber optic cable, traffic signal interconnect, and pan tilt zoom cameras. Additional ITS infrastructure such as radar detection systems and dynamic message signs will be evaluated and included if warranted.

HDR Engineering, Inc., was selected from the qualified Traffic Engineering Design and Construction Management Services list to perform engineering, construction management, and quality assurance. HDR's scope, schedule, and budget indicated the amount for design services is within the appropriated budget.

- Design Kickoff: September 2023
  - 50% Design Submittal: March 2024
  - 100% Design Submittal: July 2024
  - Construction: Fall 2024
-

**FISCAL IMPACT**

Fuel tax appropriations are included in the FY 2024 budget.

**PREVIOUS BOARD ACTION**

12/17/2021 Authorized the procurement of a qualified list of consultants to provide civil engineering, design, and construction management services for the Traffic Engineering Program and the Intelligent Transportation Systems (ITS) Program.

**AGREEMENT  
FOR  
PROFESSIONAL SERVICES**

This agreement (this “Agreement”) is dated and effective as of \_\_\_\_\_, 2023, by and between the Regional Transportation Commission of Washoe County (“RTC”) and HDR Engineering, Inc. (“CONSULTANT”).

WITNESSETH:

WHEREAS, RTC has selected HDR Engineering, Inc. from the Traffic Engineering and ITS shortlist to perform design and optional engineering during construction services in connection with the Veterans Pkwy ITS Project.

NOW, THEREFORE, RTC and CONSULTANT, in consideration of the mutual covenants and other consideration set forth herein, do hereby agree as follows:

ARTICLE 1 – TERM AND ENGAGEMENT

- 1.1. The term of this Agreement shall be from the date first written above through December 31, 2024, unless terminated at an earlier date, or extended to a later date, pursuant to the provisions herein.
- 1.2. CONSULTANT will perform the work using the project team identified in the Exhibit A - Scope of Services. Any changes to the project team must be approved by RTC’s Project Manager.
- 1.3. CONSULTANT will promptly, diligently and faithfully execute the work to completion in accordance with applicable professional standards subject to any delays due to strikes, acts of God, act of any government, civil disturbances, or any other cause beyond the reasonable control of CONSULTANT.
- 1.4. CONSULTANT shall not proceed with work until both parties have executed this Agreement and a purchase order has been issued to CONSULTANT. If CONSULTANT violates that prohibition, CONSULTANT forfeits any and all right to reimbursement and payment for that work and waives any and all claims against RTC, its employees, agents, and affiliates, including but not limited to monetary damages, and any other remedy available at law or in equity arising under the terms of this Agreement. Furthermore, prior to execution and issuance of a purchase order, CONSULTANT shall not rely on the terms of this Agreement in any way, including but not limited to any written or oral representations, assurances or warranties made by RTC or any of its agents, employees or affiliates, or on any dates of performance, deadlines, indemnities, or any term contained in this Agreement or otherwise.

## ARTICLE 2 - SERVICES OF CONSULTANT

### 2.1. SCOPE OF SERVICES

The scope of services consist of the tasks set forth in Exhibit A.

### 2.2. SCHEDULE OF SERVICES

Tasks and subtasks shall be completed in accordance with the schedule in Exhibit A. Any change(s) to the schedule must be approved by RTC's Project Manager.

### 2.3. CONTINGENCY

Contingency line items identified in the scope of services are for miscellaneous increases within the scope of work. Prior to the use of any contingency amounts, CONSULTANT shall provide a letter to RTC's Project Manager detailing the need, scope, and not-to-exceed budget for the proposed work. Work to be paid for out of contingency shall proceed only with the RTC Project Manager's written approval.

### 2.4. OPTIONS

RTC shall have the right to exercise its option(s) for all or any part of the optional tasks or subtasks identified in Exhibit A. CONSULTANT will prepare and submit a detailed scope of services reflecting the specific optional services requested, a schedule for such services, and a cost proposal. RTC will review and approve the scope of services and RTC and CONSULTANT will discuss and agree upon compensation and a schedule. CONSULTANT shall undertake no work on any optional task without written notice to proceed with the performance of said task. RTC, at its sole option and discretion, may select another individual or firm to perform the optional tasks or subtasks identified in Exhibit A.

### 2.5. ADDITIONAL SERVICES

CONSULTANT will provide additional services when agreed to in writing by RTC and CONSULTANT.

### 2.6. PERFORMANCE REQUIREMENTS

Any and all design and engineering work furnished by CONSULTANT shall be performed by or under the supervision of persons licensed to practice architecture, engineering, or surveying (as applicable) in the State of Nevada, by personnel who are careful, skilled, experienced and competent in their respective trades or professions, who are professionally qualified to perform the work, and who shall assume professional responsibility for the accuracy and completeness of documents prepared or checked by them, in accordance with appropriate prevailing professional standards. Notwithstanding the provision of any drawings, technical specifications, or other data by RTC, CONSULTANT shall have the

responsibility of supplying all items and details required for the deliverables required hereunder.

Any sampling and materials testing shall be performed by an approved testing laboratory accredited by AASHTO or other ASTM recognized accrediting organization in the applicable test methods. If any geotechnical or materials testing is performed by a sub-consultant, that laboratory shall maintain the required certification. Proof of certification shall be provided to RTC with this Agreement. If certification expires or is removed during the term of this Agreement, CONSULTANT shall notify RTC immediately, and propose a remedy. If an acceptable remedy cannot be agreed upon by both parties, RTC may terminate this Agreement for default.

CONSULTANT shall provide only Nevada Alliance for Quality Transportation Construction (NAQTC) qualified personnel to perform field and laboratory sampling and testing during the term of this Agreement. All test reports shall be signed by a licensed NAQTC tester and notated with his/her license number.

2.7. ERRORS AND OMISSIONS

CONSULTANT shall, without additional compensation, correct or revise any deficiencies, errors, or omissions caused by CONSULTANT in its analysis, reports, and services. CONSULTANT also agrees that if any error or omission is found, CONSULTANT will expeditiously make the necessary correction, at no expense to RTC. If an error or omission was directly caused by RTC, and not by CONSULTANT and RTC requires that such error or omission be corrected, CONSULTANT may be compensated for such additional work.

ARTICLE 3 - COMPENSATION

3.1. CONSULTANT shall be paid for hours worked at the hourly rates and rates for testing in Exhibit B. RTC shall not be responsible for any other costs or expenses except as provided in Exhibit B.

3.2. The maximum amount payable to CONSULTANT to complete each task is equal to the not-to-exceed amounts identified in Exhibit B. CONSULTANT can request in writing that RTC's Project Manager reallocate not-to-exceed amounts between tasks. A request to reallocate not-to-exceed amounts must be accompanied with a revised fee schedule, and must be approved in writing by RTC's Project Manager prior to performance of the work. In no case shall CONSULTANT be compensated in excess of the following not-to exceed amounts:

Design Services (Tasks 1 to 6)	\$194,990
Design Contingency (Task 7)	\$15,000
Optional EDC Services (Tasks 8 to 12)	\$155,470
<u>EDC Contingency (Task 13)</u>	<u>\$10,000</u>
Total Not-to-Exceed Amount	\$375,460

- 3.3. For any work authorized under Section 2.5, "Additional Services," RTC and CONSULTANT will negotiate not-to-exceed amounts based on the standard hourly rates and rates for testing in Exhibit B. Any work authorized under Section 2.5, "Additional Services," when performed by persons who are not employees or individuals employed by affiliates of CONSULTANT, will be billed at a mutually agreed upon rate for such services, but not more than 105% of the amounts billed to CONSULTANT for such services.
- 3.4. CONSULTANT shall receive compensation for preparing for and/or appearing in any litigation at the request of RTC, except: (1) if such litigation costs are incurred by CONSULTANT in defending its work or services or those of any of its sub-consultants; or (2) as may be required by CONSULTANT's indemnification obligations. Compensation for litigation services requested by RTC shall be paid at a mutually agreed upon rate and/or at a reasonable rate for such services.

#### ARTICLE 4 - INVOICING

- 4.1. CONSULTANT shall submit monthly invoices in the format specified by RTC. Invoices must be submitted to [accountspayable@rtcwashoe.com](mailto:accountspayable@rtcwashoe.com). RTC's payment terms are 30 days after the receipt of the invoice. Simple interest will be paid at the rate of half a percent (0.5%) per month on all invoices approved by RTC that are not paid within thirty (30) days of receipt of the invoice.
- 4.2. RTC shall notify CONSULTANT of any disagreement with any submitted invoice for consulting services within thirty (30) days of receipt of an invoice. Any amounts not in dispute shall be promptly paid by RTC.
- 4.3. CONSULTANT shall maintain complete records supporting every request for payment that may become due. Upon request, CONSULTANT shall produce all or a portion of its records and RTC shall have the right to inspect and copy such records.

#### ARTICLE 5 - ACCESS TO INFORMATION AND PROPERTY

- 5.1. Upon request and without cost to CONSULTANT, RTC will provide all pertinent information that is reasonably available to RTC including surveys, reports and any other data relative to design and construction.
- 5.2. RTC will provide access to and make all provisions for CONSULTANT to enter upon RTC facilities and public lands, as required for CONSULTANT to perform its work under this Agreement.

#### ARTICLE 6 - OWNERSHIP OF WORK

- 6.1. Plans, reports, studies, tracings, maps, software, electronic files, licenses, programs, equipment manuals, and databases and other documents or instruments of service prepared or obtained by CONSULTANT in the course of performing work under this Agreement,

shall be delivered to and become the property of RTC. Software already developed and purchased by CONSULTANT prior to the Agreement is excluded from this requirement. CONSULTANT and its sub-consultants shall convey and transfer all copyrightable interests, trademarks, licenses, and other intellectual property rights in such materials to RTC upon completion of all services under this Agreement and upon payment in full of all compensation due to CONSULTANT in accordance with the terms of this Agreement. Basic survey notes, sketches, charts, computations and similar data prepared or obtained by CONSULTANT under this Agreement shall, upon request, also be provided to RTC.

- 6.2. CONSULTANT represents that it has secured all necessary licenses, consents, or approvals to use the components of any intellectual property, including computer software, used in providing services under this Agreement, that it has full legal title to and the right to reproduce such materials, and that it has the right to convey such title and other necessary rights and interests to RTC.
- 6.3. CONSULTANT shall bear all costs arising from the use of patented, copyrighted, trade secret, or trademarked materials, equipment, devices, or processes used on or incorporated in the services and materials produced under this Agreement.
- 6.4. CONSULTANT agrees that all reports, communications, electronic files, databases, documents, and information that it obtains or prepares in connection with performing this Agreement shall be treated as confidential material and shall not be released or published without the prior written consent of RTC; provided, however, that CONSULTANT may refer to this scope of work in connection with its promotional literature in a professional and commercially reasonable manner. The provisions of this subsection shall not apply to information in whatever form that comes into the public domain. The provisions of this paragraph also shall not restrict CONSULTANT from giving notices required by law or complying with an order to provide information or data when such order is issued by a court, administrative agency, or other entity with proper jurisdiction, or if it is reasonably necessary for CONSULTANT to defend itself from any suit or claim.

## ARTICLE 7 - TERMINATION

### 7.1. CONTRACT TERMINATION FOR DEFAULT

If CONSULTANT fails to perform services in the manner called for in this Agreement or if CONSULTANT fails to comply with any other provisions of this Agreement, RTC may terminate this Agreement for default. Termination shall be effected by serving a notice of termination on CONSULTANT setting forth the manner in which CONSULTANT is in default. CONSULTANT will only be paid the contract price for services delivered and accepted, or services performed in accordance with the manner of performance set forth in this Agreement.

If it is later determined by RTC that CONSULTANT had an excusable reason for not performing, such as a fire, flood, or events which are not the fault of or are beyond the control of CONSULTANT, RTC, after setting up a new performance schedule, may allow



CONSULTANT to continue work, or treat the termination as a termination for convenience.

7.2. CONTRACT TERMINATION FOR CONVENIENCE

RTC may terminate this Agreement, in whole or in part, at any time by written notice to CONSULTANT when it is in RTC's best interest. CONSULTANT shall be paid its costs, including contract closeout costs, and profit on work performed up to the time of termination. CONSULTANT shall promptly submit its termination claim to RTC to be paid CONSULTANT. If CONSULTANT has any property in its possession belonging to RTC, CONSULTANT will account for the same, and dispose of it in the manner RTC directs.

ARTICLE 8 - INSURANCE

- 8.1. CONSULTANT shall not commence any work or permit any employee/agent to commence any work until satisfactory proof has been submitted to RTC that all insurance requirements have been met.
- 8.2. In conjunction with the performance of the services/work required by the terms of this Agreement, CONSULTANT shall obtain all types and amounts of insurance set forth in Exhibit C, and shall comply with all provisions set forth therein.

ARTICLE 9 - HOLD HARMLESS

- 9.1. CONSULTANT's obligation under this provision is as set forth in Exhibit C. Said obligation would also extend to any liability of RTC resulting from any action to clear any lien and/or to recover for damage to RTC property.

ARTICLE 10 - EQUAL EMPLOYMENT OPPORTUNITY

- 10.1. During the performance of this Agreement, CONSULTANT agrees not to discriminate against any employee or applicant for employment because of race, color, religion, sex, age, disability, or national origin. CONSULTANT will take affirmative action to ensure that applicants are employed, and that employees are treated fairly during employment, without regard to their race, color, religion, sex, age, disability, or national origin. Such action shall include, but not be limited to, the following: employment, upgrading, demotion, or transfer; recruitment or recruitment advertising; layoff or termination; rates of pay or other forms of compensation; and selection for training, including apprenticeship. CONSULTANT agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided by RTC setting forth the provisions of this nondiscrimination clause.
- 10.2. CONSULTANT will, in all solicitations or advertisements for employees placed by or on behalf of CONSULTANT, state that well qualified applicants will receive consideration of employment without regard to race, color, religion, sex, age, disability, or national origin.

- 10.3. CONSULTANT will cause the foregoing provisions to be inserted in all sub-agreements for any work covered by this Agreement so that such provisions will be binding upon each sub-consultant.

## ARTICLE 11 - RESOLUTION OF CLAIMS AND DISPUTES

### 11.1. NEGOTIATED RESOLUTION

In the event that any dispute or claim arises under this Agreement, the parties shall timely cooperate and negotiate in good faith to resolve any such dispute or claim. Such cooperation shall include providing the other party with all information in order to properly evaluate the dispute or claim and making available the necessary personnel to discuss and make decisions relative to the dispute or claim.

### 11.2. MEDIATION

If the parties have been unable to reach an informal negotiated resolution to the dispute or claim within thirty (30) days following submission in writing of the dispute or claim to the other party, or such longer period of time as the parties may agree to in writing, either party may then request, in writing, that the dispute or claim be submitted to mediation (the "Mediation Notice"). After the other party's receipt or deemed receipt of the Mediation Notice, the parties shall endeavor to agree upon a mutually acceptable mediator, but if the parties have been unable to agree upon a mediator within ten (10) days following receipt of the Mediation Notice, then each party shall select a mediator and those two selected mediators shall select the mediator. A mediator selected by the parties' designated mediators shall meet the qualification set forth in as provided in Rule 4 of Part C., "Nevada Mediation Rules" of the "Rules Governing Alternative Dispute Resolutions adopted by the Nevada Supreme Court." Unless otherwise agreed to by the parties, in writing, the mediator shall have complete discretion over the conduct of the mediation proceeding. Unless otherwise agreed to by the parties, in writing, the mediation proceeding must take place within thirty (30) days following appointment of the mediator. The parties shall share the mediator's fee and any filing fees equally. The mediation shall be held in Washoe County, Nevada, unless otherwise agreed to by the parties, in writing. Agreements reached in mediation shall be enforceable as settlement agreements in any court having jurisdiction thereof.

### 11.3. LITIGATION

In the event that the parties are unable to settle and/or resolve the dispute or claim as provided above, then either party may proceed with litigation in the Second Judicial District Court of the State of Nevada, County of Washoe.

11.4. CONTINUING CONTRACT PERFORMANCE

During the pendency of any dispute or claim the parties shall proceed diligently with performance of this Agreement and such dispute or claim shall not constitute an excuse or defense for a party's nonperformance or delay.

ARTICLE 12 – PROJECT MANAGERS

- 12.1. RTC's Project Manager is Alex Wolfson or such other person as is later designated in writing by RTC. RTC's Project Manager has authority to act as RTC's representative with respect to the performance of this Agreement.
- 12.2. CONSULTANT's Project Manager is Will Johnson or such other person as is later designated in writing by CONSULTANT. CONSULTANT's Project Manager has authority to act as CONSULTANT's representative with respect to the performance of this Agreement.

ARTICLE 13 - NOTICE

- 13.1. Notices required under this Agreement shall be given as follows:

RTC: Bill Thomas, AICP  
Executive Director  
Alex Wolfson  
RTC Project Manager  
Regional Transportation Commission  
1105 Terminal Way  
Reno, Nevada 89502  
Email: awolfson@rtcwashoe.com  
(775)335-1880

CONSULTANT: Will Johnson  
Senior ITS Project Manager  
HDR Engineering, Inc.  
9805 Double R Blvd, Suite 101  
Reno, NV 89521  
Email: will.johnson@hdrinc.com  
Phone: (720) 483-3990

ARTICLE 14 - DELAYS IN PERFORMANCE

14.1. TIME IS OF THE ESSENCE

It is understood and agreed that all times stated and referred to herein are of the essence. The period for performance may be extended by RTC's Executive Director pursuant to the

process specified herein. No extension of time shall be valid unless reduced to writing and signed by RTC's Executive Director.

14.2. UNAVOIDABLE DELAYS

If the timely completion of the services under this Agreement should be unavoidably delayed, RTC may extend the time for completion of this Agreement for not less than the number of days CONSULTANT was excusably delayed. A delay is unavoidable only if the delay is not reasonably expected to occur in connection with or during CONSULTANT's performance, is not caused directly or substantially by acts, omissions, negligence or mistakes of CONSULTANT, is substantial and in fact causes CONSULTANT to miss specified completion dates, and cannot adequately be guarded against by contractual or legal means.

14.3. NOTIFICATION OF DELAYS

CONSULTANT shall notify RTC as soon as CONSULTANT has knowledge that an event has occurred or otherwise becomes aware that CONSULTANT will be delayed in the completion of the work. Within ten (10) working days thereafter, CONSULTANT shall provide such notice to RTC, in writing, furnishing as much detail on the delay as possible and requesting an extension of time.

14.4. REQUEST FOR EXTENSION

Any request by CONSULTANT for an extension of time to complete the work under this Agreement shall be made in writing to RTC. CONSULTANT shall supply to RTC documentation to substantiate and justify the additional time needed to complete the work and shall provide a revised schedule. RTC shall provide CONSULTANT with notice of its decision within a reasonable time after receipt of a request.

ARTICLE 15 - GENERAL PROVISIONS

15.1. SUCCESSORS AND ASSIGNS

RTC and CONSULTANT bind themselves and their successors and assigns to the other party and to the successors and assigns of such party, with respect to the performance of all covenants of this Agreement. Except as set forth herein, neither RTC nor CONSULTANT shall assign or transfer interest in this Agreement without the written consent of the other. Nothing herein shall be construed as creating a personal liability on the part of any officer or agent or any public body which may be a party hereto, nor shall it be construed as giving any rights or benefits hereunder to anyone other than RTC and CONSULTANT.

15.2. NON TRANSFERABILITY

This Agreement is for CONSULTANT's professional services, and CONSULTANT's rights and obligations hereunder may not be assigned without the prior written consent of RTC.

15.3. SEVERABILITY

If any part, term, article, or provision of this Agreement is, by a court of competent jurisdiction, held to be illegal, void, or unenforceable, or to be in conflict with any law of the State of Nevada, the validity of the remaining provisions or portions of this Agreement are not affected, and the rights and obligations of the parties shall be construed and enforced as if this Agreement did not contain the particular part, term, or provision held invalid.

15.4. RELATIONSHIP OF PARTIES

CONSULTANT is an independent contractor to RTC under this Agreement. Accordingly, CONSULTANT is not entitled to participate in any retirement, deferred compensation, health insurance plans or other benefits RTC provides to its employees. CONSULTANT shall be free to contract to provide similar services for others while it is under contract to RTC, so long as said services and advocacy are not in direct conflict, as determined by RTC, with services being provided by CONSULTANT to RTC.

15.5. WAIVER/BREACH

Any waiver or breach of a provision in this Agreement shall not be deemed a waiver of any other provision in this Agreement and no waiver is valid unless in writing and executed by the waiving party. An extension of the time for performance of any obligation or act shall not be deemed an extension of time for the performance of any other obligation or act. This Agreement inures to the benefit of and is binding upon the parties to this Agreement and their respective heirs, successors and assigns.

15.6. REGULATORY COMPLIANCE

A. CONSULTANT shall comply with all applicable federal, state and local government laws, regulations and ordinances. CONSULTANT shall be responsible for obtaining all necessary permits and licenses for performance of services under this Agreement. Upon request of RTC, CONSULTANT shall furnish RTC certificates of compliance with all such laws, orders and regulations.

B. CONSULTANT represents and warrants that none of the services to be rendered pursuant to this Agreement constitute the performance of public work, as that term is defined by Section 338.010(17) of the Nevada Revised Statutes. To the extent CONSULTANT does engage in such public work, CONSULTANT shall be responsible for paying the prevailing wage as required by Chapter 338 of the Nevada Revised Statutes.

15.7. EXCLUSIVE AGREEMENT

There are no verbal agreements, representations or understandings affecting this Agreement, and all negotiations, representations and undertakings are set forth herein with the understanding that this Agreement constitutes the entire understanding by and between the parties.

15.8. AMENDMENTS

No alteration, amendment or modification of this Agreement shall be effective unless it is in writing and signed by both parties.

15.9. CONTINUING OBLIGATION

CONSULTANT agrees that if, because of death or any other occurrence it becomes impossible for any principal or employee of CONSULTANT to render the services required under this Agreement, neither CONSULTANT nor the surviving principals shall be relieved of any obligation to render complete performance. However, in such event, RTC may terminate this Agreement if it considers the death or incapacity of such principal or employee to be a loss of such magnitude as to affect CONSULTANT's ability to satisfactorily complete the performance of this Agreement.

15.10. APPLICABLE LAW AND VENUE

The provisions of this Agreement shall be governed and construed in accordance with the laws of the State of Nevada. The exclusive venue and court for all lawsuits concerning this Agreement shall be the Second Judicial District Court of the State of Nevada, County of Washoe, and the parties hereto submit to the jurisdiction of that District Court.

15.11. ATTORNEYS' FEES

In the event of a dispute between the parties result in a proceeding in any Court of Nevada having jurisdiction, the prevailing party shall be entitled to an award of costs and any reasonable attorneys' fees.

15.12. CERTIFICATION REQUIRED BY NEVADA SENATE BILL 27 (2017)

CONSULTANT expressly certifies and agrees, as a material part of this Agreement, that it is not currently engaged in a boycott of Israel. CONSULTANT further agrees, as a material part of this Agreement, it will not engage in a boycott of Israel for the duration of this Agreement. If, at any time during the formation or duration of this Agreement, CONSULTANT is engaged or engages in a boycott of Israel, it will constitute a material breach of this Agreement.

IN WITNESS WHEREOF, the parties hereto have made and executed this Agreement the day and year first above written.

REGIONAL TRANSPORTATION COMMISSION  
OF WASHOE COUNTY

By: \_\_\_\_\_  
Bill Thomas, AICP, Executive Director

HDR ENGINEERING, INC.

By: \_\_\_\_\_  
Craig Smart, PE, Associate Vice President

**EXHIBIT A**

**SCOPE OF SERVICES**  
**FOR THE**  
**VETERANS PKWY ITS PROJECT**

This scope of services includes design and construction support of the Veterans Parkway ITS Project. The project lies within the City of Reno. Procurement and installation of actual infrastructure will be provided by the selected contractor after the bid has been awarded for the bidding documents provided by the CONSULTANT.

The extent of planning and design work includes the improvement objectives at the following locations:

<u>#</u>	<u>Main Street</u>	<u>Side Street</u>	<u>Improvement Objective</u>
<b>City of Reno</b>			
1	Veterans Pkwy (Approx. 8000 LF of new fiber and conduit (where needed))	Steamboat Pkwy to Long Meadow Dr.	Signal interconnect with fiber optic cables and conduit (where needed) for better operational coordination with connections to the existing City of Reno ITS Network. Additional ITS infrastructure such as pan tilt zoom (PTZ) cameras, flow detectors, and dynamic message signs as determined by the RTC and City of Reno.

**A. Preliminary and General Items (Project Management):**

1. Coordination with RTC project manager and staff will be ongoing throughout the project. Project management and coordination meetings or conference calls will be held with the RTC and other parties as appropriate. CONSULTANT will coordinate kick-off meeting and hold progress meetings during course of project monthly. The anticipated schedule will be nine (9) months.
2. Coordination meetings with the City of Reno. CONSULTANT will conduct up to four (4) in-person one-on-one agency meetings, outside of meetings included in other tasks, in order to confirm existing design conditions, needs, and coordinate design to complete project design objectives. The RTC Project Manager will determine appropriate contact personnel at each agency to be met with. Meeting notes will be developed and provided as a summary of discussions.

**B. Data Collection, Analysis, and Design**

1. Investigate Existing Conditions and Field Inventory for interconnect design:



- a. CONSULTANT will obtain and review available as-built plans from the RTC and City of Reno for project locations.
- b. CONSULTANT will conduct a field review with City of Reno to evaluate existing field conditions for new and existing conduit paths.
- c. CONSULTANT will evaluate existing network equipment at locations identified herein for connection to network.
- d. CONSULTANT will not evaluate existing pedestrian ramps for general compliance with current ADA standards. If the ramps are in conflict with proposed improvements evaluation of ramps will be considered an additional service.

## 2. Survey and Mapping

- a. Aerial Mapping: Aerial photography for these sites will be using NearMap or Washoe County aerials provided by the RTC. The aerials will be put in the same coordinate system for consistency.
- b. Center lines will be developed off of existing features.
- c. CONSULTANT will evaluate right of way and easement needs based on County GIS data and where new equipment is to be installed.
- d. Boundary Survey: Survey boundary services will not be done for these sites as work will be limited to infrastructure within existing right of way.
- e. Supplemental Design Survey (Optional): Supplemental Survey is not anticipated to be required for these project sites. Where required it will be considered an additional service.

3. Subsurface Utilities: CONSULTANT will investigate the site for the presence of ITS conduit and pull boxes and if existing, designate the ITS conduit along the east side of Veterans Parkway from Steamboat Parkway to Long Meadow Drive in accordance with the American Society of Civil Engineers Standard guideline for the Collection and Depiction of Existing Subsurface Utility Data to Quality Level B (QL B) standards. This effort will require access to the ITS pull boxes along the route and connection to a trace wire assumed to be within. Should pull boxes not be accessible and/or trace wire not exist within the conduit, the ability to designate to QL B standards will be eliminated and QL A standards will be required.

If QL A is required, up to four (4) test holes (QL A) will be performed in accordance with the American Society of Civil Engineers Standard guideline for the Collection and Depiction of Existing Subsurface Utility Data to QL A standards. Test hole locations will be determined after consulting with the project team. Should the conduit not be detectable, additional test hole efforts may be required to confirm location.

A NV licensed PLS will collect all designation and test hole points. Horizontal Datum shall be based on NAD83/94 Nevada State Plane Coordinate System, West Zone, modified ground, based on Washoe County Network. Vertical Datum shall be based on NAD88 based on City of Reno published benchmarks.

Deliverables will include:

Designating (QL B)

- Place pink paint / flags depicting the approximate horizontal location of detected target.
- Provide a not to scale utility field sketch in KMZ format depicting the horizontal alignment as detected in the field.
- Provide Civil 3D CAD file and CSV point file.

Test Holes (QL A)

- Provide a Test Hole Data Report in a Portable Document Format (.pdf) for each completed location. Such Report shall include the following information.
- Test hole number and date of completion.
- Approximate plan and section view (not to scale) of utility and test hole location in relationship to the existing roadway and ground surface.
- Collected utility data: type, depth, size and material as readily obtainable.
- Provide utility photos where obtainable at exposed locations.
- Provide CSV point file

All located, existing underground utilities will be shown on the Plan Sheets accompanied with the following “Note: Subsurface utilities are depicted by their Quality Levels in accordance American Society of Civil Engineers Standard Guidelines for the Collection and Depiction of Existing Subsurface Utility Data (CI/ASCE 38-02). All utility information shown hereon is depicted to Quality Level ‘A/B’, unless otherwise noted.”

4. Utility coordination: Based on field investigation, CONSULTANT will provide RTC a list of utility company whose utilities are likely to be within the project limits or reasonably affected by the project. RTC will issue the initial notification to the utility agencies on the list and CONSULTANT will coordinate with the utility agencies for upcoming work, facility relocation and new installation, and to insure utilities likely affected by the project are drawn on the plans, evaluate potential conflicts through field investigation, investigate conflict resolution strategies.
5. Traffic Signal Basis of Design: Existing traffic signal/ ITS equipment shall be depicted in a “greyed out” format using “new” and “existing” stipple patterns with new installations shown superimposed unless important details are obscured.
6. A geotechnical evaluation will not be performed under this project as it is not required.
7. Drainage Evaluations are not anticipated for this project and therefore excluded.

8. Environmental Services are not anticipated for this project and therefore excluded.

**C. 50% Design**

1. Prepare 50% Plans and Specifications

- a. The construction plans will be on 11" x 17" size sheets and will show all elements of the project construction, including plan/profile view, right-of-way lines, cross-sections and construction/slope limits. The final plan set will include, as a minimum: Cover Sheet, Interconnect Plan Sheets (at 1"=40' scale, double banked), Detail Sheets (scales as noted).
- b. Plan updates and further development. Prepare plans, and an OPC suitable for RTC and Local Government review.

Project Plans (21) will include:

- i. Cover (1)
- ii. Sheet index (1)
- iii. General notes (1)
- iv. Project details (6)
- v. Project fiber splice details (3)
- vi. Plan sheets (9)  
(x) indicated the number of plan sheets

- c. CONSULTANT will perform a project walk through to evaluate that every aspect of the project scope has been captured.
- d. A quality control review of the plans, contract documents and technical specifications will be performed by the CONSULTANT which will focus on technical aspects of the plans and specifications and will review that all items of work are adequately covered. The quality control reviewer will check, initial, and date each plan sheet as appropriate.
- e. CONSULTANT will submit 50% Plans to RTC, City of Reno, and to utility agencies.

**D. 90% Design**

1. Prepare 90% Plans and Specifications

- a. CONSULTANT shall incorporate review comments from RTC, City of Reno, and utility companies into the 90% design process. CONSULTANT shall conduct a 50% comment resolution meeting with the RTC and all applicable commenters as part of this process.
- b. Plan updates and further development. Prepare plans and an OPC suitable

for RTC and Local Government review.

Project Plans (21) will include:

- i. Cover (1)
- ii. Sheet index (1)
- iii. General notes (1)
- iv. Project details (6)
- v. Project fiber splice details (3)
- vi. Plan sheets (9)  
(x) indicated the number of plan sheets

- c. Project Specifications: CONSUTLANT will prepare Contract Documents and Technical Specifications which will reference the latest edition of Standard Specifications for Public Works Construction (Orange Book) for standard construction items.
- d. A quality control review of the plans, contract documents and technical specifications will be performed by the CONSULTANT which will focus on technical aspects of the plans and specifications and will review that all items of work are adequately covered. The quality control reviewer will check, initial, and date each plan sheet as appropriate.
- e. CONSULTANT will perform a project walk through to evaluate that every aspect of the project scope has been captured.
- f. CONSULTANT will submit 90% Plans to RTC, City of Reno, and to utility agencies with a comment review matrix to document 50% comment responses.

**E. Final Design (100% Signed and sealed bid documents)**

1. CONSULTANT shall incorporate review comments from RTC, City of Sparks and City of Reno into Final Construction Document process. CONSULTANT shall conduct a 90% comment resolution meeting with the RTC and all applicable commenters as part of this process.
2. Prepare Final Plans and Specifications: Prepare Final Construction Plans, Contract Documents and Technical Specifications suitable for construction bid advertisement for the approved alignment in accordance with RTC standards and requirements. RTC will provide the boilerplate via e-mail in MS Word format.
3. A PDF cover sheet with digital signature blocks will be produced at this submittal for routing to the agencies for project bidding.
4. A quality control review of the plans, contract documents and technical specifications will be performed by the CONSULTANT which will focus on technical aspects of the plans and specifications and will review that all items of work are adequately covered.

The quality control reviewer will check, initial, and date each plan sheet as appropriate. The RTC, Local Entity and Quality Control review comments will be incorporated into the final Plans and Specifications.

5. The final construction plans will be on 11" x 17" size sheets and will show all elements of the project construction. The final plans and specifications will be signed and sealed by a Nevada Registered Professional Civil Engineer in responsible charge of preparation. Plans and specifications will be submitted to the RTC, City of Reno, utility agencies and other affected parties for review at the 50%, 90%, and final (100%) stages of completion per the following:
  - a. Electronic (pdf) 11"x17" plans and specifications will be sent to the review agencies at each submittal including the final cover sheet which will be routed digitally.

#### **F. Bidding Services**

1. Plan Set and Specification Distribution. CONSULTANT will provide the RTC with final plans and specifications, including addenda, in Portable Document Format (PDF), for use in the Procureware system.
2. Pre-bid Meeting. CONSULTANT will be available during the bidding process to answer technical questions and will participate in the pre-bid meeting. All questions and responses will be documented and provided to RTC. CONSULTANT will prepare and provide PDF addenda, if required. All questions regarding legal aspects of the contract documents will be referred directly to RTC. CONSULTANT will prepare and provide a PDF summary of the pre-bid meeting, as directed by the RTC.
3. Bid Opening. CONSULTANT will attend the bid opening and review the bids received for irregularities and provide a recommendation for award. CONSULTANT will tabulate bid results into a MS Excel spreadsheet and check multiplication and addition of bid items.

#### **G. Construction Phase Services or Engineering During Construction (Sole Option and Discretion of RTC)**

The RTC and CONSULTANT shall review Optional Construction Services following the completion of final design. The consultant shall provide staff to monitor the construction activities of the contractor. The consultant shall provide one part-time Resident Engineer (licensed as a professional engineer in the State of Nevada), one full time Inspector Tech IV, one part time technician/tester (qualified through the NAQCT process). Estimated time for each position on the project can be found in section 2 below. In addition to the inspection and testing listed here, the CONSULTANT shall provide a part time 2 person survey crew for use on the project as needed. The following tasks summarize the major elements of Construction Support Services anticipated with this project.

1. Contract Administration: Provide contract administration services as follows:

- a. Pre-Construction Conference: CONSULTANT will attend a Pre-Construction Conference prior to commencement of Work at the Site.
- b. Visits to Site and Observation of Construction: CONSULTANT will provide on-site construction observation services during the construction phase. CONSULTANT will make visits at intervals as directed by RTC in order to observe the progress of the Work. Such visits and observations by CONSULTANT are not intended to be exhaustive or to extend to every aspect of Contractor's work in progress. Observations are to be limited to spot checking, selective measurement, and similar methods of general observation of the Work based on CONSULTANT's exercise of professional judgment. Based on information obtained during such visits and such observations, CONSULTANT will evaluate whether Contractor's work is generally proceeding in accordance with the Contract Documents, and CONSULTANT will keep RTC informed of the general progress of the Work. CONSULTANT shall also provide recommendations to RTC for any construction changes necessitated by field conditions.
- c. The purpose of CONSULTANT's site visits will be to enable CONSULTANT to better carry out the duties and responsibilities specifically assigned in this Agreement to CONSULTANT, and to provide RTC a greater degree of confidence that the completed Work will conform in general to the Contract Documents. CONSULTANT shall not, during such visits or as a result of such observations of Contractor's work in progress, supervise, direct, or have control over Contractor's work, nor shall CONSULTANT have authority over or responsibility for the means, methods, techniques, equipment choice and usage, sequences, schedules, or procedures of construction selected by Contractor, for safety precautions and programs incident to Contractor's work, nor for any failure of Contractor to comply with laws and regulations applicable to Contractor's furnishing and performing the Work. Accordingly, CONSULTANT neither guarantees the performance of any Contractor nor assumes responsibility for any Contractor's failure to furnish and perform its work in accordance with the Contract Documents.
- d. Review and provide recommendations on contractor's traffic control plans.
- e. CONSULTANT will review and provide feedback to the RTC on traffic control plans submitted to RTC for approval.
- f. Review and provide recommendations on contractor's construction schedule and work progress CONSULTANT will review and provide feedback to the RTC on the construction schedule and work progress submitted to the RTC for approval.

- g. Submittals: CONSULTANT will review and approve or take other appropriate action in respect to Submittals, Shop Drawings, Samples, and other data which Contractor is required to submit, but only for conformance with the information given in the Contract Documents. Such review and approvals or other action will not extend to means, methods, techniques, equipment choice and usage, sequences, schedules, or procedures of construction or to related safety precautions and programs.
- h. Substitutes and "or-equal": Evaluate and determine the acceptability of substitute or "or-equal" materials and equipment proposed by Contractor in accordance with the Contract Documents, but subject to the provisions of applicable standards of state or local government entities.
- i. Recommendations with Respect to Defective Work. CONSULTANT will recommend to RTC that Contractor's work be disapproved and rejected while it is in progress if, on the basis of such observations, CONSULTANT believes that such work will not produce a completed Project that conforms generally to Contract Documents.
- j. Clarifications and Interpretations: CONSULTANT will respond to reasonable and appropriate Contractor requests for information and issue necessary clarifications and interpretations of the Contract Documents to RTC as appropriate to the orderly completion of Contractor's work. Any orders authorizing variations from the Contract Documents will be made by RTC.
- k. Disagreements between RTC and Contractor: CONSULTANT will, if requested by RTC, render written decision on all claims of RTC and Contractor relating to the acceptability of Contractor's work or the interpretation of the requirements of the Contract Documents pertaining to the progress of Contractor's work. In rendering such decisions, CONSULTANT shall be fair and not show partiality to RTC or Contractor and shall not be liable in connection with any decision rendered in good faith in such capacity.
- l. Applications for Payment: Based on its observations and on review of applications for payment and accompanying supporting documentation, CONSULTANT will determine the amounts that CONSULTANT recommends Contractor be paid. Such recommendations of payment will be in writing and will constitute CONSULTANT's representation to RTC, based on such observations and review, that, to the best of CONSULTANT's knowledge, information and belief, Contractor's work has progressed to the point indicated and that such work-in-progress is generally in accordance with the Contract Documents subject to any qualifications stated in the recommendation. In the case of unit price work, CONSULTANT's recommendations of payment will include

determinations of quantities and classifications of Contractor's work, based on observations and measurements of quantities provided with pay requests.

- m. By recommending any payment, CONSULTANT shall not thereby be deemed to have represented that its observations to check Contractor's work have been exhaustive, extended to every aspect of Contractor's work in progress, or involved detailed inspections of the Work beyond the responsibilities specifically assigned to CONSULTANT in this Agreement. It will also not impose responsibility on CONSULTANT to make any examination to ascertain how or for what purposes Contractor has used the moneys paid on account of the Contract Price, nor to determine that title to any portion of the work in progress, materials, or equipment has passed to RTC free and clear of any liens, claims, security interests, or encumbrances, nor that there may not be other matters at issue between RTC and Contractor that might affect the amount that should be paid.
- n. Substantial Completion: CONSULTANT will, promptly after notice from Contractor that it considers the entire Work ready for its intended use, in company with RTC and Contractor, conduct a site visit to determine if the Work is substantially complete. Work will be considered substantially complete following satisfactory completion of all items with the exception of those identified on a final punch list. If after considering any objections of RTC, CONSULTANT considers the Work substantially complete, CONSULTANT will notify RTC and Contractor.
- o. Final Notice of Acceptability of the Work: CONSULTANT will conduct a final site visit to determine if the completed Work of Contractor is generally in accordance with the Contract Documents and the final punch list so that CONSULTANT may recommend, in writing, final payment to Contractor. Accompanying the recommendation for final payment, CONSULTANT shall also provide a notice that the Work is generally in accordance with the Contract Documents to the best of CONSULTANT's knowledge, information, and belief based on the extent of its services and based upon information provided to CONSULTANT upon which it is entitled to rely.
- p. Inspections and Tests: CONSULTANT may require special inspections or tests of Contractor's work as CONSULTANT deems appropriate and as further defined in the scope of services below, and will receive and review certificates of inspections from Subconsultants within CONSULTANT's area of responsibility or of tests and approvals required by laws and regulations or the Contract Documents. CONSULTANT's review of such certificates will be for the purpose of determining that the results certified indicate compliance with the Contract Documents and will not constitute an independent evaluation that the content or procedures of such inspections, tests, or approvals comply with the requirements of the Contract Documents. CONSULTANT shall be entitled to rely on the results of such tests and the facts being certified.



- q. Change Orders: CONSULTANT may recommend Change Orders to RTC, and will review and make recommendations related to reasonable and appropriate Change Orders submitted or proposed by the Contractor.
- r. Limitation of Responsibilities: CONSULTANT shall not be responsible for the acts or omissions of any Contractor, or of any of their subcontractors, suppliers, or of any other individual or entity performing or furnishing the Work. CONSULTANT shall not have the authority or responsibility to stop the work of any Contractor.

## 2. Inspection

- a. "Inspection services" means the observation of construction process for the purpose of determining that the Contractor is in substantial compliance with the plans and specifications and reporting to the RTC any observed deficiencies. The following tasks summarize the major elements of Inspection anticipated with this project.
- b. Construction of the project that requires inspection is anticipated at 40 working days (8 weeks). A full-time (8 hours per day) primary inspector will be on site during working days that include trenching, pouring concrete, grading, utility adjustments and paving (15 working days anticipated). A part-time (3 hours per day) Resident Engineer (P.E.) will be on site during all working days (45 working days anticipated). A full-time supplemental technician/tester will be provided at 8 hours per day (20 working days anticipated) and will perform asphalt sampling and density testing during paving operations. The primary inspector will be responsible for the following items:
  - i. Attend the preconstruction conference.
  - ii. Monitor the work performed by the Contractor to ascertain whether the work is in substantial accordance with the plans and specifications.
  - iii. Assist in problem resolution with the RTC, contractor personnel, utility agencies, the public and others.
  - iv. Prepare daily inspection reports, submitted weekly to RTC and copied to the appropriate government jurisdiction(s). The daily inspection will contain materials delivered to the site, excavation and earthwork, preparation of sub grades, placement of aggregate base material, asphaltic concrete, Portland cement concrete, the forming, placement or erection of structures, and review of contractor daily progress logs.
  - v. Provide materials quantity reports and assist in reviewing and analyzing contractor's monthly progress payments.
  - vi. Provide verification of the distribution of public relation notices required to be delivered by the Contractor.
  - vii. Assist in preparation of the Punch List.
  - viii. Maintain a field blue-line set of drawings (bond copy) to incorporate contractor record drawing mark-ups.

### 3. Materials Testing

- a. Upon receiving authorization from the RTC, the CONSULTANT will engage a Subconsultant to perform the Materials Testing services. The following tasks summarize the major elements of testing anticipated with this project.
- b. Provide Material Testing for compliance with the specifications per the latest edition of the Standard Specifications for Public Works Construction (Orange Book) testing requirements. Materials to be tested will include plantmix bituminous pavement, aggregate base, concrete cylinder samples, and trenching backfill material. Test reports, accompanied with CONSULTANT's recommendation regarding acceptance/mitigation of materials, shall be submitted promptly to the RTC and CC'd to appropriate governmental jurisdiction(s).

### 4. Surveying

- a. Upon receiving authorization from the RTC, the CONSULTANT will engage a Subconsultant to perform Surveying services. The following tasks summarize the major elements of surveying anticipated with this project.
  - i. Provide survey services for all survey on the project not required by the contractor.
  - ii. Provide initial stake out of the project in the field.
  - iii. Verify contractor ITS line offsets.
  - iv. Provide survey information to the RTC.

### 5. As-Built Information

- a. Record Drawings. Provide as-built record drawings for the completed project. A single file PDF format (11" x 17" at 300 dpi), will be provided to RTC for its files and distribution. The PDF file shall include all plan sheets in one file with index/bookmark for easy access to different sheets or sections of the plan set.
- b. The final record drawings must be identified, dated, and signed as the record drawings and must also contain the engineer's stamp and signature. The Consultant may either:
  - i. Provide the final revisions on the original engineer-stamped/signed reproducible drawings, which will then also be identified as the record drawings, or
  - ii. Provide new engineer-stamped/signed reproducible drawings identified as the record drawings.
- c. The Record Drawings shall include a scan of the original title sheet (including the appropriate signatures by RTC, local government, signed and stamped by the CONSULTANT) and identified as record drawings.

**I. Design/Construction Contingency**

1. This is a contingency for miscellaneous increases within the scope of this contract. CONSULTANT shall provide a letter detailing the need, scope, and not-to-exceed budget for any proposed work. Work under this task shall proceed only with the RTC Project Manager's written approval. A 10% contingency was added to the design tasks only.

## **Veterans Pkwy ITS Project Anticipated Schedule**

*\*Schedule will be refined as design progresses*

- Design Kick-Off - September 2023
- Data Collection and Analysis – 2023 Q4
- 50% Design – 2023 Q4 thru 2024 Q1
- 90% Design – 2024 Q1 thru 2024 Q2
- 100% Design – 2024 Q2 thru 2024 Q3
- Bidding Services – 2024 Q3
- Construction – 2024 Q3 thru 2024 Q4
- Construction Closeout – 2024 Q4 thru 2025 Q1

# Exhibit B - Compensation

Task No.	Task Description	Rates	Jim Hanson	Will Johnson	Nicole Jolly	Laura Sida	Jaymee Miller	Laycee Kolkman	Shari Jackson	Dean Weitzel	Sue Harrowa	Total HDR	Total HDR	Total HDR	Subs	Total
			Project Director	Project Manager	Senior Engineer	Engineering Intern	Project Representative	QA/QC	Administrative	Construction Administrator	Construction Inspector	Labor Hours	Labor (\$)	Expenses (\$)	(\$)	Cost (\$)
<b>Task 1. PRELIMINARY AND GENERAL ITEMS (TASK A)</b>																
			\$ 385.00	\$ 310.00	\$ 160.00	\$ 135.00	\$ 120.00	\$ 280.00	\$ 115.00	\$ 210.00	\$ 190.00					
1.1	Invoicing and Progress Reports			18			18		8			44	\$8,660			\$8,660
1.2	Project Management Plan			1	4				4			9	\$1,410			\$1,410
1.3	Quality Management Plan			1	4				4			9	\$1,410			\$1,410
1.4	Progress Meetings (18 meeting - 1 hour per meeting)	4		18	28							50	\$11,600			\$11,600
1.5	In person Coordination Meetings (4 - 1 hour meetings)			4	16							20	\$3,800	\$1,000		\$4,800
												0	\$0			\$0
	<b>Subtotal Task 1</b>		<b>4</b>	<b>58</b>	<b>76</b>	<b>0</b>	<b>18</b>	<b>0</b>	<b>16</b>	<b>0</b>	<b>0</b>	<b>172</b>	<b>\$35,680</b>	<b>\$1,000</b>	<b>\$0</b>	<b>\$36,680</b>
<b>Task 2. DATA COLLECTION, ANALYSIS AND DESIGN (TASK B)</b>																
2.1	Investigate Existing Conditions and Field Inventory			8	8	32		2				50	\$8,640			\$8,640
2.2	Survey and Mapping			4	8	32		2				46	\$7,400			\$7,400
2.3	Subsurface Utilities			4	8			2				14	\$3,080	\$26,000		\$29,080
2.4	Utility Coordination			4	8							12	\$2,520			\$2,520
												0	\$0			\$0
	<b>Subtotal Task 2</b>		<b>0</b>	<b>20</b>	<b>32</b>	<b>64</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>122</b>	<b>\$21,640</b>	<b>\$0</b>	<b>\$26,000</b>	<b>\$47,640</b>
<b>Task 3. 50% DESIGN PHASE (TASK C)</b>																
3.1	50% Design Topographic and ROW Mapping			4	16	8			8			36	\$5,800			\$5,800
3.2	50% Design Phase Plans			12	60	88		6	4			170	\$27,340			\$27,340
3.3	50% Design Phase Cost Estimate			12	8	2			2		4	28	\$6,260			\$6,260
3.4	50% Comment Resolution Meeting			4	4							8	\$1,880			\$1,880
	<b>Subtotal Task 3</b>		<b>0</b>	<b>32</b>	<b>88</b>	<b>98</b>	<b>0</b>	<b>6</b>	<b>14</b>	<b>0</b>	<b>4</b>	<b>242</b>	<b>\$41,280</b>	<b>\$0</b>	<b>\$0</b>	<b>\$41,280</b>
<b>Task 4. 90% DESIGN PHASE (TASK D)</b>																
4.1	90% Design Phase Plans			4	60	88		6	8			166	\$25,320			\$25,320
4.2	90% Design Phase Specifications			4	8	8		4	4	8		36	\$6,860			\$6,860
4.3	90% Design Phase Construction Cost Estimate			4	8			2	2		4	20	\$4,070			\$4,070
4.4	90% Comment Resolution Meeting			4	4							8	\$1,880			\$1,880
	<b>Subtotal Task 4</b>		<b>0</b>	<b>16</b>	<b>80</b>	<b>96</b>	<b>0</b>	<b>12</b>	<b>14</b>	<b>8</b>	<b>4</b>	<b>230</b>	<b>\$38,130</b>	<b>\$0</b>	<b>\$0</b>	<b>\$38,130</b>
<b>Task 5. FINAL DESIGN PHASE - 100% SIGNED AND SEALED (TASK E)</b>																
5.1	100% Design Phase Plans			4	8	64		6	8			90	\$13,760			\$13,760
5.3	100% Design Phase Specifications			4	8	8		4	4		2	30	\$5,560			\$5,560
5.4	100% Design Phase Construction Cost Estimate			4	8			2	4		2	20	\$3,920			\$3,920
5.5	100% Comment Resolution Meeting			4	8							12	\$2,520			\$2,520
	<b>Subtotal Task 5</b>		<b>0</b>	<b>16</b>	<b>32</b>	<b>72</b>	<b>0</b>	<b>12</b>	<b>16</b>	<b>0</b>	<b>4</b>	<b>152</b>	<b>\$25,760</b>	<b>\$0</b>	<b>\$0</b>	<b>\$25,760</b>
<b>Task 6. BIDDING SERVICES (TASK F)</b>																
6.1	Plan Set and Specification Distribution									8		8	\$1,680			\$1,680
6.2	Pre-bid Meeting			4	4							8	\$1,880			\$1,880
6.3	Bid Opening			2	2							4	\$940	\$1,000		\$1,940
												0	\$0			\$0
	<b>Subtotal Task 6</b>		<b>0</b>	<b>6</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>20</b>	<b>\$4,500</b>	<b>\$1,000</b>	<b>\$0</b>	<b>\$5,500</b>
<b>Task 7. DESIGN CONTINGENCY (TASK G) - OPTIONAL</b>																
7.1	Design Contingency			4	16	83						103	\$15,000			\$15,000
												0	\$0			\$0
	<b>Subtotal Task 7</b>		<b>0</b>	<b>4</b>	<b>16</b>	<b>83</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>103</b>	<b>\$15,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$15,000</b>
<b>Task 8. CONSTRUCTION ADMINISTRATIONS (TASK H) - OPTIONAL</b>																
8.1	Construction Administration			8					12	60	28	108	\$21,780			\$21,780
												0	\$0			\$0
	<b>Subtotal Task 8</b>		<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>12</b>	<b>60</b>	<b>28</b>	<b>108</b>	<b>\$21,780</b>	<b>\$0</b>	<b>\$0</b>	<b>\$21,780</b>
<b>Task 9. CONSTRUCTION INSPECTION (TASK I) - OPTIONAL</b>																
9.1	Construction Inspection			8						24	205	237	\$46,470			\$46,470
												0	\$0			\$0
	<b>Subtotal Task 9</b>		<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>24</b>	<b>205</b>	<b>237</b>	<b>\$46,470</b>	<b>\$0</b>	<b>\$0</b>	<b>\$46,470</b>
<b>Task 10. CONSTRUCTION SURVEYING (TASK J) - OPTIONAL</b>																
10.1	Construction Surveying			8						8	36	52	\$11,000		\$24,000	\$35,000
												0	\$0			\$0
	<b>Subtotal Task 10</b>		<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>36</b>	<b>52</b>	<b>\$11,000</b>	<b>\$0</b>	<b>\$24,000</b>	<b>\$35,000</b>
<b>Task 11. CONSTRUCTION MATERIALS TESTING (TASK K) - OPTIONAL</b>																
11.1	Construction Materials Testing			8						12	36	56	\$11,840		\$29,000	\$40,840
												0	\$0			\$0
	<b>Subtotal Task 11</b>		<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>12</b>	<b>36</b>	<b>56</b>	<b>\$11,840</b>	<b>\$0</b>	<b>\$29,000</b>	<b>\$40,840</b>
<b>Task 12. CONSTRUCTION AS-BUILTS INFORMATION (TASK L) - OPTIONAL</b>																
12.1	Construction As-Builts Information			8						8	38	54	\$11,380			\$11,380
												0	\$0			\$0
	<b>Subtotal Task 12</b>		<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>38</b>	<b>54</b>	<b>\$11,380</b>	<b>\$0</b>	<b>\$0</b>	<b>\$11,380</b>
<b>Task 13. EDC CONTINGENCY (TASK M) - OPTIONAL</b>																
13.1	EDC Contingency									8	44		\$10,000			\$10,000
												0	\$0			\$0
	<b>Subtotal Task 13</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>44</b>	<b>0</b>	<b>\$10,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$10,000</b>
	<b>Grand Total</b>		<b>4</b>	<b>192</b>	<b>330</b>	<b>413</b>	<b>18</b>	<b>36</b>	<b>72</b>	<b>136</b>	<b>399</b>	<b>1548</b>	<b>\$294,460</b>	<b>\$2,000</b>	<b>\$79,000</b>	<b>\$375,460</b>

## EXHIBIT C

### **INDEMNIFICATION AND INSURANCE REQUIREMENTS FOR PROFESSIONAL SERVICE AGREEMENTS [NRS 338 DESIGN PROFESSIONAL]**

2022-07-08 Version

#### **1. INTRODUCTION**

IT IS HIGHLY RECOMMENDED THAT CONSULTANTS CONFER WITH THEIR INSURANCE CARRIERS OR BROKERS TO DETERMINE THE AVAILABILITY OF THESE INSURANCE CERTIFICATES AND ENDORSEMENTS IN ADVANCE OF PROPOSAL SUBMISSION. IF THERE ARE ANY QUESTIONS REGARDING THESE INSURANCE REQUIREMENTS, IT IS RECOMMENDED THAT THE AGENT/BROKER CONTACT RTC'S FINANCE DIRECTOR AT (775) 335-1845.

#### **2. INDEMNIFICATION**

CONSULTANT agrees, subject to the limitations in Nevada Revised Statutes Section 338.155, to save and hold harmless and fully indemnify RTC, Washoe County, and City of Reno including their elected officials, officers, employees, and agents (hereafter, "Indemnitees") from and against any and all claims, proceedings, actions, liability and damages, including reasonable attorneys' fees and defense costs incurred in any action or proceeding (collectively "Damages") arising out of the:

- A. Negligence, errors, omissions, recklessness or intentional misconduct of CONSULTANT or CONSULTANT's agents, employees, officers, directors, subconsultants, or anyone else for whom CONSULTANT may be legally responsible, which are based upon or arising out of the professional services of CONSULTANT; and
- B. Violation of law or any contractual provisions or any infringement related to trade names, licenses, franchises, patents or other means of protecting interests in products or inventions resulting from the use by the Indemnitees of any materials, devices, processes, equipment, or other deliverable (including software) supplied by CONSULTANT under or as a result of this Agreement, but excluding any violation or infringement resulting from the modification or alteration by the Indemnitees of any materials, devices, processes, equipment, or other deliverable (including software) not consented to by CONSULTANT.

CONSULTANT further agrees to defend, save and hold harmless and fully indemnify the Indemnitees from and against any and all Damages arising out the negligence, errors, omissions, recklessness or intentional misconduct of CONSULTANT or CONSULTANT's agents, employees, officers, directors, subconsultants, or anyone else for whom CONSULTANT may be legally responsible, which are not based upon or arising out of the professional services of CONSULTANT.

The Damages shall include, but are not limited to, those resulting from personal injury to any person, including bodily injury, sickness, disease or death and injury to real property or personal

property, tangible or intangible, and the loss of use of any of that property, whether or not it is physically injured.

If the Indemnitees are involved in defending actions of CONSULTANT or anyone else for whom CONSULTANT is legally responsible, CONSULTANT shall reimburse the Indemnitees for the time spent by such personnel at the rate of the Indemnitees pay or compensation for such services.

If an Indemnitee is found to be liable in the proceeding, then CONSULTANT'S obligation hereunder shall be limited to the proportional share of the liability attributed to CONSULTANT.

In determining whether a claim is subject to indemnification, the incident underlying the claim shall determine the nature of the claim.

In the event of a violation or an infringement under paragraph 2.B above and the use is enjoined, CONSULTANT, at its sole expense, shall either (1) secure for the Indemnitees the right to continue using the materials by suspension of any injunction or by procuring a license or licenses for the Indemnitees; or (2) modify the materials so that they become non-infringing. This covenant shall survive the termination of the Professional Services Agreement.

The provisions of this Agreement are separate and severable and it is the intent of the Parties hereto that in the event any provision of this Agreement should be determined by any court of competent jurisdiction to be void, voidable or too restrictive for any reason whatsoever, the remaining provisions of this Agreement shall remain valid and binding upon said Parties. It is also understood and agreed that in the event any provision should be considered, by any court of competent jurisdiction, to be void because it imposes a greater obligation on CONSULTANT than is permitted by law, such court may reduce and reform such provisions to limitations which are deemed reasonable and enforceable by said court.

### **3. GENERAL REQUIREMENTS**

Prior to the start of any work on a RTC project, CONSULTANT shall purchase and maintain insurance of the types and limits as described below insuring against claims for injuries to persons or damages to property which may arise from or in connection with the performance of the work hereunder by CONSULTANT, its subconsultants, or their employees, agents, or representatives. The cost of all such insurance shall be borne by CONSULTANT.

### **4. VERIFICATION OF COVERAGE**

CONSULTANT shall furnish RTC with a certificate(s) of insurance, executed by a duly authorized representative of each insurer, showing compliance with the insurance requirements set forth herein, on forms acceptable to RTC. All deductibles and self-insured retentions requiring RTC approval shall be shown on the certificate. All certificates and endorsements are to be addressed to RTC's Finance Director and be received by RTC before work commences. Upon request, CONSULTANT agrees that RTC has the right to review CONSULTANT'S and the Sub's insurance policies, or certified copies of the policies. Copies of applicable policy forms or endorsements confirming required additional insured, waiver of subrogation and notice of

cancellation provisions are required to be provided with any certificate(s) evidencing the required coverage.

## **5. NOTICE OF CANCELLATION**

CONSULTANT or its insurers shall provide at least thirty (30) days' prior written notice to RTC prior to the cancellation or non-renewal of any insurance required under this Agreement. An exception may be included to provide at least ten (10) days' written notice if cancellation is due to non-payment of premium. CONSULTANT shall be responsible to provide prior written notice to RTC as soon as practicable upon receipt of any notice of cancellation, non-renewal, reduction in required limits or other material change in the insurance required under this Agreement.

## **6. SUBCONSULTANTS & SUBCONTRACTORS**

CONSULTANT shall include all Subcontractors and Subconsultants (referred to collectively as "Subs") as insureds under its liability policies OR shall cause Subs employed by CONSULTANT to purchase and maintain separate liability coverages and limits of the types specified herein. If any Subs maintain separate liability coverages and limits, each shall include the RTC, Washoe County, and City of Reno as additional insureds under its commercial general liability policy, subject to the same requirements stated herein, without requiring a written contract or agreement between each of the additional insureds and any sub-consultant or sub-contractor. Any separate coverage limits of liability maintained by Subs shall be at least **\$1,000,000** per occurrence and at least **\$2,000,000** for any applicable coverage aggregates or the amount customarily carried by the Sub, whichever is GREATER. If any Subs provide their own insurance with limits less than required of the Contractor, Contractor shall include Subs in their coverage up to the full limits required of the Contractor. When requested by RTC, CONSULTANT shall furnish copies of certificates of insurance evidencing coverage for each subconsultant. CONSULTANT need not require its non-design subcontractors to carry Professional Errors and Omissions Liability insurance.

## **7. DEDUCTIBLES AND SELF-INSURED RETENTIONS**

Any deductibles or self-insured retentions that exceed \$25,000 per occurrence or claim must be declared to RTC's Finance Director prior to signing this Agreement. RTC is entitled to request and receive additional documentation, financial or otherwise, prior to giving its approval of the deductibles and self-insured retentions. Any changes to the deductibles or self-insured retentions made during the term of this Agreement or during the term of any policy must be declared to RTC's Finance Director prior to the change taking effect.

## **8. ACCEPTABILITY OF INSURERS**

Required insurance is to be placed with insurers with a Best's rating of no less than A-VII and acceptable to RTC. RTC may accept coverage with carriers having lower Best's ratings upon review of financial information concerning CONSULTANT and the insurance carrier. RTC reserves the right to require that CONSULTANT'S insurer(s) be licensed and admitted in the State



of Nevada or meet any applicable state and federal laws and regulations for non-admitted insurance placements.

## **9. OTHER CONDITIONS**

- A. Failure to furnish the required certificate(s) or failure to maintain the required insurance may result in termination of this Agreement at RTC's option.
- B. If CONSULTANT fails to furnish the required certificate or fails to maintain the required insurance as set forth herein, RTC shall have the right, but not the obligation, to purchase said insurance at CONSULTANT's expense.
- C. Any waiver of CONSULTANT's obligation to furnish such certificate or maintain such insurance must be in writing and signed by an authorized representative of RTC. Failure of RTC to demand such certificate or other evidence of full compliance with these insurance requirements or failure of RTC to identify a deficiency from evidence that is provided shall not be construed as a waiver of CONSULTANT's obligation to maintain such insurance, or as a waiver as to the enforcement of any of these provisions at a later date.
- D. By requiring insurance herein, RTC does not represent that coverage and limits will necessarily be adequate to protect CONSULTANT, and such coverage and limits shall not be deemed as a limitation on CONSULTANT's liability under the indemnities granted to RTC in this contract.
- E. If CONSULTANT'S liability policies do not contain the standard ISO separation of insureds condition, or a substantially similar clause, they shall be endorsed to provide cross-liability coverage.

## **10. COMMERCIAL GENERAL LIABILITY**

CONSULTANT shall maintain commercial general liability (CGL) and, if necessary, commercial umbrella insurance with a limit of not less than **\$2,000,000** each occurrence. If such CGL insurance contains a general aggregate limit, it shall be increased to equal twice the required occurrence limit or revised to apply separately to this project.

CGL insurance shall be written on ISO occurrence form CG 00 01 04 13 (or a substitute form providing equivalent coverage) and shall cover liability arising from premises, operations, products-completed operations, personal and advertising injury, and liability assumed under an insured contract (including the tort liability of another assumed in a business contract).

RTC and any other Indemnitees listed in Section 2. INDEMNIFICATION of this Agreement shall be included as an additional insured under the CGL, using ISO additional insured endorsement CG 20 10 07/04 or CG 20 33 07/04 or a substitute providing equivalent coverage, and under the commercial umbrella, if any.

This insurance shall apply as primary insurance with respect to any other insurance or self-insurance programs afforded to RTC or any other Indemnitees under this Agreement.

CONSULTANT waives all rights against RTC and any other Indemnitees listed in section 2. INDEMNIFICATION of this Agreement for recovery of damages to the extent these damages are covered by the commercial general liability or commercial umbrella liability insurance maintained pursuant to this agreement. CONSULTANT's insurer shall endorse CGL policy to waive subrogation against RTC with respect to any loss paid under the policy.

## **11. COMMERCIAL AUTOMOBILE LIABILITY**

CONSULTANT shall maintain automobile liability and, if necessary, commercial umbrella liability insurance with a limit of not less than **\$1,000,000** each accident. Such insurance shall cover liability arising out of any auto (including owned, hired, and non-owned autos).

Coverage shall be written on ISO form CA 00 01, CA 00 05, CA 00 25, or a substitute form providing equivalent liability coverage for all owned, leased, hired (rented) and non-owned vehicles (as applicable). RTC may agree to accept auto liability for non-owned and hired (rented) vehicles under the CGL if CONSULTANT does not own or operate any owned or leased vehicles.

CONSULTANT waives all rights against RTC, its officers, employees and volunteers for recovery of damages to the extent these damages are covered by the automobile liability or commercial umbrella liability insurance obtained by CONSULTANT pursuant to this Agreement.

## **12. INDUSTRIAL (WORKER'S COMPENSATION AND EMPLOYER'S LIABILITY) INSURANCE**

It is understood and agreed that there shall be no Industrial (Worker's Compensation and Employer's Liability) Insurance coverage provided for CONSULTANT or any subconsultants by RTC. CONSULTANT, and any subconsultants, shall procure, pay for and maintain the required coverages.

CONSULTANT shall maintain workers' compensation and employer's liability insurance meeting the statutory requirements of the State of Nevada, including but not limited to NRS 616B.627 and NRS 617.210. The employer's liability limits shall not be less than **\$1,000,000** each accident for bodily injury by accident or **\$1,000,000** each employee for bodily injury by disease.

CONSULTANT shall provide a Final Certificate for itself and each subconsultant evidencing that CONSULTANT and each subconsultant maintained workers' compensation and employer's liability insurance throughout the entire course of the project.

If CONSULTANT, or any subconsultant is a sole proprietor, coverage for the sole proprietor must be purchased and evidence of coverage must appear on the Certificate of Insurance and Final Certificate.

CONSULTANT waives all rights against RTC, its elected officials, officers, employees and agents for recovery of damages to the extent these damages are covered by the workers compensation and employer's liability or commercial umbrella liability insurance obtained by Tenant pursuant to this agreement. CONSULTANT shall obtain an endorsement equivalent to WC 00 03 13 to affect this waiver.

### **13. PROFESSIONAL ERRORS AND OMISSIONS LIABILITY**

CONSULTANT shall maintain professional liability insurance applying to liability for a professional error, omission, or negligent act arising out of the scope of CONSULTANT'S services provided under this Agreement with a limit of not less than **\$1,000,000** each claim and annual aggregate. CONSULTANT shall maintain professional liability insurance during the term of this Agreement and, if coverage is provided on a "claims made" or "claims made and reported" basis, shall maintain coverage or purchase an extended reporting period for a period of at least three (3) years following the termination of this Agreement.



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.4.2

To: Regional Transportation Commission

From: Michele Payne, Property Agent

**SUBJECT: Recommendation to Sell Surplus Property**

**RECOMMENDED ACTION**

Approve a recommendation to commence with the sale of four parcels (APN# 020-111-66; 020-111-67; 020-111-36; and 020-111-37) acquired in connection with the Peppermill Bus Rapid Transit (BRT) Station Project, by sale to adjoining property owners, sealed bids, auction, direct sale, or as otherwise authorized by law.

**BACKGROUND AND DISCUSSION**

RTC acquired four parcels for the Peppermill BRT Station Project. APN# 020-111-66 and 020-111-67 are remnants of two larger parcels that were needed for the project; APN# 020-111-36 and 020-111-37 were acquired from the same property owner. The parcels are shown on the attached map. On January 15, 2021, the RTC Board approved an administrative settlement for the purchase of the four parcels.

The parcels are no longer needed for the project or for any other public use. If this item is approved, RTC will attempt to sell the parcels by sale to adjoining property owners, sealed bids, public auction, or direct sale pursuant to NRS 277A.255, or as otherwise authorized by law. After RTC and the buyer(s) agree on the terms and conditions of sale(s), staff will present the material terms of the sale(s) to the Board for approval along with any resolution or other action required by law.

Management Policy P-63 (Real Property Disposition) requires approval of this item before staff can commence with the sale.

**FISCAL IMPACT**

Sale proceeds will be returned to the fund of origin.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

# HILLBRAE TRACT - UNOFFICIAL (R.S. #29 & 30)

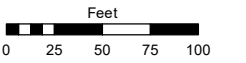
PORTION OF THE S 1/2 SECTION 24, T19N - R19E

Assessor's Map Number

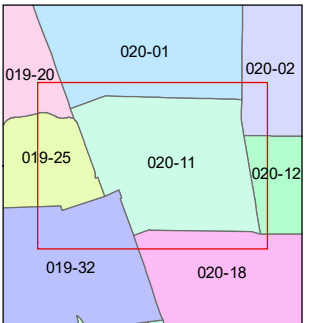
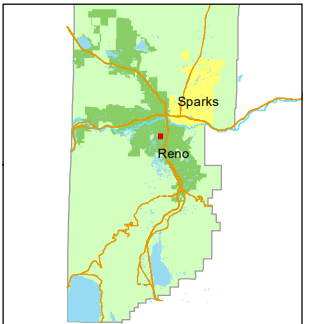
**020-11**

STATE OF NEVADA  
**WASHOE COUNTY**  
**ASSESSOR'S OFFICE**

1001 East Ninth Street, Building D  
 Reno, Nevada 89512  
 (775) 328-2231



1 inch = 100 feet



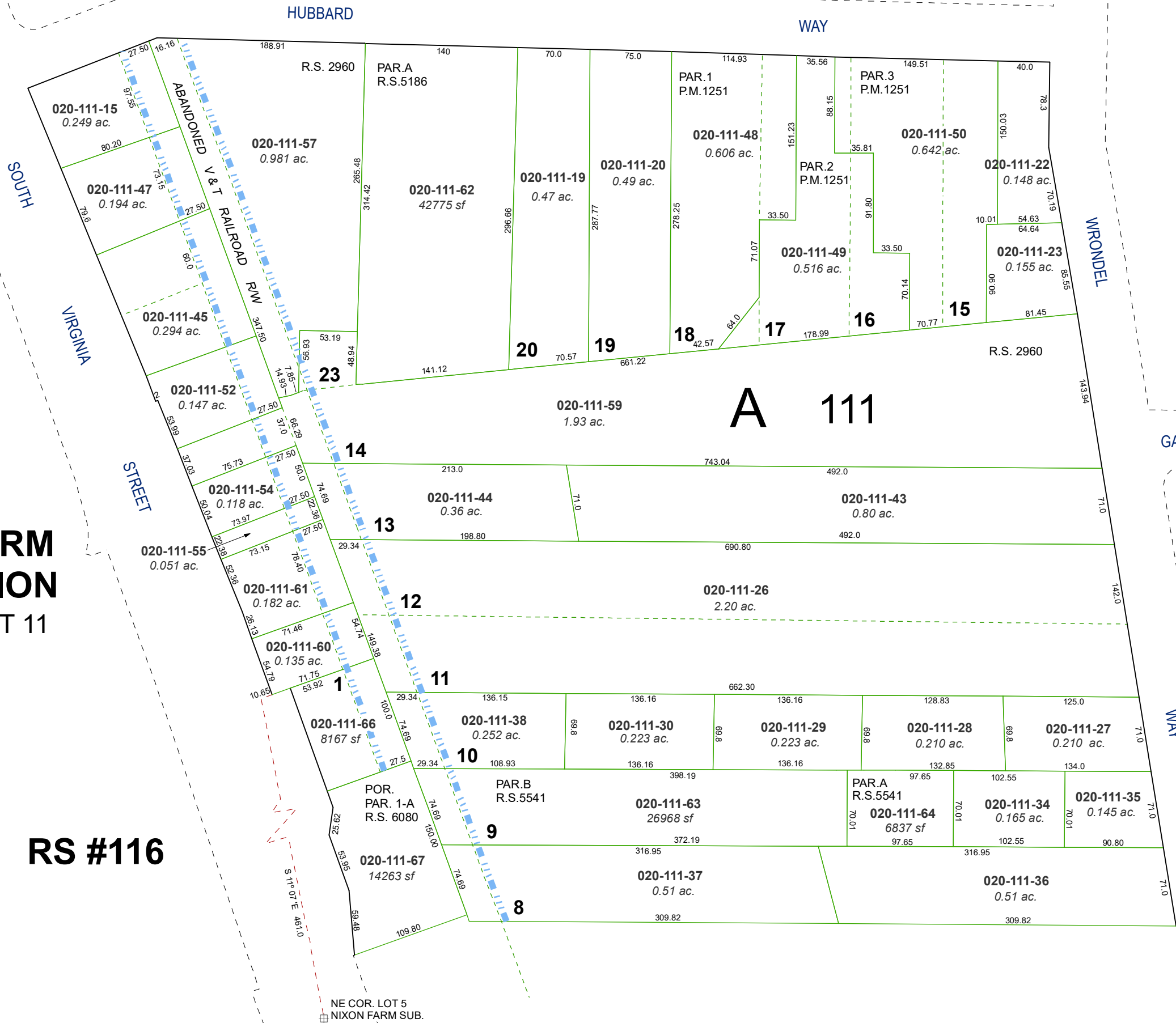
created by: CFB 10/26/2009

updated: KSB 9/08/10 CFB 2/05/14

JKF 9/16/19 KSB 6/30/23

area previously shown on map(s):

NOTE: This map was prepared for the use of the Washoe County Assessor for assessment and illustrative purposes only. It does not represent a survey of the premises. No liability is assumed as to the sufficiency or accuracy of the data delineated hereon.



**(#165)**  
**NIXON FARM**  
**SUBDIVISION**  
 PORTION LOT 11

**RS #116**

NE COR. LOT 5  
 NIXON FARM SUB.



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.5.1

To: Regional Transportation Commission

From: Mark Maloney, Director of Public Transportation and Operations

**SUBJECT: Bus Inspection Platform Award**

**RECOMMENDED ACTION**

Approve a contract with Spika Design & Manufacturing for the purchase of a bus inspection platform, in an amount not-to-exceed \$200,790.

**BACKGROUND AND DISCUSSION**

The RTC issued a Request for Proposal (RFP) in May 2023 for a bus maintenance platform to provide access to the roof section of fleet buses. Proposals were received by the RTC from one qualified firm.

A three-person proposal evaluation team consisting of three RTC employees reviewed and ranked the proposal. The proposal was evaluated based on the criteria set forth in the RFP:

- 25% Adjustability/flexibility of equipment
- 25% Grade of material
- 20% Material Finish
- 20% Warranty
- 10% Experience

Staff is recommending an award to Spika Design & Manufacturing as being the most advantageous and of the best value to the RTC.

**FISCAL IMPACT**

Funds are available in the FY 2024 budget for this purchase. Funding is federal funding 80% with a 20% local sales tax match.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

## AGREEMENT FOR GOODS

### Villanova Bus Inspection Platform

This agreement (“Agreement”) is dated and effective as of \_\_\_\_\_, 2023, by and between the Regional Transportation Commission of Washoe County, Nevada (“RTC”) and Spika Design & Manufacturing (“Contractor”).

1. **Term.** The term of this agreement shall commence on the effective date above and shall end on August 31, 2025.
2. **Scope of Work.** Contractor shall provide the goods and services described in the scope of work attached as Exhibit A
3. **Time for Performance.** The work shall be completed [by December 31, 2023 pursuant to the schedule of deliverables attached as Exhibit A at the latest.
4. **Compensation.** RTC shall pay Contractor for the goods and services pursuant to, and in an amount not to exceed, the pricing and fee schedule attached as Exhibit B.
5. **Proceeding with Work.** Contractor shall not proceed with work until both parties have executed this Agreement and RTC has issued a purchase order. If Contractor proceeds with work before those conditions have been satisfied, Contractor shall forfeit any and all right to reimbursement and payment for work performed during that period. In the event Contractor violates this section, Contractor waives any and all claims and damages against RTC, its employees, agents, and affiliates, including but not limited to monetary damages, and any other remedy available at law or in equity arising under the terms of this Agreement.
6. **Invoices/Payment.** Contractor shall submit invoices to [accountspayable@rtcwashoe.com](mailto:accountspayable@rtcwashoe.com). RTC’s payment terms are 30 days after the receipt of the invoice. Simple interest will be paid at the rate of half a percent (0.5%) per month on all invoices approved by RTC that are not paid within thirty (30) days of receipt of the invoice.
7. **Legal/Regulatory Compliance.**
  - a. Contractor shall comply with all applicable federal, state and local government laws, regulations and ordinances. Contractor shall be responsible for obtaining all necessary permits and licenses for performance of services under this Agreement. Upon request of RTC, Contractor shall furnish RTC certificates of compliance with all such laws, orders and regulations.
  - b. Contractor represents and warrants that none of the services to be rendered pursuant to this Agreement constitute the performance of public work, as that term is defined by Section 338.010(17) of the Nevada Revised Statutes. To the extent Contractor does engage in such public work, Contractor shall be responsible for paying the prevailing wage as required by Chapter 338 of the Nevada Revised Statutes.
8. **Insurance.** Contractor shall obtain all types and amounts of insurance set forth in Exhibit C, and shall comply with all of its terms. Contractor shall not commence any work or permit any

employee/agent to commence any work until satisfactory proof has been submitted to RTC that all insurance requirements have been met.

**9. Indemnification.** Contractor's obligations are set forth in Exhibit C. Said obligation would also extend to any liability of RTC resulting from any action to clear any lien and/or to recover for damage to RTC property.

**10. Termination.**

- a. Mutual Assent. This Agreement may be terminated by mutual written agreement of the parties.
- b. Convenience. RTC may terminate this Agreement in whole or in part for convenience upon written notice to Contractor.
- c. Default. Either party may terminate this Agreement for default by providing written notice of termination, provided that the non-defaulting party must first provide written notice of default and give the defaulting party and opportunity to cure the default within a reasonable period of time.

**11. Rights, Remedies and Disputes**

- a. RTC shall have the following rights in the event that RTC deems the Contractor guilty of a breach of any term under the Agreement:
  - i. The right to take over and complete the work or any part thereof as agency for and at the expense of the Contractor, either directly or through other contractors;
  - ii. The right to cancel this Agreement as to any or all of the work yet to be performed;
  - iii. The right to specific performance, an injunction or any other appropriate equitable remedy; and
  - iv. The right to money damages.
- b. Inasmuch as the Contractor can be adequately compensated by money damages for any breach of this Agreement, which may be committed by RTC, the Contractor expressly agrees that no default, act or omission of RTC shall constitute a material breach of this Contract, entitling Contractor to cancel or rescind the Agreement (unless RTC directs Contractor to do so) or to suspend or abandon performance.
- c. Disputes arising in the performance of this Agreement that are not resolved by agreement of the parties shall be decided in writing by the authorized representative of RTC's Executive Director. This decision shall be final and conclusive unless within 10 days from the date of receipt of its copy, Contractor mails or otherwise furnishes a written appeal to RTC's Executive Director. In connection with any such appeal, Contractor shall be afforded an opportunity to be heard and to offer evidence in support of its position. The decision of RTC's Executive Director shall be binding upon the Contractor and the Contractor shall abide by the decision.
- d. Unless otherwise directed by RTC, Contractor shall continue performance under this Agreement while matters in dispute are being resolved.

**12. Warranty.** If within 2 years from the date of acceptance of installation the equipment fails due to a defect in material or workmanship, Contractor will repair or replace it, at its sole option and expense, free of charge to RTC. This limited warranty only applies to RTC and is not



transferrable. This limited warranty is in addition to other implied warranties, including the warranty of merchantability and the warranty of fitness for a particular use.

**13. Ownership of Work.** Plans, reports, studies, tracings, maps, software, electronic files, licenses, programs, equipment manuals, and databases and other documents or instruments of service prepared or obtained by Contractor in the course of performing work under this Agreement, shall be delivered to and become the property of RTC. Software already developed and purchased by Contractor prior to the execution of the Project that will be used in the Project and services rendered under this Agreement, is excluded from this requirement. Contractor and its sub-contractors shall convey and transfer all copyrightable interests, trademarks, licenses, and other intellectual property rights in such materials to RTC upon completion of all services under this Agreement and upon payment in full of all compensation due to Contractor in accordance with the terms of this Agreement. Basic survey notes, sketches, charts, computations and similar data prepared or obtained by Contractor under this Agreement shall, upon request, also be provided to RTC.

**14. Records.** Contractor will permit RTC access to any books, documents, papers and records of Contractor pertaining to this Agreement, and shall maintain such records for a period of not less than three years.

**15. Exhibits.** The exhibits to this Agreement, and any additional terms and conditions specified therein, are a material part hereof and are incorporated by reference as though fully set forth herein.

**16. Exclusive Agreement.** This Agreement constitutes the entire agreement of the parties and supersedes any prior verbal or written statements or agreements between the parties.

**17. Amendment.** No alteration, amendment or modification of this Agreement shall be effective unless it is in writing and signed by both parties.

**18. No Assignment.** Contractor shall not assign, sublease, or transfer this Agreement or any interest therein, directly or indirectly by operation of law, without the prior written consent of RTC. Any attempt to do so without the prior written consent of RTC shall be null and void, and any assignee, subleasee, or transferee shall acquire no right or interest by reason thereof.

**19. Governing Law.** This Agreement shall be construed in accordance with and governed by the laws of the State of Nevada.

**20. Venue.** Any lawsuit brought to enforce this Agreement shall be brought in the Second Judicial District Court of the State of Nevada, County of Washoe appropriate court in the State of Nevada.

**21. Attorneys' Fees.** In the event of a dispute between the parties result in a proceeding in any Court of Nevada having jurisdiction, the prevailing party shall be entitled to an award of costs and any reasonable attorneys' fees.

**22. Certification Required by Nevada Senate Bill 27 (2017).** Contractor expressly certifies and agrees, as a material part of this Agreement, that it is not currently engaged in a boycott of Israel. Contractor further agrees, as a material part of this Agreement, it will not engage in a boycott of Israel for the duration of this Agreement. If, at any time during the formation or duration of this Agreement, Contractor is engaged or engages in a boycott of Israel, it will constitute a material breach of this Agreement.

**23. Federal Clauses.** This Agreement is funded, in whole or in part, with federal funds. As a condition for receiving payment under this Agreement, Contractor agrees to comply with any and all applicable federal clauses attached as Exhibit D, and those clauses are incorporated herein by reference.

REGIONAL TRANSPORTATION  
COMMISSION OF WASHOE COUNTY

BY: \_\_\_\_\_  
Bill Thomas, AICP, Executive Director

[FULL LEGAL NAME]

BY: \_\_\_\_\_  
[Name, Title]

## ADJUSTABILITY/FLEXIBILITY

### a) Range of vertical adjustment

Spika is incorporating their standard Spika-manufactured, 24" jack into this system, which will provide a greater range of vertical travel than required. Infinite deck height adjustment between 9'6" (114") and 11'6" (138") off the ground ensures the stand can accommodate variations in bus height. The height-adjustable staircase also provides perfect positioning with the deck's height. Spika's variable-pitch staircase design ensures OSHA compliance by maintaining a consistent step interval at any height.

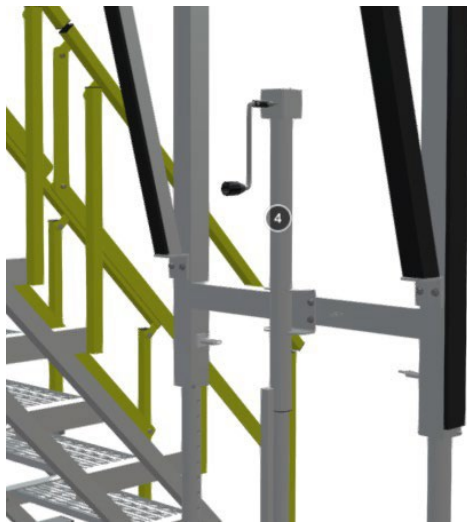


Figure 1 Manual Jack

### b) Convenience of function to adjust height

Spika's same screw jacks are located on each end of each deck with height indicators that assist users in positioning the deck evenly from side to side. The deck raises 1" for every 5 jack handle rotations. A drill attachment to increase the speed of adjustment can be purchased separately if desired. Self-lubricated bushings in the leg sets ensure reliable and smooth height adjustability without any required maintenance.

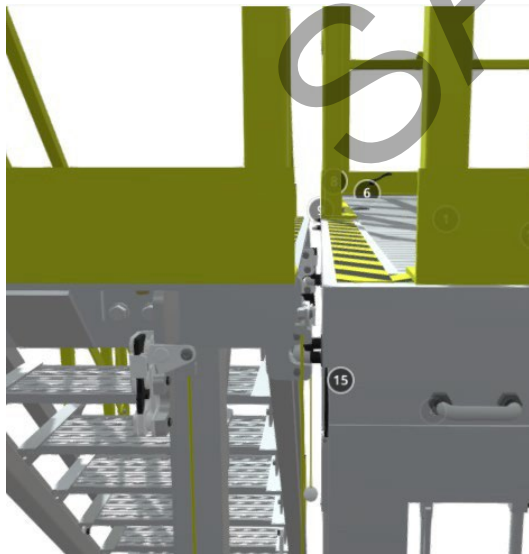


Figure 2 Quick Connect Mechanism

### c) Adjustably of other pieces/parts

The variable-pitch staircase can be attached in parallel or perpendicular position to multiple points on the deck to accommodate facility layout changes or interferences. The staircase easily locks into place on the deck via a quick-latch mechanism that automatically engages once the staircase is pushed into place. Guardrails at the staircase attachment points employ tool-free removal and attachment cam-lock mechanisms to allow for quick reconfiguration.

The end rails that cross overtop the bus are also attached with cam-locks to support quick reconfiguration for different bus lengths. A guardrail that moves with the crossover rail closes off the unused deck space when configured for shorter buses to eliminate fall risks. The crossover rail is vertically adjustable and can be pinned into position at various heights to adapt to any variances in bus height. It also includes a telescopic rail that lowers to close off the gap between the bus and top crossover rail.

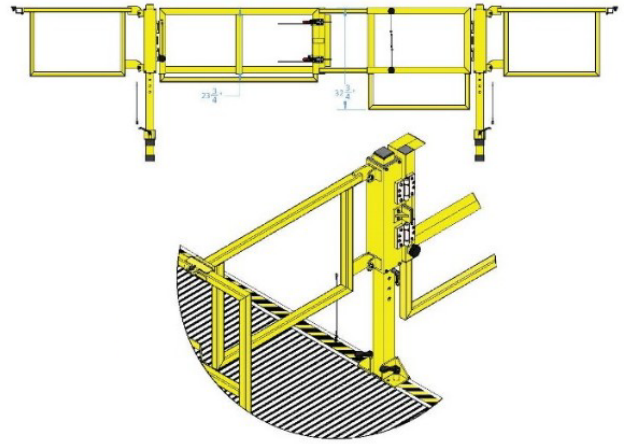


Figure 3 Crossover Rail Details

#### **d) Portability**

Each module in this system is able to be moved into position manually by 1-2 people. Premium 8" industrial dual-disc Swivel-Eze casters ensure reliable mobility with low-effort movement in any direction. The use of lightweight aluminum for all structures further supports easy portability. Each caster includes a face brake that engages when stepped on and an index lock that locks the casters into parallel or perpendicular orientation to ensure stability while in use.

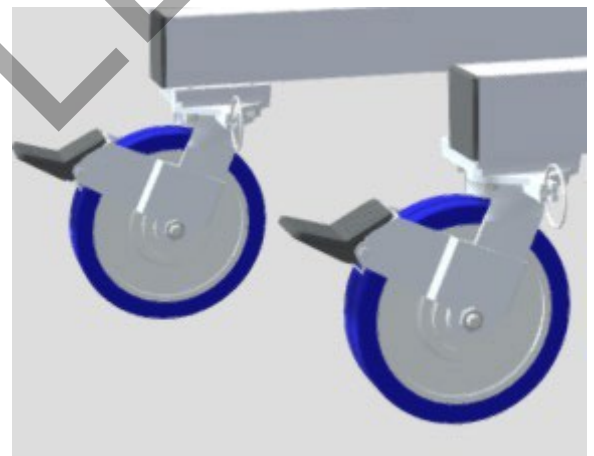


Figure 4 Casters and Brakes

**e) Ease of assembly and disassembly**

This proposal includes an onsite installation and training for initial assembly. Once installed, this system will not require assembly or disassembly, just reconfiguration. The product's users manual includes a full parts manual and provides details of operation and reconfiguration.

Tool-free attachment hardware allows for quick reconfiguration of the crossover guardrail and staircase. If this system needs to be moved away from the bus for transitions into/out of the maintenance bay, the crossover rail disconnects in the center and folds back toward the platforms to eliminate overhead obstructions. The deck modules can be quickly disconnected by removing link bars at ground level to allow them to be independently pushed out of the way. The staircase also quickly disconnects from the deck module to be independently moved out of the way.

## **GRADE OF MATERIAL**

**a) High-grade lightweight material**

Every module is constructed of lightweight aerospace-grade 5000/6000 series aluminum.

**b) Corrosion resistant**

Aluminum is an inherently corrosion-resistant material, so the deck, leg sets, and staircase are finished in bare aluminum. Safety Yellow powder coat is applied to the guardrails and handrails to mitigate aluminum dust transferring to hands while in use.

**c) Slip resistant**

The deck utilizes an extruded aluminum with gentle traction ribbing for comfortable and sure-footed use. The staircase incorporates aggressive high-traction tread to prevent slips.

## **MATERIAL FINISH**

**a) Corrosion resistant**

Aluminum is an inherently corrosion-resistant material, so the deck, leg sets, and staircase are finished in bare aluminum. Safety Yellow powder coat is applied to the guardrails and handrails to mitigate aluminum dust transferring to hands

while in use.

**b) Specify type of finish and provide information regarding durability**

The Cardinal and Sherwin brand of Safety Yellow powder coat on the guardrails is rated to have good to excellent resistance to most solvents, oils, acids, and alkalies.

## WARRANTY

**a) Main platform**

Structural components: 2 years

**b) All connection points including welds**

Structural components: 2 years

**c) Caster Wheels**

Structural components: 2 years

**d) Main platform padding**

The only padding on this platform system is the foam on the front of the decks to protect the bus from scratches or impacts while the stands are in use. Wear and tear parts, such as this foam, are not covered under this warranty.

**e) Moving parts**

Structural components: 2 years

Delivery time frame by December 31, 2023 to the address below:

2050 Villanova Dr. Reno, NV 89502

Payment: Contractor shall submit invoice after delivery to [accountspayable@rtcwashoe.com](mailto:accountspayable@rtcwashoe.com). RTC's payment terms are 30 days after the receipt of the invoice.

Exhibit B – Price Sheet

NONRECURRING CHARGES

**Item:**

Design & Engineering 111134	\$3,900.00
111134 Electric Bus Work Platform	\$144,150.00
Shipping	\$7,590.00
Total Product Cost	\$155,640.00
Options:	
Tool Tray Recommended Quantity (4) @1090.00 per tray	\$4360.00
Service Air Upgrade includes air on every deck	\$7,190.00
Service Electric Upgrade on every deck	\$14,180.00
Above Deck Lighting Upgrade includes 2 lights per deck	\$10,250.00
Below Deck Lighting Upgrade includes 2 lights per deck	\$9,160.00
Total Not to Exceed Amount	\$200,790.00

SAMPLE

**INDEMNIFICATION AND INSURANCE REQUIREMENTS FOR  
MAINTENANCE, OPERATIONS & SERVICE AGREEMENTS**

2022-03-09 Version

**1. INTRODUCTION**

IT IS HIGHLY RECOMMENDED THAT BIDDERS CONFER WITH THEIR INSURANCE CARRIERS OR BROKERS TO DETERMINE THE AVAILABILITY OF THESE INSURANCE CERTIFICATES AND ENDORSEMENTS IN ADVANCE OF BID OR PROPOSAL SUBMISSION. IF THERE ARE ANY QUESTIONS REGARDING THESE INSURANCE REQUIREMENTS, IT IS RECOMMENDED THAT THE AGENT/BROKER CONTACT RTC'S FINANCE DIRECTOR DIRECTLY AT (775) 335-1845.

**2. INDEMNIFICATION**

CONTRACTOR agrees to defend save and hold harmless and fully indemnify RTC, including their elected officials, officers, employees, and agents (hereafter, "Indemnitees") from and against any and all claims, proceedings, actions, liability and damages, including reasonable attorneys' fees and defense costs incurred in any action or proceeding (collectively "Damages") arising out of:

- A. Any breach of duty, neglect, or negligent error, misstatement, misleading statement or omission committed in the conduct of CONTRACTOR'S profession by CONTRACTOR, its employees, agents, officers, directors, Subs (as that term is defined below) , or anyone else for which CONTRACTOR may be legally responsible; and
- B. The negligent acts of CONTRACTOR, its employees, agents, officers, directors, subs, or anyone else for which CONTRACTOR is legally responsible; and
- C. The infringement of any patent or copyright resulting from the use by the Indemnitees of any equipment, part, component, or other deliverable (including software) supplied by CONTRACTOR under or as a result of this Agreement, but excluding any infringement resulting from the modification or alteration by the Indemnitees of any equipment, part, component, or other deliverable (including software) except as consented to by CONTRACTOR.

The Damages shall include, but are not limited to, those resulting from personal injury to any person, including bodily injury, sickness, disease or death and injury to real property or personal property, tangible or intangible, and the loss of use of any of that property, whether or not it is physically injured.

If the Indemnitees are involved in defending actions, CONTRACTOR shall reimburse the Indemnitees for the time spent by such personnel at the rate the Indemnitees pay for such services.

If an Indemnitee is found to be liable in the proceeding, then CONTRACTOR'S obligation here under shall be limited to the proportional share of the liability attributed to CONTRACTOR.



In determining whether a claim is subject to indemnification, the incident underlying the claim shall determine the nature of the claim.

In the event of a violation or an infringement under paragraph 2.C above and the use is enjoined, CONTRACTOR, at its sole expense, shall either (1) secure for the Indemnitees the right to continue using the materials by suspension of any injunction or by procuring a license or licenses for the Indemnitees; or (2) modify the materials so that they become non-infringing. This covenant shall survive the termination of this Agreement.

### **3. GENERAL REQUIREMENTS**

Prior to the start of any work on a RTC project, CONTRACTOR shall purchase and maintain insurance of the types and limits as described herein insuring against claims for injuries to persons or damages to property which may arise from or in connection with the performance of the work hereunder by CONTRACTOR, its Subs, or their employees, agents, or representatives. The cost of all such insurance shall be borne by CONTRACTOR.

### **4. VERIFICATION OF COVERAGE**

CONTRACTOR shall furnish RTC with a certificate(s) of insurance, executed by a duly authorized representative of each insurer, showing compliance with the insurance requirements set forth herein, on forms acceptable to RTC. All deductibles and self-insured retentions requiring RTC approval shall be shown on the certificate. All certificates and endorsements are to be addressed to RTC's Finance Director and be received by RTC before work commences. Upon request, RTC reserves the right to review complete, certified copies of all required insurance policies, including all Subs' policies. Copies of applicable policy forms or endorsements confirming required additional insured, waiver of subrogation and notice of cancellation provisions are required to be provided with any certificate(s) evidencing the required coverage.

### **5. NOTICE OF CANCELLATION**

Contractor or its insurers shall provide at least thirty (30) days' prior written notice to RTC prior to the cancellation or non-renewal of any insurance required under this Agreement. An exception may be included to provide at least ten (10) days' written notice if cancellation is due to non-payment of premium. CONTRACTOR shall be responsible to provide prior written notice to RTC as soon as practicable upon receipt of any notice of cancellation, non-renewal, reduction in required limits or other material change in the insurance required under this Agreement.

### **6. SUBCONTRACTORS & SUBCONSULTANTS**

CONTRACTOR shall include all subcontractors and subconsultants (referred to collectively as "Subs") as insureds under its liability policies OR it shall require its Subs to maintain separate liability coverages and limits of the same types specified herein. If any Subs maintain separate liability coverages and limits, each shall include the RTC, Washoe County, City of Reno and City of Sparks as additional insureds under its commercial general liability policy subject to the same requirements stated herein without requiring a written contract or agreement between each of the

additional insureds and any sub-consultant or sub-contractor. Any separate coverage limits of liability maintained by Subs shall be at least be \$1,000,000 per occurrence \$1,000,000 for any applicable coverage aggregates for or the amount customarily carried by the Sub, whichever is GREATER. If any Subs provide their own insurance with limits less than required of the Contractor, Contractor shall include Subs in their coverage up to the full limits required of the Contractor. When requested by RTC, CONTRACTOR shall furnish copies of certificates of insurance evidencing coverage for each Sub. CONTRACTOR shall require its Subs provide appropriate certificates and endorsements from their own insurance carriers naming CONTRACTOR and the Indemnitees (see paragraph 2 above) as additional insureds.

## **7. DEDUCTIBLES AND SELF-INSURED RETENTIONS**

Any deductibles or self-insured retentions that exceed \$25,000 per occurrence or claim must be declared to RTC's Finance Director prior to signing this Contract. RTC is entitled to request and receive additional documentation, financial or otherwise, prior to giving its approval of the deductibles and self-insured retentions. Any changes to the deductibles or self-insured retentions made during the term of this Contract or during the term of any policy must be declared to RTC's Finance Director prior to the change taking effect. Contractor is responsible for any losses within deductibles or self-insured retentions.

## **8. ACCEPTABILITY OF INSURERS**

Insurance is to be placed with insurers with a Best's rating of no less than A-VII and acceptable to RTC. RTC may accept coverage with carriers having lower Best's ratings upon review of financial information concerning CONTRACTOR and insurance carrier. RTC reserves the right to require that CONTRACTOR'S insurer be a licensed and admitted insurer in the State of Nevada or meet any applicable state and federal laws and regulations for non-admitted insurance placements.

## **9. MISCELLANEOUS CONDITIONS**

- A. Failure to furnish the required certificate(s) or failure to maintain the required insurance may result in termination of this Agreement at RTC's option.
- B. If CONTRACTOR fails to furnish the required certificate or fails to maintain the required insurance as set forth herein, RTC shall have the right, but not the obligation, to purchase said insurance at CONTRACTOR's expense.
- C. Any waiver of CONTRACTOR's obligation to furnish such certificate or maintain such insurance must be in writing and signed by an authorized representative of RTC. Failure of RTC to demand such certificate or other evidence of full compliance with these insurance requirements or failure of RTC to identify a deficiency from evidence that is provided shall not be construed as a waiver of CONTRACTOR's obligation to maintain such insurance, or as a waiver as to the enforcement of any of these provisions at a later date.

- D. By requiring insurance herein, RTC does not represent that coverage and limits will necessarily be adequate to protect CONTRACTOR, and such coverage and limits shall not be deemed as a limitation on CONTRACTOR's liability under the indemnities granted to RTC in this contract.
- E. If CONTRACTOR'S liability policies do not contain the standard ISO separation of insureds condition, or a substantially similar clause, they shall be endorsed to provide cross-liability coverage.

## **10. COMMERCIAL GENERAL LIABILITY**

CONTRACTOR shall maintain commercial general liability (CGL) and, if necessary, commercial umbrella insurance with a limit of not less than \$1,000,000 each occurrence. If such CGL insurance contains a general aggregate limit, it shall be increased to equal twice the required occurrence limit or revised to apply separately to this project or location.

CGL insurance shall be written on ISO occurrence form CG 00 01 04 13 (or a substitute form providing equivalent coverage) and shall cover liability arising from premises, operations, products-completed operations, personal and advertising injury, and liability assumed under an insured contract (including the tort liability of another assumed in a business contract).

There shall be no endorsement or modification of the CGL limiting the scope of coverage for liability arising from pollution, explosion, collapse, underground property damage, or damage to the named insured's work. In addition, coverage for Explosion, Collapse and Underground exposures (as applicable to the project) must be reflected in the insurance certificates.

RTC and any other Indemnitees listed in section 2. INDEMNIFICATION of this Agreement shall be included as an additional insured under the CGL, using ISO additional insured endorsement CG 20 10 07/04 or a substitute providing equivalent coverage, and under the commercial umbrella, if any.

This insurance shall apply as primary insurance with respect to any other insurance or self-insurance programs afforded to RTC or any other Indemnitees under this Agreement

The status of RTC as an additional insured under a CGL obtained in compliance with this agreement shall not restrict coverage under such CGL with respect to the escape of release of pollutants at or from a site owned or occupied by or rented or loaned to RTC.

CONTRACTOR waives all rights against RTC and any other Indemnitees listed in section 2. INDEMNIFICATION of this Agreement for recovery of damages to the extent these damages are covered by the commercial general liability or commercial umbrella liability insurance maintained pursuant to this agreement. CONTRACTOR's insurer shall endorse CGL policy to waive subrogation against RTC with respect to any loss paid under the policy.

**Continuing Completed Operations Liability Insurance.** CONTRACTOR shall maintain commercial general liability (CGL) and, if necessary, commercial umbrella liability insurance, both applicable to liability arising out of CONTRACTOR's completed operations, with a limit of not less than \$1,000,000 each occurrence for at least 5 years following substantial completion of the work.

- a. Continuing CGL insurance shall be written on ISO occurrence form CG 00 01 04 13 (or a substitute form providing equivalent coverage) and shall, at minimum, cover liability arising from products-completed operations and liability assumed under an insured contract
- b. Continuing CGL insurance shall have a products-completed operations aggregate of at least two times the each occurrence limit.
- c. Continuing commercial umbrella coverage, if any, shall include liability coverage for damage to the insured's completed work equivalent to that provided under ISO form CG 00 01.

## **11. COMMERCIAL AUTOMOBILE LIABILITY**

CONTRACTOR shall maintain automobile liability and, if necessary, commercial umbrella liability insurance with a limit of not less than \$1,000,000 each accident. Such insurance shall cover liability arising out of any auto (including owned, hired, and non-owned autos).

Coverage shall be written on ISO form CA 00 01, CA 00 05, CA 00 25, or a substitute form providing equivalent liability coverage for all owned, leased, hired (rented) and non-owned vehicles (as applicable). RTC may agree to accept auto liability for non-owned and hired (rented) vehicles under the CGL if CONTRACTOR does not own or operate any owned or leased vehicles.

CONTRACTOR waives all rights against RTC, its officers, employees and volunteers for recovery of damages to the extent these damages are covered by the automobile liability or commercial umbrella liability insurance obtained by CONTRACTOR pursuant to this Agreement.

In lieu of a separate Business Auto Liability Policy, RTC may agree to accept Auto Liability covered in the General Liability Policy, if CONTRACTOR does not have any owned or leased automobiles and non-owned and hired auto liability coverage is included.

If project involves the transport of hazardous wastes or other materials that could be considered pollutants, CONTRACTOR shall maintain pollution liability coverage equivalent to that provided under the ISO pollution liability-broadened coverage for covered autos endorsement (CA 99 48) shall be provided, and, if applicable, the Motor Carrier Act endorsement (MCS 90) shall be attached.

**Waiver of Subrogation.** CONTRACTOR waives all rights against RTC and its agents, officers, directors and employees for recovery of damages to the extent these damages are covered by the business auto liability or commercial umbrella liability insurance obtained by Contractor pursuant to this agreement.

## **12. INDUSTRIAL (WORKER'S COMPENSATION AND EMPLOYER'S LIABILITY) INSURANCE**

It is understood and agreed that there shall be no Industrial (Worker's Compensation and Employer's Liability) Insurance coverage provided for CONTRACTOR or any Sub by RTC. CONTRACTOR, and any Subs, shall procure, pay for and maintain required coverages.

CONTRACTOR shall maintain workers' compensation and employer's liability insurance meeting the statutory requirements of the State of Nevada, including but not limited to NRS 616B.627 and NRS 617.210. The employer's liability limits shall not be less than \$1,000,000 each accident for bodily injury by accident or \$1,000,000 each employee for bodily injury by disease.

Should CONTRACTOR be self-funded for Industrial Insurance, CONTRACTOR shall so notify RTC in writing prior to the signing of a Contract. RTC reserves the right to accept or reject a self-funded CONTRACTOR and to approve the amount of any self-insured retentions. CONTRACTOR agrees that RTC is entitled to obtain additional documentation, financial or otherwise, for review prior to entering into a Contract with the self-funded CONTRACTOR.

Upon completion of the project, CONTRACTOR shall, if requested by RTC, provide RTC with a Final Certificate for itself and each Sub showing that CONTRACTOR and each Sub had maintained Industrial Insurance by paying all premiums due throughout the entire course of the project.

If CONTRACTOR or Sub is a sole proprietor, coverage for the sole proprietor must be purchased and evidence of coverage must appear on the Certificate of Insurance and Final Certificate.

CONTRACTOR waives all rights against RTC, its elected officials, officers, employees and agents. for recovery of damages to the extent these damages are covered by the workers compensation and employer's liability or commercial umbrella liability insurance obtained by Tenant pursuant to this agreement. CONTRACTOR shall obtain an endorsement equivalent to WC 00 03 13 to affect this waiver.

## Exhibit D

### FTA REQUIRED CLAUSES

#### **1 - NO GOVERNMENT OBLIGATION TO THIRD PARTIES**

- A. The RTC and Contractor acknowledge and agree that, notwithstanding any concurrence by the Federal Government in or approval of the solicitation or award of the underlying Agreement, absent the express written consent by the Federal Government, the Federal Government is not a party to the Agreement and shall not be subject to any obligations or liabilities to the RTC, the Contractor, or any other party (whether or not a part to that Agreement) pertaining to any matter resulting from the underlying Agreement.
- B. The Contractor agrees to include the above clause in each subcontract financed in whole or in part with Federal assistance provided by FTA. It is further agreed that the clause shall not be modified, except to identify the subcontractor who will be subject to its provisions.

#### **2 - PROGRAM FRAUD AND FALSE OR FRAUDULENT STATEMENTS AND RELATED ACTS [49 U.S.C. § 5323(l) (1); 31 U.S.C. §§ 3801-3812; 18 U.S.C. § 1001; 49 C.F.R. part 31]**

- A. The Contractor acknowledges that the provisions of the Program Fraud Civil Remedies Act of 1986, as amended, 31 U.S.C. § 3801, et seq., and U.S. DOT regulations, "Program Fraud Civil Remedies", 49 C.F.R. Part 31, apply to its actions pertaining to the Agreement. Upon execution of the Agreement, the Contractor certifies or affirms the truthfulness and accuracy of any statement it has made, it makes, it may make, or causes to be made, pertaining to the Agreement or the FTA assisted project for which the work is being performed. In addition to other penalties that may be applicable, the Contractor further acknowledges that if it makes, or causes to be made, a false, fictitious, or fraudulent claim, statement, submission, or certification, the Federal Government reserves the right to impose the penalties of the Program Fraud Civil Remedies Act of 1986 on the Contractor to the extent the Federal Government deems appropriate.
- B. The Contractor also acknowledges that if it makes, or causes to be made, a false, fictitious, or fraudulent claim, statement, submission, or certification to the Federal Government under a contract connected with a project that is financed in whole or in part with Federal assistance originally awarded by FTA under the authority of 49 U.S.C. § 5307, the Government reserves the right to impose the penalties of 18 U.S.C. § 1001 and 49 U.S.C. § 5323(l)(1) on the Contractor, to the extent the Federal Government deems appropriate.
- C. The Contractor agrees to include the above two clauses in each subcontract financed in whole or in part with Federal assistance provided by FTA. It is further

agreed that the clauses shall not be modified, except to identify the subcontractor who will be subject to the provisions.

**3 - ACCESS TO RECORDS AND REPORTS** [49 U.S.C. § 5325(g); 2 C.F.R. § 200.333; 49 C.F.R. part 633]

The following access to records requirements apply to the Agreement:

- A. The Contractor agrees to provide the RTC, the FTA Administrator, the DOT Office of Inspector General, Comptroller General of the United States, or any of their authorized representatives access to any books, documents, papers, and records of the Contractor which are directly pertinent to the Agreement for the purposes of making audits, examinations, excerpts, and transcriptions, and as may be necessary for the RTC to meet its obligations under 2 CFR Part 200. This access includes timely and reasonable access to personnel for interviews and discussions related to the records. This right of access is not limited to the required retention period set forth in subsection C below, but continues as long as the records are retained.
- B. The Contractor agrees to permit any of the foregoing parties to reproduce by any means whatsoever or to copy excerpts and transcriptions as reasonably needed.
- C. The Contractor agrees to maintain all books, records, accounts, and reports required under the Agreement for a period of not less than three years, except in the event of litigation or settlement of claims arising from the performance of the Agreement, in which case the Contractor agrees to maintain such materials until the RTC, the FTA Administrator, the Comptroller General, or any of their duly authorized representatives, have disposed of all such litigation, appeals, claims, or exceptions related thereto. The retention period commences after the RTC makes final payment and all other pending contract matters are closed.
- D. The Contractor shall include this clause in all subcontracts and shall require all subcontractors to include the clause in their subcontracts, regardless of tier.

**4 - FEDERAL CHANGES**

The Contractor shall at all times comply with all applicable FTA regulations, policies, procedures, and directives, including without limitation those listed directly or by reference in the Master Agreement between the RTC and the FTA, as they may be amended or promulgated from time to time during the term of the Agreement. The Contractor's failure to so comply shall constitute a material breach of the Agreement.

**5 - ENERGY CONSERVATION** [42 U.S.C. 6321 et seq.; 49 C.F.R. part 622, subpart C]

The Contractor agrees to comply with the mandatory standards and policies relating to energy efficiency that are contained in the State Energy Conservation Plan issued in compliance with the Energy Policy and Conservation Act (42 U.S.C. § 6321, et seq.).

## 6 - CIVIL RIGHTS LAWS AND REGULATIONS

The Contractor agrees to comply with all applicable civil rights laws and regulations in accordance with applicable federal directives. The Contractor agrees to include these requirements in each subcontract financed in whole or in part with Federal assistance provided by FTA, modified only if necessary to identify the affected parties. These include, but are not limited to, the following:

### A. Nondiscrimination in Federal Public Transportation Programs:

Contractor shall prohibit discrimination on the basis of race, color, religion, national origin, sex (including gender identity), disability, or age. Contractor shall prohibit the (i) exclusion from participation in employment or a business opportunity for reasons identified in 49 U.S.C. § 5332; (ii) denial of program benefits in employment or a business opportunity identified in 49 U.S.C. § 5332; or (iii) discrimination identified in 49 U.S.C. § 5332, including discrimination in employment or a business opportunity. Contractor shall follow the most recent edition of Federal Transit Administration Circular 4702.1, "Title VI Requirements and Guidelines for Federal Transit Administration Recipients," to the extent consistent with applicable Federal laws, regulations, requirements, and guidance, and other applicable Federal guidance that may be issued.

### B. Nondiscrimination—Title VI of the Civil Rights Act

1. Contractor shall prohibit discrimination on the basis of race, color, or national origin.
2. Contractor shall comply with (i) Title VI of the Civil Rights Act of 1964, as amended, 42 U.S.C. § 2000d et seq.; (ii) U.S. Department of Transportation regulations, "Nondiscrimination in Federally-Assisted Programs of the Department of Transportation—Effectuation of Title VI of the Civil Rights Act of 1964," 49 CFR Part 21; and (iii) Federal transit law, specifically 49 U.S.C. § 5332.
3. Contractor shall follow (i) the most recent edition of Federal Transit Administration Circular 4702.1, "Title VI Requirements and Guidelines for Federal Transit Administration Recipients," to the extent consistent with applicable Federal laws, regulations, requirements, and guidance; (ii) U.S. Department of Justice "Guidelines for the enforcement of Title VI, Civil Rights Act of 1964," 28 CFR 50.3; and (iii) all other applicable Federal guidance that may be issued.

### C. Equal Employment Opportunity

1. Federal Requirements and Guidance. Contractor shall prohibit discrimination on the basis of race, color, religion, sex, sexual orientation, gender identity, or national origin, and (i) comply with Title VII of the Civil Rights Act of 1964, as amended, 42 U.S.C. § 2000e et seq.; (ii) facilitate compliance with Executive



Order No. 11246, "Equal Employment Opportunity" September 24, 1965, 42 U.S.C. § 2000e note, as amended by any later Executive Order that amends or supersedes it in part and is applicable to Federal assistance programs; (iii) comply with Federal transit law, specifically 49 U.S.C. § 5332; (iv) comply with Federal Transit Administration Circular 4704.1 "Equal Employment Opportunity (EEO) Requirements and Guidelines for Federal Transit Administration Recipients;" and (v) follow other Federal guidance pertaining to equal employment opportunity laws, regulations, and requirements, and prohibitions against discrimination on the basis of disability.

2. Specifics. Contractor shall ensure that applicants for employment are employed and employees are treated during employment without discrimination on the basis of their race, color, religion, national origin, disability, age, sexual orientation, gender identity, or status as a parent, as provided in Executive Order No. 11246 and by any later executive order that amends or supersedes it, and as specified by U.S. Department of Labor regulations. Contractor shall take affirmative action that includes but is not limited to (i) recruitment advertising, recruitment, and employment; (ii) rates of pay and other forms of compensation; (iii) selection for training, including apprenticeship, and upgrading; and (iv) transfers, demotions, layoffs, and terminations. Contractor recognizes that Title VII of the Civil Rights Act of 1964, as amended, exempts Indian Tribes under the definition of "Employer."
3. Equal Employment Opportunity Requirements for Construction Activities. Contractor shall comply, when undertaking "construction" as recognized by the U.S. Department of Labor, with (i) U.S. Department of Labor regulations, "Office of Federal Contract Compliance Programs, Equal Employment Opportunity, Department of Labor," 41 CFR Chapter 60; and (ii) Executive Order No. 11246, "Equal Employment Opportunity in Federal Employment," September 24, 1965, 42 U.S.C. § 2000e note, as amended by any later executive order that amends or supersedes it, referenced in 42 U.S.C. § 2000e note.

D. Nondiscrimination on the Basis of Sex:

Title IX of the Education Amendments of 1972, as amended, 20 U.S.C. § 1681 et seq. and implementing Federal regulations, "Nondiscrimination on the Basis of Sex in Education Programs or Activities Receiving Federal Financial Assistance," 49 CFR Part 25 prohibit discrimination on the basis of sex.

E. Nondiscrimination on the Basis of Age:

In accordance with section 4 of the Age Discrimination in Employment Act of 1967, as amended, 29 U.S.C. §§ 621-634; Federal transit law at 49 U.S.C. § 5332; the Age Discrimination Act of 1975, as amended, 42 U.S.C. § 6101 et seq.; 49 CFR Part 90, and 29 CFR Part 1625, Contractor agrees to refrain from discrimination for reason of age. In addition, Contractor agrees to comply with applicable Federal implementing regulations.

F. Nondiscrimination on the Basis of Disability:

In accordance with Section 504 of the Rehabilitation Act of 1973, as amended, 29 U.S.C. § 794; the Americans with Disabilities Act of 1990, as amended, 42 U.S.C.

§ 12101 et seq.; the Architectural Barriers Act of 1968, as amended, 42 U.S.C. § 4151 et seq.; and Federal transit law at 49 U.S.C. § 5332, Contractor agrees that it will not discriminate against individuals on the basis of disability. Contractor further agrees that it will comply with the requirements of U.S. Equal Employment Opportunity Commission, "Regulations to Implement the Equal Employment Provisions of the Americans with Disabilities Act," 29 CFR Part 1630, pertaining to employment of persons with disabilities. In addition, Contractor agrees to comply with applicable Federal implementing regulations.

G. Drug or Alcohol Abuse - Confidentiality and Other Civil Rights Protections:

To the extent applicable, Contractor agrees to comply with the confidentiality and civil rights protections of the Drug Abuse Office and Treatment Act of 1972, as amended, 21 U.S.C. § 1101, et seq., the Comprehensive Alcohol Abuse and Alcoholism Prevention, Treatment, and Rehabilitation Act of 1970, as amended, 42 U.S.C. § 4541, et seq., and the Public Health Service Act, as amended, 42 U.S.C. §§ 290dd-290dd-2.

H. Access to Services for Persons with Limited English Proficiency:

Contractor agrees to promote accessibility of public transportation services to persons with limited understanding of English by following Executive Order No. 13166, "Improving Access to Services for Persons with Limited English Proficiency," 42 U.S.C. § 2000d-1 note, and U.S. DOT Notice, "DOT Policy Guidance Concerning Recipients' Responsibilities to Limited English Proficiency (LEP) Persons," 70 Fed. Reg. 74087, Dec. 14, 2005.

## 7 - INCORPORATION OF FTA TERMS

The preceding provisions include, in part, certain standard terms and conditions required by DOT, whether or not expressly set forth in the preceding contract provisions. All contractual provisions required by DOT, as set forth in FTA Circular 4220.1F, and FTA's Master Agreement, are hereby incorporated by reference. Anything to the contrary herein notwithstanding, all FTA mandated terms shall be deemed to control in the event of a conflict with other provisions contained in this Agreement. The Contractor shall not perform any act, fail to perform any act, or refuse to comply with any RTC requests which would cause the RTC to be in violation of the FTA terms and conditions.

## 8 - SAFE OPERATION OF MOTOR VEHICLES [23 U.S.C. part 402; Executive Order No. 13043; Executive Order No. 13513; U.S. DOT Order No. 3902.10]

A. Seat Belt Use. Contractor is encouraged to adopt and promote on-the-job seat belt use policies and programs for its employees and other personnel that operate company-owned vehicles, company-rented vehicles, or personally operated vehicles. The terms "company-owned" and "company-leased" refer to vehicles owned or leased either by Contractor or the RTC.

B. Distracted Driving. Contractor agrees to adopt and enforce workplace safety policies to decrease crashes caused by distracted drivers, including policies to ban

text messaging while using an electronic device supplied by an employer, and driving a vehicle the driver owns or rents, a vehicle Contactor owns, leases, or rents, or a privately-owned vehicle when on official business in connection with the work performed under this Agreement.

- C. Contractor shall require the inclusion of these requirements in subcontracts of all tiers.

**9 - PROHIBITION ON CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT [2 CFR § 200.216]**

Contractor is prohibited from obligating or expending loan or grant funds to:

- A. Procure or obtain;
- B. Extend or renew a contract to procure or obtain; or
- C. Enter into a contract (or extend or renew a contract) to procure or obtain equipment, services, or systems that use covered telecommunications equipment or services as a substantial or essential component of any system, or as critical technology as part of any system. As described in Public Law 115-232, section 889, covered telecommunications equipment is telecommunications equipment produced by Huawei Technologies Company or ZTE Corporation (or any subsidiary or affiliate of such entities).
  - 1. For the purpose of public safety, security of government facilities, physical security surveillance of critical infrastructure, and other national security purposes, video surveillance and telecommunications equipment produced by Hytera Communications Corporation, Hangzhou Hikvision Digital Technology Company, or Dahua Technology Company (or any subsidiary or affiliate of such entities).
  - 2. Telecommunications or video surveillance services provided by such entities or using such equipment.
  - 3. Telecommunications or video surveillance equipment or services produced or provided by an entity that the Secretary of Defense, in consultation with the Director of the National Intelligence or the Director of the Federal Bureau of Investigation, reasonably believes to be an entity owned or controlled by, or otherwise connected to, the government of a covered foreign country.

**10 - NOTICE TO FTA AND U.S. DOT INSPECTOR GENERAL OF INFORMATION RELATED TO FRAUD, WASTE, ABUSE, OR OTHER LEGAL MATTERS [FTA Master Agreement (28), Section 39(b)]**

Notification to FTA; Flow Down Requirement. If a current or prospective legal matter that may affect the Federal Government emerges, Contractor must promptly notify RTC, which will promptly notify the FTA Chief Counsel and FTA Regional Counsel for the Region in which RTC is located. Contractor must include an equivalent provision in its

sub-agreements at every tier, for any agreement that is a “covered transaction” according to 2 C.F.R. §§ 180.220 and 1200.220.

- A. The types of legal matters that require notification include, but are not limited to, a major dispute, breach, default, litigation, or naming the Federal Government as a party to litigation or a legal disagreement in any forum for any reason.
- B. Matters that may affect the Federal Government include, but are not limited to, the Federal Government’s interests in the Award, the accompanying Underlying Agreement, and any Amendments thereto, or the Federal Government’s administration or enforcement of federal laws, regulations, and requirements.
- C. Additional Notice to U.S. DOT Inspector General. Contractor must promptly notify RTC, which will promptly notify the U.S. DOT Inspector General in addition to the FTA Chief Counsel or Regional Counsel for the Region in which RTC is located, if Contractor has knowledge of potential fraud, waste, or abuse occurring on a project receiving assistance from FTA. The notification provision applies if a person has or may have submitted a false claim under the False Claims Act, 31 U.S.C. § 3729, et seq., or has or may have committed a criminal or civil violation of law pertaining to such matters as fraud, conflict of interest, bid rigging, misappropriation or embezzlement, bribery, gratuity, or similar misconduct involving federal assistance. This responsibility occurs whether the project is subject to this Agreement or another agreement involving a principal, officer, employee, agent, or Third Party Participant of Contractor. It also applies to subcontractors at any tier. Knowledge, as used in this paragraph, includes, but is not limited to, knowledge of a criminal or civil investigation by a Federal, state, or local law enforcement or other investigative agency, a criminal indictment or civil complaint, or probable cause that could support a criminal indictment, or any other credible information in the possession of Contractor. In this paragraph, “promptly” means to refer information without delay and without change. This notification provision applies to all divisions of Contractor, including divisions tasked with law enforcement or investigatory functions.

**11 - GOVERNMENT-WIDE DEBARMENT AND SUSPENSION** [2 C.F.R. part 180; 2 C.F.R part 1200; 2 C.F.R. § 200.213; 2 C.F.R. part 200 Appendix II (I); Executive Order 12549; Executive Order 12689]

- A. Contractor shall comply and facilitate compliance with U.S. Department of Transportation regulations, “Non-procurement Suspension and Debarment,” 2 CFR Part 1200, which adopts and supplements the U.S. Office of Management and Budget “Guidelines to Agencies on Government wide Debarment and Suspension (Non-procurement),” 2 CFR Part 180. These provisions apply to each contract at any tier of \$25,000 or more, and to each contract at any tier for a federally required audit (irrespective of the contract amount), and to each contract at any tier that must be approved by a Federal Transit Administration official irrespective of the contract amount. As such, Contractor shall verify that its principals, affiliates, and subcontractors are eligible to participate in this federally

funded contract and are not presently declared by any Federal department or agency to be:

1. Debarred from participation in any federally assisted award;
2. Suspended from participation in any federally assisted award;
3. Proposed for debarment from participation in any federally assisted award;
4. Declared ineligible to participate in any federally assisted award;
5. Voluntarily excluded from participation in any federally assisted award; or
6. Disqualified from participation in any federally assisted award.

B. Contractor certifies that it and/or its principals, affiliates, and subcontractors are not currently debarred or suspended. Contractor shall promptly inform the RTC of any change in the suspension or debarment status of Contractor or its principals, affiliates, and subcontractors during the term of the Agreement. Further, Contractor shall include a provision requiring compliance with the requirements of 2 CFR Part 180, Subpart C, as supplemented by 2 CFR Part 1200 in its lower-tier covered transactions.

C. The certification in this clause is a material representation of fact relied upon by RTC. If it is later determined by the RTC that Contractor knowingly rendered an erroneous certification, in addition to remedies available to the RTC, the Federal Government may pursue available remedies, including but not limited to suspension and/or debarment.

D. Contractor agrees to comply with the requirements of 2 CFR Part 180, Subpart C, as supplemented by 2 CFR Part 1200, throughout the term of the Agreement.

**12 - LOBBYING RESTRICTIONS** [31 U.S.C. § 1352; 2 C.F.R. § 200.450; 2 C.F.R. part 200 appendix II (J); 49 C.F.R. part 20]

Contractors who apply or bid for an award of \$100,000 or more shall file the certification required by 49 C.F.R. Part 20, "New Restrictions on Lobbying." Each tier certifies to the tier above that it will not and has not used Federal appropriated funds to pay any person or organization for influencing or attempting to influence an officer or employee of any agency, a member of Congress, officer or employee of Congress, or an employee of a member of Congress in connection with obtaining any Federal contract, grant, or any other award covered by 31 U.S.C. § 1352. Each tier shall also disclose the name of any registrant under the Lobbying Disclosure Act of 1995 who has made lobbying contacts on its behalf with non-Federal funds with respect to that Federal contract, grant or award covered by 31 U.S.C. § 1352. Such disclosures are forwarded from tier to tier up to the RTC.

**13 - CONTRACT WORK HOURS AND SAFETY STANDARDS ACT—NON-CONSTRUCTION**

A. Contractor shall comply with all Federal laws, regulations, and requirements providing wage and hour protections for non-construction employees, in

accordance with 40 U.S.C. § 3702, Contract Work Hours and Safety Standards Act, and other relevant parts of that Act, 40 U.S.C. § 3701 et seq., and U.S. Department of Labor regulations, "Labor Standards Provisions Applicable to Contracts Covering Federally Financed and Assisted Construction (also Labor Standards Provisions Applicable to Non-construction Contracts Subject to the Contract Work Hours and Safety Standards Act)," 29 CFR Part 5.

- B. Contractor shall maintain payrolls and basic payroll records during the course of the work and shall preserve them for a period of three (3) years from the completion of the Agreement for all laborers and mechanics, including guards and watchmen, working on the Agreement. Such records shall contain the name and address of each such employee, social security number, correct classifications, hourly rates of wages paid, daily and weekly number of hours worked, deductions made, and actual wages paid.
- C. Such records maintained under this section shall be made available by Contractor for inspection, copying, or transcription by authorized representatives of the Federal Transit Administration and the U.S. Department of Labor, and Contractor will permit such representatives to interview employees during working hours on the job.
- D. Contractor shall require the inclusion of the language of this section in subcontracts of all tiers.

**14 - CLEAN WATER REQUIREMENTS** [33 U.S.C. §§ 1251-1387; 2 C.F.R. part 200, Appendix II (G)]

- A. The Contractor agrees to comply with all applicable standards, orders, or regulations issued pursuant to the Federal Water Pollution Control Act, as amended, 33 U.S.C. § 1251, et seq. The Contractor agrees to report each violation to the RTC and understands and acknowledges that the RTC will, in turn, report each violation as required to assure notification to FTA and the appropriate EPA Regional Office.
- B. The Contractor also agrees to include these requirements in each subcontract exceeding \$150,000 financed in whole or in part with Federal assistance provided by FTA.

**15 - CLEAN AIR ACT** [42 U.S.C. §§ 7401 – 7671q; 2 C.F.R. part 200, Appendix II (G)]

- A. The Contractor agrees to comply with all applicable standards, orders or regulations issued pursuant to the Clean Air Act, as amended, 42 U.S.C. §§ 7401, et seq. The Contractor agrees to report each violation to the RTC and understands and agrees that the RTC will, in turn, report each violation as required to assure notification to FTA and the appropriate EPA Regional Office.

- B. The Contractor also agrees to include these requirements in each subcontract exceeding \$150,000 financed in whole or in part with Federal assistance provided by FTA

**16 - BUY AMERICA** [49 U.S.C. 5323(j); 49 C.F.R. part 661]

Contractor agrees to comply with 49 U.S.C. § 5323(j) and 49 CFR Part 661, which state that Federal funds may not be obligated unless all steel, iron, and manufactured products used in Federal Transit Administration-funded projects are produced in the United States, unless a waiver has been granted by the Federal Transit Administration or the product is subject to a general waiver. General waivers are listed in 49 CFR 661.7. Separate requirements for rolling stock are set out at 49 U.S.C. § 5323(j)(2)(C) and 49 CFR 661.11. The Contractor shall be responsible for providing any required Buy America certifications under such regulations.

SAMPLE



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.5.2

To: Regional Transportation Commission

From: Mark Maloney, Director of Transportation and Operations

**SUBJECT: ED Pass Program**

**RECOMMENDED ACTION**

Approve an Interlocal Cooperative Agreement (ICA) with the Nevada Department of Transportation to provide funding to the RTC for the ED Pass Program in the amount of \$350,000.

**BACKGROUND AND DISCUSSION**

The ED Pass Program allows all University of Nevada, Reno and Truckee Meadows Community College students, faculty and staff to ride all RTC fixed route services by showing the driver their school ID. Since its inception, ridership at both schools has increased. Ridership at UNR during FY23 (98,883 trips) nearly doubled from FY22 (53,651 trips). TMCC had its second highest ridership year in FY23 with 50,103 trips. Approval of the Cooperative Agreement (ICA) with the Nevada Department of Transportation will allow the RTC to receive funding for the ED Pass Program. The current Transportation Act provides funding for all modes of transportation under which the ED Pass Program is eligible for ninety-five percent (95%) federal funds and five percent (5%) RTC funds. This agreement authorizes the state to utilize \$350,000 in CMAQ funds to reimburse the RTC for 95% of program related expenses. The remaining 5% of the expenses are paid by the RTC in the amount of \$18,420 as the required local sales tax match for these federal funds. The project is included in the current Regional Transportation Improvement Program (RTIP).

**FISCAL IMPACT**

The reimbursable not to exceed amount is \$350,000, and the local sales tax required match is \$18,420. Funding for the ED Pass Program is included in the FY2024 Board Approved Budget.

**PREVIOUS BOARD ACTION**

7/15/2022      Approved the Interlocal Cooperative Agreement to provide funding to the RTC for implementation of the ED Pass Program.



COOPERATIVE AGREEMENT

This Agreement is made and entered into on \_\_\_\_\_, by and between the State of Nevada, acting by and through its Department of Transportation, hereinafter called the "DEPARTMENT", and Regional Transportation Commission of Washoe County, 1105 Terminal Way, Reno, NV 89502, hereinafter called the "RTC".

WITNESSETH:

WHEREAS, a Cooperative Agreement is defined pursuant to Nevada Revised Statutes (NRS) 277.110 as an agreement between two or more public agencies for the joint exercise of powers, privileges, and authority; and

WHEREAS, pursuant to the provisions contained in Chapter 408 of the Nevada Revised Statutes, the Director of the DEPARTMENT may enter into those agreements necessary to carry out the provisions of the Chapter; and

WHEREAS, NRS 277.110 authorizes any two or more public agencies to enter into agreements for joint or cooperative action; and

WHEREAS, the parties to this Agreement are public agencies and authorized to enter into agreements in accordance with NRS 277.080 to 277.110, inclusive; and

WHEREAS, the purpose of this Agreement is to provide funding to the RTC for the implementation of the ED PASS Grant Program which will provide transit passes to all University of Nevada, Reno (UNR) and Truckee Meadows Community College (TMCC) students, faculty and staff,, hereinafter called the "PROJECT;" and

WHEREAS, the current Transportation Act provides funding for all modes of transportation under which this PROJECT is eligible for ninety-five percent (95%) federal funds and five percent (5%) matching funds; and

WHEREAS, the PROJECT has been approved for Federal Congestion Mitigation and Air Quality (CMAQ) funds, Catalog of Federal Domestic Assistance (CFDA) Number CFDA 20.205; and

WHEREAS, the ED PASS Program to be provided by the RTC shall be of benefit to the DEPARTMENT, the RTC, and to the people of the State of Nevada; and

WHEREAS, the parties hereto are willing and able to perform the services described herein.

NOW, THEREFORE, in consideration of the premises and of the mutual covenants herein contained, it is agreed as follows:

ARTICLE I - RTC AGREES

1. To provide the DEPARTMENT with an analysis of student, faculty, and staff commuting needs, program design and implementation, ongoing program evaluation and consulting, bus pass subsidy program newsletters and mass mailings, print/radio/TV promotion, community education programs, student and university target marketing, flex-fare transit programs, and coordinated transportation programs.

2. To bill the DEPARTMENT upon completion of the PROJECT for actual PROJECT costs (with supporting documentation) of which ninety-five percent (95%) of CMAQ funds of the eligible PROJECT costs will be reimbursed not to exceed Three Hundred Fifty Thousand and No/100 Dollars (\$350,000.00) per year. The RTC is responsible for the five percent (5%) match of Eighteen Thousand Four Hundred Twenty and No/100 Dollars (\$18,420.00) per year. The RTC shall provide supporting documentation to be audited to confirm that work performed conforms to DEPARTMENT and Federal Highway Administration guidelines.

3. During the performance of this Agreement, the RTC, for itself, its assignees, and successors in interest agrees as follows:

a. Compliance with Regulations: The RTC shall comply with all of the regulations relative to nondiscrimination in federally-assisted programs of 49 CFR Part 21 as they may be amended from time to time (hereinafter "Regulations"), which are herein incorporated by reference and made a part of this Agreement.

b. Nondiscrimination: The RTC, with regard to the professional services performed by it during the Agreement, shall not discriminate on the grounds of race, color, religion, sex, sexual orientation, gender identity or expression, age, disability or national origin in the selection and retention of subcontractors, including procurement of materials and leases of equipment. The RTC shall not participate either directly or indirectly in the discrimination prohibited by Section 21.5 of the Regulations, including employment practices, when this Agreement covers a program set forth in Appendix B of the Regulations.

c. Solicitations for Subcontracts, Including Procurement of Materials, and Equipment: In all solicitations either by competitive bidding or negotiation made by the RTC for professional services to be performed under a subcontract, including procurement of materials or leases of equipment, each potential subcontractor or supplier shall be notified by the RTC of the subcontractor's obligations under this Agreement and the Regulations relative to nondiscrimination on the grounds of race, color, religion, sex, sexual orientation, gender identity or expression, age, disability or national origin.

d. Information and Reports: The RTC shall provide all information and reports required by the Regulations, or directives issued pursuant thereto, and shall permit access to its facilities as may be determined by the DEPARTMENT or the Federal Highway Administration (FHWA) to be pertinent to ascertain compliance with such Regulations or directives. Where any information required of the RTC is in the exclusive possession of another who fails or refuses to furnish this information, the RTC shall so certify to the DEPARTMENT, or the FHWA as appropriate, and shall set forth what efforts it has made to obtain the information.

e. Sanctions for Noncompliance: In the event of the RTC's noncompliance with the nondiscrimination provisions of this Agreement, the DEPARTMENT shall impose such Agreement sanctions as it or the FHWA may determine to be appropriate, including, but not limited to:

1. Withholding of payments to the RTC under this Agreement until the RTC complies, and/or

2. Cancellation, termination or suspension of this Agreement, in whole or in part.

f. Agreements with subcontractors shall include provisions making all subcontractor records available for audit by the DEPARTMENT and/or the FHWA.

g. Incorporation of Provisions: The RTC shall include the provisions of Paragraphs (a) through (f) above in every subcontract, including procurement of materials and leases of equipment, unless exempt by Regulations, order, or instructions issued pursuant thereto. The RTC shall take such action with respect to any subcontract or procurement as the DEPARTMENT or the FHWA may direct as a means of enforcing such provisions, including sanctions for non-compliance. In the event the RTC becomes involved in, or is threatened with, litigation by a subcontractor or supplier as a result of such direction, the RTC may request the DEPARTMENT to enter into such litigation to protect the interests of the DEPARTMENT, and the RTC may request the United States to enter into such litigation to protect the interests of the United States.

## ARTICLE II - DEPARTMENT AGREES

1. To fund ninety-five percent (95%) of the PROJECT with CMAQ funds, estimated to be and not to exceed Three Hundred Fifty Thousand and No/100 Dollars (\$350,000.00) per year.
2. To establish and maintain a budget for the ED Pass Program PROJECT. This budget shall be maintained by the Multimodal and Program Development (MPD) Division of the DEPARTMENT, and all invoices shall be submitted to the Multimodal and Program Development (MPD) Division for approval and reimbursement.

## ARTICLE III - IT IS MUTUALLY AGREED

1. The term of this Agreement shall be from the date first written above through and including September 30, 2024.
2. This Agreement shall not become effective until and unless approved by appropriate official action of the governing body of each party.
3. This Agreement may be terminated by either party prior to the date set forth above, provided that a termination shall not be effective until thirty (30) calendar days after a party has served written notice upon the other party. This Agreement may be terminated by mutual consent of both parties or unilaterally by either party without cause. The parties expressly agree that this Agreement shall be terminated immediately if for any reason Federal and/or State Legislature funding ability to satisfy this Agreement is withdrawn, limited, or impaired.
4. All notices or other communications required or permitted to be given under this Agreement shall be in writing and shall be deemed to have been duly given if delivered personally in hand, by telephonic facsimile or electronic mail with simultaneous regular mail, or mailed certified mail, return receipt requested, postage prepaid on the date posted, and addressed to the other party at the address set forth below:

FOR DEPARTMENT:

Tracy Larkin Thomason, P.E., Director  
Attn.: Sondra Rosenberg, Assistant Director, Planning  
Nevada Department of Transportation  
Division: Multimodal and Program Development  
1263 South Stewart Street  
Carson City, Nevada 89712  
Phone: 775-888-7440  
Fax: 775-888-7202  
Email: [srosenberg@dot.nv.gov](mailto:srosenberg@dot.nv.gov)

FOR RTC:

William A. Thomas, Executive Director  
Regional Transportation Commission of Washoe County  
1105 Terminal Way  
Reno, NV 89502  
Phone: (775) 335-1826  
Fax: (775) 348-3240  
Email: [bthomas@rtcwashoe.com](mailto:bthomas@rtcwashoe.com)

5. The RTC's match will be calculated as the applicable percentage of the total PROJECT costs eligible for Federal funding, plus all costs not eligible for Federal funding.

6. Should this Agreement be terminated by the RTC prior to completion of the PROJECT, the RTC shall reimburse the DEPARTMENT for all improvement costs incurred up to the point of Agreement termination and all costs incurred by the DEPARTMENT because of this Agreement's termination.

7. The RTC agrees to pay actual PROJECT costs whether they be greater than or less than the estimates shown herein.

8. The RTC shall ensure that any reports, materials, studies, photographs, negatives, drawings or other documents prepared in the performance obligations under this Agreement shall be the exclusive, joint property of the RTC and the DEPARTMENT. The RTC shall ensure any subconsultant shall not use, willingly allow or cause to have such documents used for any purpose other than performance of obligations under this Agreement without the written consent of both the RTC and the DEPARTMENT. The RTC shall not utilize (and shall ensure any subconsultant shall not utilize) any materials, information, or data obtained as a result of performance of this Agreement in any commercial or academic publication or presentation without the express written permission of the DEPARTMENT. The RTC (and any subconsultant) shall not reference an opinion of an employee or agent of the DEPARTMENT obtained as a result of performance of this Agreement in any publication or presentation without the written permission of the employee or agent to whom the opinion is attributed, in addition to the written permission of the DEPARTMENT.

9. Neither party shall be deemed to be in violation of this Agreement if it is prevented from performing any of its obligations hereunder due to strikes, failure of public transportation, civil or military authority, act of public enemy, accidents, fires, explosions, or acts of God, including, without limitations, earthquakes, floods, winds or storms. In such an event, the intervening cause must not be through the fault of the party asserting such an excuse, and the excused party is obligated to promptly perform in accordance with the terms of the Agreement after the intervening cause ceases.

10. To the fullest extent of NRS Chapter 41 liability limitations, each party shall indemnify, hold harmless, and defend, not excluding the other's right to participate, the other from and against all liability, claims, actions, damages, losses, and expenses, including, but not limited to, reasonable attorneys' fees and costs, caused by the negligence, errors, omissions, recklessness, or intentional misconduct of its own officers, employees, and agents. Such obligation shall not be construed to negate, abridge, or otherwise reduce any other right or obligation of indemnity which would otherwise exist as to any party or person described herein. This indemnification obligation is conditioned upon the performance of the duty of the party seeking indemnification (indemnified party) to serve the other party (indemnifying party) with written notice of an actual or pending claim, within thirty (30) calendar days of the indemnified party's notice of such actual or pending claim or cause of action. The indemnifying party shall not

be liable for reimbursement of any attorney's fees and costs incurred by the indemnified party due to said party exercising its right to participate with legal counsel.

11. The parties do not waive and intend to assert available NRS Chapter 41 liability limitations in all cases. Agreement liability of both parties shall not be subject to punitive damages. Actual damages for any DEPARTMENT breach shall never exceed the amount of funds which have been appropriated for payment under this Agreement, but not yet paid, for the fiscal year budget in existence at the time of the breach.

12. Failure to declare a breach or the actual waiver of any particular breach of this Agreement or any of its material or nonmaterial terms by either party shall not operate as a waiver by such party of any of its rights or remedies as to any other breach, including a breach of the same term.

13. An alteration ordered by the DEPARTMENT, which substantially changes the services provided for by the expressed intent of this Agreement shall be considered extra work and shall be specified in a written amendment which shall set forth the nature and scope thereof. The method of payment for extra work shall be specified at the time the amendment is written.

14. This Agreement and the rights and obligations of the parties hereto shall be governed by, and construed according to, the laws of the State of Nevada. The parties consent to the exclusive jurisdiction of the Nevada state district courts for enforcement of this Agreement.

15. The illegality or invalidity of any provision or portion of this Agreement shall not affect the validity of the remainder of the Agreement, and this Agreement shall be construed as if such provision did not exist. The unenforceability of such provision shall not be held to render any other provision or provisions of this Agreement unenforceable.

16. Except as otherwise expressly provided within this Agreement, all or any property presently owned by either party shall remain in such ownership upon termination of this Agreement, and there shall be no transfer of property between the parties during the course of this Agreement.

17. It is specifically agreed between the parties executing this Agreement that it is not intended by any of the provisions of any part of this Agreement to create in the public or any member thereof a third party beneficiary status hereunder or to authorize anyone not a party to this Agreement to maintain a suit for personal injuries or property damage pursuant to the terms or provisions of this Agreement.

18. Each party agrees to keep and maintain under generally accepted accounting principles full, true, and complete records and documents pertaining to this Agreement and present, at any reasonable time, such information for inspection, examination, review, audit, and copying at any office where such records and documentation are maintained. Such records and documentation shall be maintained for three (3) years after final payment is made.

19. The parties are associated with each other only for the purposes and to the extent set forth in this Agreement. Each party is, and shall be, a public agency separate and distinct from the other party and shall have the right to supervise, manage, operate, control, and direct performance of the details incident to its duties under this Agreement. Nothing contained in this Agreement shall be deemed or construed to create a partnership or joint venture, to create relationships of an employer-employee or principal-agent, or to otherwise create any liability for one agency whatsoever with respect to the indebtedness, liabilities, and obligations of the other agency or any other party.

20. Neither party shall assign, transfer or delegate any rights, obligations, or duties under this Agreement without the prior written consent of the other party.

21. The parties hereto represent and warrant that the person executing this Agreement on behalf of each party has full power and authority to enter into this Agreement and that the parties are authorized by law to engage in the cooperative action set forth herein.

22. Pursuant to NRS 239 information or documents may be open to public inspection and copying. The parties shall have the duty to disclose unless a particular record is confidential by law or a common law balancing of interests.

23. Each party shall keep confidential all information, in whatever form, produced, prepared, observed, or received by that party to the extent that such information is confidential by law or otherwise required to be kept confidential by this Agreement.

24. This Agreement shall not become effective until and unless approved by appropriate official action of the governing body of each party.

25. This Agreement constitutes the entire agreement of the parties and such is intended as a complete and exclusive statement of the promises, representations, negotiations, discussions, and other agreements that may have been made in connection with the subject matter hereof. Unless an integrated attachment to this Agreement specifically displays a mutual intent to amend a particular part of this Agreement, general conflicts in language between any such attachment and this Agreement shall be construed consistent with the terms of this Agreement. Unless otherwise expressly authorized by the terms of this Agreement, no modification or amendment to this Agreement shall be binding upon the parties unless the same is in writing and signed by the respective parties hereto and approved by the Attorney General.

IN WITNESS WHEREOF, the parties have executed this Agreement on the day and year first above written.

RTC of Washoe County

State of Nevada, acting by and through its  
DEPARTMENT OF TRANSPORTATION

\_\_\_\_\_  
William A. Thomas, AICP, Executive Director

\_\_\_\_\_  
Tracy Larkin Thomason, P.E., Director

Approved as to Legality and Form:

DocuSigned by:  
*Shane Chesney*  
\_\_\_\_\_  
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Deputy Attorney General



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 4.6.1

To: Regional Transportation Commission

From: Bill Thomas, AICP, Executive Director

**SUBJECT: Quarterly RTC Agency Goals and Strategic Roadmap Report**

**RECOMMENDED ACTION**

Acknowledge receipt of a report regarding quarterly progress on RTC Agency Goals and Strategic Roadmap - FY 2023 (Q4).

**BACKGROUND AND DISCUSSION**

The RTC Strategic Roadmap outlines workplans for staff to accomplish the Board's direction. These workplans include detailed outcomes, champions, approaches, objectives, and measures of success for all goals. The Board also sets performance goals for the Agency on an annual basis. These goals are aligned with the Strategic Roadmap and reflect the Board's priorities for the Agency. When rating the Agency's performance annually, the Board is presented with a summary of the status of the agency goals and accomplishments. When approving the most recent Strategic Roadmap and Agency Goals (FY24), the Board requested quarterly status updates on both items. This is the first requested update and includes the status of RTC Agency Goals and Strategic Roadmap for FY 2023 (Q4).

**FISCAL IMPACT**

Funding for this item is included in the approved FY 2024 budget, and there is no additional cost in connection with this agenda item.

**PREVIOUS BOARD ACTION**

- 06/17/2022 Acknowledged receipt of the RTC Strategic Roadmap and approved the RTC Goals for FY 2023.
  - 06/16/2023 Acknowledged receipt of the RTC Strategic Roadmap and approved the RTC Goals for FY 2024.
-

# RTC Organizational Performance Scorecard (FY23-Q4)

## Strategic Roadmap Goals

Goal	Re-Build Public Transportation	YTD Progress	Status
1	Rethink the Transportation System		
	Fixed-Route On-Time Performance - 85% of departures at timepoints within 1 minute early and 5 min late	89%	Achieved
	ACCESS On-Time Performance - 85% of departures at timepoints within 1 minute early and 5 min late	88%	Achieved
	FlexRIDE On-Time Performance - 85% of departures at timepoints within 1 minute early and 5 min late	94%	Achieved
	Fixed-Route Customer Service - Less than 1 valid complaints per 20,000 boardings	1.09	Off Target
	ACCESS Customer Service - Less than 1 valid complaints per 1,000 boardings	0.56	Achieved
	FlexRIDE Customer Service - Average 90% or higher positive rating	96%	Achieved
	Fixed-Route Service Productivity - 18 or more boardings per revenue hour (increasing annually to 30 boardings by FY26-27)	19.07	Achieved
	ACCESS Service Productivity - 2.3 or more boardings per revenue hour	1.78	Off Target
	FlexRIDE Service Productivity - 2.3 or more boardings per revenue hour	2.88	Achieved
	Fixed-Route Public Transportation GHG Emission Reductions	13,439,248	KPI
	VanPOOL Public Transportation GHG Emission Reductions	19,704,353	KPI
2	Healthy Transportation Services	100%	Achieved
Goal	Enhance RTC s Role in Anticipating and Meeting Future Transportation Needs	YTD Progress	Status
1	New Roads: Identify, analyze, and potentially re-define relationships and responsibilities among RTC, NDOT, and local jurisdictions.	100%	Achieved
2	Improvements/Expansions to Existing Roads: Identify, analyze, and potentially re-define relationships and responsibilities among RTC, NDOT, and local jurisdictions.	100%	Achieved
3	Maintenance of Existing Roads: Identify, analyze, and potentially re-define relationships and responsibilities among RTC, NDOT, and local jurisdictions.	100%	Achieved
KPI	KPI: % of roadway categories verified	100%	Achieved
KPI	KPI: % of funding sources analyzed	100%	Achieved
KPI	KPI: % of regional roadway lane miles identified	100%	Achieved
Goal	Improve our Community s Network Experience	YTD Progress	Status
1	#1. RTP Process: Enhance data-based decision-making for prioritizing mobility solutions using the RTP.	100%	Achieved
2	#2 RTIP: Commit to implementing federally funded projects listed in the RTIP and prioritize these projects when applying for discretionary funding opportunities. (Discretionary \$'s Applied Toward Fed Priorities)	\$83,955,429.82	Achieved
3	#3 RTP Project Prioritization: Refine project prioritization framework to better inform the public, local partners, and the Board the choices made when selecting projects.	1	Achieved
KPI	KPI: Number of RTIP Projects under construction within RTIP timeframe	5	Achieved
KPI	KPI: Number of Public Engagement Interactions	20	Achieved
Goal	Financial & Organizational Stewardship	YTD Progress	Status
1	Capital Efficiency: Maximize the value RTC can create through funding sources fine-tuned financial modeling	100%	Achieved
2	Project Funding: Ensure funding is available to build projects when the community needs them in the most efficient way possible with the resources available to us.	100%	Achieved
3	Long-Range Planning: Expand the use of the long-range financial model be used as a decision-making tool for the agency so that capital expenditures stay within 5% of budgeted amount.	-54%	Off Target



# RTC Organizational Performance Scorecard (FY23-Q4)

## Agency Goals

Goal #	Engineering Department Priorities	YTD Progress	Status	Board Priority
1	Begin Design for Projects (Keystone Ave, Pembroke Dr., Sierra St. S. Virginia St.)	100%	Achieved	
2	Begin Project Construction (2023 Pavement, Sky Vista Pkwy, Sparks Blvd., W. 4th St.)	100%	Achieved	
3	Complete Project Construction (2022 Pavement, Lemon Dr., Roundabouts, S. River Path, Virginia St.)	100%	Achieved	
4	Complete ITS Strategic Master Plan to identify how to operate our system as a whole.	100%	Achieved	
5	Continue to improve on data reporting for the Engineering Department focused on the e-builder tool and project management consistency. Produce clear/concise reports that can improve communication and transparency.	100%	Achieved	
6	Increase partnership with other agencies to complete more projects with less resources (NDOT/County/City of Sparks/City of Reno) considering the discretionary federal grants coming through the infrastructure investment Jobs Act to maximize federal investment into the regional road system. (Federal Funds Spent on S&H Projects)	\$2,117,727.43	Achieved	Goal 2
7	Initiate preliminary alignment study of Butch Cassidy Drive.	100%	Achieved	
8	Update Green Book	33%	Off Target	
Goal #	Planning Department Priorities	YTD Progress	Status	Board Priority
9	Complete McCarran Blvd Corridor Study, Travel Demand Model Expansion and Verdi Area Transportation Study	98%	Off Target	
10	Coordinate with NDOT to prioritize and fund a project(s) identified in the Mt. Rose Corridor and study for near-term implementation.	100%	Achieved	Goal 2
11	Develop scope for Household Travel Survey.	100%	Achieved	
12	Initiate Active Transport Update and Virginia St TOD Plan	100%	Achieved	Goal 1
13	Initiate collaborative safety audit with City of Reno to assess and address pedestrian safety and vehicle needs on Wells (south of Mill).	100%	Achieved	
14	Update: Active transportation count program.	100%	Achieved	
Goal #	Public Transportation Department Priorities	YTD Progress	Status	Board Priority
15	Address barriers to opportunity by increasing the availability of public transit through implementation of our TOPS service recommendations, as measured by population with transit service, jobs accessible through public transit, and an increase in RTC passenger miles		Achieved	Goal 1
16	Begin implementation of the technology recommendations contained in TOPS to improve the customer experience.	100%	Achieved	Goal 1
17	Complete the design of a hydrogen maintenance/fueling facility to be located at RTC's Sutro location. Begin procurement of fueling infrastructure.	100%	Achieved	
18	Complete the "Digital Twin Paradigm for Real Time Infrastructure Maintenance" project with the University of Nevada, Reno to demonstrate an advanced technology automated method of collecting maintenance data at the bus stop level.	100%	Achieved	
19	Implement and complete construction on RTC facilities projects	100%	Achieved	
20	Increase the Van Pool program to 380 vehicles	92%	Off Target	
21	Replace 15 ACCESS/FlexRIDE CNG paratransit buses and 6 RIDE Electric Hybrid fixed-route buses	100%	Achieved	
Goal #	Executive Department Priorities	YTD Progress	Status	Board Priority
22	Align Government Affairs Officer and Public Information Officer within the Executive Department to provide increased engagement throughout the Agency.	100%	Achieved	
23	Clarify and re-define relationships and responsibilities for both roadway maintenance and project leadership among regional partners, including NDOT (# of NDOT Outreaches)	37	Achieved	Goal 2
24	Complete analysis to increase housing opportunities along BRT routes.	42%	Off Target	Goal 1
25	Conceptualize an updated RTP process that utilizes expanded data-based decision making and innovative technologies to best reflect user experiences to prioritize future actions	100%	Achieved	Goal 3
26	Continue long term property acquisition strategy, including replacement of Villanova.	92%	Achieved	
27	Maintain positive RTC role in working with Federal delegation to support RTC funding needs. (# of Federal Delegation Outreaches)	12	Achieved	
28	Participate in advisory groups created during the 81st session of the NV Legislature by AB54 (Advisory Committee on Traffic Safety) and AB413 (Advisory Working Group to Study Certain Issues Related to Transportation).	100%	Achieved	
29	Update capital funding allocations for FY24 budget process to move toward 5-year Project Implementation Concept	100%	Achieved	Goal 4
30	Strategically adjust goals as needed throughout the year to respond to board direction in a prompt manner.		Achieved	



## **REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item: 5.1.**

To: Regional Transportation Commission

From: Graham Dollarhide, Senior Technical Planner

**SUBJECT: FFY 2023-2027 RTIP Adoption**

### **RECOMMENDED ACTION**

Approve a resolution adopting the Federal Fiscal Years (FFY) 2023-2027 Regional Transportation Improvement Program (RTIP).

### **BACKGROUND AND DISCUSSION**

The FFY 2023-2027 Regional Transportation Improvement Program (RTIP) provides documentation of regional transportation improvements and programs with identified funding sources over a five-year period. Projects in the RTIP were developed through the 2050 Regional Transportation Plan (RTP) process. Unlike an amendment to an existing RTIP, which incorporates specific changes to select projects, an adoption makes a holistic update to all projects to extend the time frame and make any modifications, as needed.

This RTIP will continue many of the projects that are currently underway as well as standing programs contained in the previous document, including purchase of RIDE and ACCESS replacement vehicles; transit and paratransit operations; and the Safe Routes to School, trip reduction, traffic management, and pavement preservation programs. New projects included in the FFY 2023-2027 RTIP have previously been identified in the 2050 RTP. The FFY 2023-2027 document also updates current projects, reflecting any necessary changes to project phasing, scope, cost, funding sources, and/or timing. Some examples of notable projects included as part of this adoption, as well as any applicable changes, are:

- Sparks Boulevard Corridor – Phase 2: updated mix of funding sources
  - Plumas Street Sidewalk – Phase 2: new project added according to Transportation Alternatives Set-Aside Program awards for FY 23/24
  - US 395, North Valleys, McCarran to Golden Valley, Phase 1B
  - US 395, North Valleys, Golden Valley to Stead, Phase 2 – capacity, operational, and pavement rehab
-

Bridge projects:

- Keystone Avenue Improvements – Packages 1 and 2
- Sierra St Bridge Replacement
- Arlington Avenue Bridges Replacement

Micromobility projects:

- 3rd Street Bicycle Facility
- 5th Street Multimodal Improvements: project moved to FY 24/25 pending stakeholder input
- E 6th Street Bicycle Facility and Safety Improvements: project moved to FY 24/25 pending results of SS4A grant application
- Vine Street Bike Facility: project moved to FY 24/25 pending stakeholder input
- Center Street Multimodal Improvements: project construction phase moved project moved to FY 24 pending stakeholder input

The complete project listing and proposed changes are included in the attachment. An air quality analysis is not required as part of this RTIP, as there are no changes to projects involving the addition of roadway capacity.

A 21-day public comment period (July 21 – August 10, 2023) preceded this public hearing. The draft RTIP was posted on the agency website and notice of a 21-day public comment period was published in multiple newspapers and through social media outlets per the RTC Public Participation Plan. Following approval by the Board, the document will be submitted to the Nevada Department of Transportation (NDOT) for inclusion into the Statewide Transportation Improvement Program (STIP). Public comment will also be accepted on individual projects any time they are included in a formal amendment.

Both the Citizens Multimodal Advisory Committee (CMAC) and Technical Advisory Committee (TAC) recommended approval of the draft RTIP at their August 2 and August 3, 2023 meetings, respectively.

**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

3/19/2021      Approved a resolution adopting the FFY 2021-2025 RTIP.

## **RESOLUTION**

### **RESOLUTION AUTHORIZING THE ADOPTION OF THE FEDERAL FISCAL YEARS (FFY) 2023-2027 REGIONAL TRANSPORTATION IMPROVEMENT PROGRAM (RTIP) FOR THE RENO-SPARKS URBANIZED AREA.**

**WHEREAS**, Title 23 Code of Federal Regulations, Part 450, and Title 49 Code of Federal Regulations, Part 613, require the preparation of a Regional Transportation Improvement Program (RTIP) by the Metropolitan Planning Organization (MPO) at least every four years; and

**WHEREAS**, the Regional Transportation Commission of Washoe County (RTC) has been designated by the Governor of the State of Nevada as the Metropolitan Planning Organization (MPO) for Washoe County; and

**WHEREAS**, the RTC, through the conduct of a continuing, comprehensive and coordinated transportation planning process carried out in conjunction with the RTC member entities and the Nevada Department of Transportation and in conformance with all applicable federal requirements, prepared the FFY 2023-2027 RTIP which includes all federal and non-federal regionally significant transportation projects; and

**WHEREAS**, the RTC finds the FFY 2023-2027 RTIP in conformance with the 2050 Regional Transportation Plan (RTP); and

**WHEREAS**, the RTC finds that pursuant to Title 40 of the Code of Federal Regulations, Part 93, this RTIP amendment conforms with the intent of the State Air Quality Implementation Plan; and,

**WHEREAS**, the RTC finds that current fiscal resources are adequate to develop, operate and maintain the transportation system, and finds that the FFY 2023-2027 RTIP is limited to projects for which funds are available or committed; and

**WHEREAS**, the FFY 2023-2027 RTIP has been prepared through a process of community and agency coordination and participation in accordance with the RTC's adopted Public Participation Plan and all applicable federal requirements;

**NOW, THEREFORE, BE IT RESOLVED** that the Regional Transportation Commission does hereby adopt and endorse the FFY 2023-2027 Regional Transportation Improvement Program.

## **CERTIFICATE**

The undersigned, duly qualified Chairperson of the Regional Transportation Commission, certifies that the foregoing is a true and correct copy of a resolution adopted at a legally convened meeting held on August 18, 2023.

---

Ed Lawson, Chair  
Regional Transportation Commission

# Federal Fiscal Year 2023 - 2027 REGIONAL TRANSPORTATION IMPROVEMENT PROGRAM

Draft

Multimodal Safety  
Public Transportation  
Complete Streets  
Public Participation



Building a Better Community Through Quality Transportation  
[rtcwashoe.com](http://rtcwashoe.com)



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## Chapter 1: Introduction

The State Legislature created the Regional Transportation Commission of Washoe County (RTC) in 1979, combining the previous statutory authority of the Regional Street and Highway Commission, the Regional Transit Commission and the Washoe County Area Transportation Study Policy Committee. The responsibilities of the RTC include design and construction of major streets and highways and administration of public transportation systems serving Washoe County.

The RTC functions as the Metropolitan Planning Organization (MPO) for the Reno-Sparks urbanized area pursuant to federal law (23 USC 134). In this capacity, RTC is responsible under the Code of Federal Regulations (23 CFR Part 450) for carrying out a "continuing, cooperative, and comprehensive" transportation planning process that results in plans and programs consistent with the planned development of the urbanized area. The RTC prepares short- and long-range transportation plans for the region, programs multimodal transportation and safety improvements through the RTIP process, and develops and administers the Unified Planning Work Program.

### Overview of the Regional Transportation Improvement Program (RTIP) Process

The Federal Fiscal Year 2023-2027 Regional Transportation Improvement Program (RTIP) is a five-year plan of street and highway, transit, bicycle and pedestrian projects for Washoe County and is based on the federal fiscal year (October – September). The RTIP includes a summary of projects by federal fiscal year and shows the agency responsible for implementing the project, funding source and other related information. The RTIP represents a prioritized program directed at meeting Washoe County's growing transportation needs while improving the region's safety, air quality, transportation efficiency, and mobility. The RTIP is required by federal regulation and serves as a useful tool in planning and programming transportation system improvements.

The RTIP assists in implementing the Regional Transportation Plan (RTP) by advancing projects from the first five years of the long-range plan. Projects in the RTIP further the RTP guiding principles of supporting safe and healthy communities; economic prosperity, equity, and innovation; sustainability and climate action; and increased travel choices.

The initial federal legislation that established overall transportation program direction and authorized funding levels to the RTC as the MPO for Washoe County was included in the 1990 Clean Air Act Amendment (CAAA) and the Intermodal Surface Transportation Efficiency Act of 1991 (ISTEA). There have been several surface transportation funding and authorization bills since ISTEA, including the Moving Ahead for Progress in the 21<sup>st</sup> Century (MAP-21) Act and the current Infrastructure Investment and Jobs Act (IIJA), which was signed into law on November 15, 2021. The passage of MAP-21 established a streamlined, performance-based, and multimodal program to address the many challenges facing the U.S. transportation system. It

signaled a transition to a performance and outcome-based program, and directed states and MPOs to invest resources in projects to achieve individual targets that collectively will make progress toward national goals. IJJA carries forward and expands the performance-based transportation planning framework established under MAP-21.

## Conformity with the Clean Air Act Amendment of 1990

The commitment of Congress to promote and continue major reforms in the transportation planning process is shown with CAAA and all transportation legislation since ISTEA in 1991. The conformity provisions of the CAAA established important requirements that transportation plans, programs and projects conform to the purpose of the State Implementation Plan (SIP). The SIP establishes actions designed to improve air quality and meet National Ambient Air Quality Standards (NAAQS) for each criteria air pollutant, according to the schedules included in the CAAA, and is a formal submission of the region's air quality strategy to the federal government.

The emissions from motor vehicles make a significant contribution to air pollution, therefore, CAAA requires that transportation officials make a commitment to programs and projects that will help achieve air quality goals. Examples of these air quality goals include providing for greater integration of the transportation and air quality process; ensuring that transportation plans, programs and projects conform with the SIP and contribute to attainment of national standards; and reducing growth in vehicle miles traveled (VMT) and congestion in areas that have not attained the U.S. Environmental Protection Agency (EPA) air quality standards. The federal standards require that certain pollutants do not exceed specified levels. Areas that violate this standard for specified pollutants are designated as non-attainment areas.

The core area of the Truckee Meadows is designated as Hydrographic Area #87 and is fully incorporated within the metropolitan planning area boundary. The hydrographic area is designated as in "attainment/maintenance" for both carbon monoxide (CO) and particulate matter of less than 10 microns (PM<sub>10</sub>). A regional emissions analysis must be performed for each pollutant that the area is determined to be in maintenance status for. The results from this analysis are shown in Chapter 8.

Washoe County is currently designated as "attainment/unclassifiable" for the 2015 ozone NAAQS, however, the most recent certified ozone data indicates the Truckee Meadows is not meeting the standard. Non-compliance with the NAAQS could lead to EPA formally re-designating the urban area to "non-attainment." In 2016, the RTC Board adopted a resolution supporting the goals identified in the Washoe County Health District, Air Quality Management Division's (WCHD—AQMD) Ozone Advance Path Forward. Ozone Advance encourages voluntary actions to proactively reduce VMT, improve air quality, and avoid a "non-attainment" designation. Some of the projects included in this RTIP that yield improvements to the region's air quality include:

- Acquisition of Electric/Zero Emission or Alternative Fuel Transit Buses
- Implementation of Bicycle/Pedestrian Facilities
- Traffic Flow/Intersection Improvements
- Traffic Signal Operations Program
- Trip Reduction Program

## Implementing IIJA and MAP-21

Under MAP-21, the transportation planning process was enhanced to incorporate performance goals, measures, and targets in identifying needed transportation improvements and project selection. The RTP describes the performance measures and targets used in assessing system performance and identifies progress made in achieving the performance targets. The RTIP must also be developed to make progress toward established performance targets, and the projects identified in the Federal Fiscal Year (FFY) 2023-2027 RTIP support multiple national performance goals by offering measurable contributions toward those targets.

The following national performance goals for Federal highway programs were established under MAP-21 and then continued under subsequent transportation bills—Fixing America’s Surface Transportation (FAST) Act and IIJA:

- Safety—To achieve a significant reduction in traffic fatalities and serious injuries on all public roads.
- Infrastructure condition—To maintain the highway infrastructure asset system in a state of good repair.
- Congestion reduction—To achieve a significant reduction in congestion on the National Highway System (NHS).
- System reliability—To improve the efficiency of the surface transportation system.
- Freight movement and economic vitality—To improve the national freight network, strengthen the ability of rural communities to access national and international trade markets, and support regional economic development.
- Environmental sustainability—To enhance the performance of the transportation system while protecting and enhancing the natural environment.
- Reduced project delivery delays—To reduce project costs, promote jobs and the economy, and expedite the movement of people and goods by accelerating project completion through eliminating delays in the project development and delivery process, including reducing regulatory burdens and improving agencies’ work practices.

In December 2021, the Federal Highway Administration (FHWA) and Federal Transit Administration (FTA) released a joint letter asking federal field offices to work with their MPOs, state departments of transportation, transit agencies, and federal land management agencies to incorporate new Planning Emphasis Areas into their planning work programs. Below is a list—including brief descriptions—of these priorities that function as strategic objectives for the

Surface Transportation Program and have been integrated into the RTC planning work program and RTIP.

- Transition to a clean energy, resilient future – transportation plans that are developed and implemented to accelerate the transition toward electric and other alternative fueled vehicles, plan for a sustainable infrastructure system that works for all users, and undertake actions to prepare for and adapt to the impacts of climate change.
- Equity and Justice40 in Transportation Planning – advancement of racial equity and support for underserved and disadvantaged communities through increased public involvement in the planning process that results in plans and strategies that reflect various perspectives, concerns, and priorities from impacted areas. Environmental justice will be further advanced through the Justice40 Initiative, which states that 40 percent of federal investments flow to disadvantaged communities.
- Complete streets – policies, rules, and procedures that are created and revised to consider impact on safety for all road users, ensuring provisions for safety in future transportation infrastructure, particularly those outside automobiles. The goal is to provide an equitable and safe transportation network for travelers of all ages and abilities, including those from marginalized communities facing historic disinvestment.
- Public involvement – increase meaningful public involvement in transportation planning through integration of Virtual Public Involvement tools into the overall public involvement approach while ensuring continued public participation by individuals without access to computers and mobile devices.
- Strategic Highway Network (STRAHNET)/U.S. Department of Defense (DOD) coordination – coordination with representatives from DOD in the transportation planning and project programming process on infrastructure and connectivity needs for STRAHNET routes and other public roads that connect to DOD facilities.
- Federal Land Management Agency (FLMA) coordination – coordination with FLMAs in the transportation planning and project programming process on infrastructure and connectivity needs related to access routes and other public roads and transportation services that connect to federal lands. Opportunities to leverage transportation funding to support access and transportation needs of FLMAs should be explored before transportation projects are programmed in the RTIP. According to 23 CFR 450.316(d), MPOs must appropriately involve FLMAs in the development of the RTP and the RTIP.
- Planning and Environmental Linkages (PEL) – implementation of PEL as part of the transportation planning and environmental review processes. The use of PEL is a collaborative and integrated approach to transportation decision-making that considers environmental, community, and economic goals early in the transportation planning process, and uses the information, analysis, and products developed during planning to inform the environmental review process.
- Data in transportation planning – incorporation of data sharing and consideration into the transportation planning process. This should be done to address the emerging topic areas of data sharing, needs, and analytics because data assets have value across multiple programs. Developing and advancing data sharing principles allows for efficient

use of resources and improved policy and decision-making at all levels of jurisdiction and for all parties involved.

## **FHWA Transportation Performance Management**

The FHWA defines Transportation Performance Management program as a strategic approach that uses system information to make investment and policy decisions to achieve national performance goals. The first of three Performance Management Final Rules, which focuses on safety, supports the Highway Safety Improvement Program (HSIP), as it establishes safety performance measure requirements for the purpose of carrying out the HSIP and to assess fatalities and serious injuries on all public roads.

This Final Rule, known as PM1, establishes five performance measures as five-year rolling averages to include:

1. Number of Fatalities
2. Rate of Fatalities per 100 million VMT
3. Number of Serious Injuries
4. Rate of Serious Injuries per 100 million VMT
5. Number of Non-motorized Fatalities and Non-motorized Serious Injuries

The RTC's aspirational vision is that zero fatalities on our region's roadways is the only acceptable goal and RTC recognizes that reaching that goal will require time and significant effort by all stakeholders. The annual safety performance targets identified in this document represent an important step in working toward the ultimate goal of eliminating traffic-related deaths and serious injuries. The safety performance targets are considered interim-performance levels that make progress toward the long-term goal of zero fatalities. This approach is consistent with guidance from the U.S. Department of Transportation as well as states and MPOs across the nation, including the Nevada Department of Transportation (NDOT). The RTC is also an active stakeholder in the Vision Zero Truckee Meadows Task Force. The RTC recently adopted NDOT's performance targets, and through localized efforts, works toward the statewide goal of zero fatalities. Table 1-1 shows the current targets and status of these measures.

**Table 1-1  
National PM1 Performance Measures**

<b>RTP Goal</b>	<b>Performance Measures</b>	<b>Performance Target</b>	<b>2021 Performance Measure Status</b>	<b>2021 Performance Target Status</b>
<b>Improve Safety</b>	Number of fatal crashes (5-year average)	Reduction in the # of traffic fatalities compared to target value (330.2)	Actual = 329.4	Met 2021 goal and working towards aspirational goal of Zero Fatalities
	Number of fatal crashes per 100 million VMT (5-year average)	Reduction in the # of traffic fatalities per 100M VMT compared to target value (1.226)	Actual = 1.203	Met 2021 goal and working towards aspirational goal of Zero Fatalities
	Number of serious injury crashes (5-year average)	Reduction in the # of serious injuries compared to target value (1,154.7)	Actual = 1,035.0	Met 2021 goal
	Number of serious injury crashes per 100 million VMT (5-year average)	Reduction in the # of serious injuries per 100M VMT compared to target value (3.835)	Actual = 3.854	Did not meet 2021 goal
	Number of non-motorized fatalities & serious injuries (5-year average)	Reduction in the # of non-motorized traffic fatalities & serious injuries compared to target value (309.8)	Actual = 282.0	Met 2021 goal and working towards aspirational goal of Zero Fatalities

Note: 2021 crash data is the most recent data available at the time of this publication.  
Source: NDOT State Fiscal Year 2022 Performance Management Report

As part of the Transportation Performance Management program, MAP-21 established measures for assessing pavement and bridge condition for the National Highway Performance Program (known as PM2). It also established measures for travel time reliability on the Interstate and non-Interstate NHS for all vehicles as well as for truck traffic specifically (Interstate only), and the assessment of the Congestion Mitigation and Air Quality Improvement Program (CMAQ) through measurement of total emissions reduction of on-road mobile source emissions. These measures are known as PM3.

The six national performance measures developed under PM2 include the percentage of pavements in good condition and percentage of pavements in poor condition on both the Interstate System and non-Interstate NHS, as well as the percentage of bridges in good condition and the percentage of bridges in poor condition. The national performance measures for assessing pavement condition do not apply to all regional roads and the FHWA requires measures that reflect data elements in the Highway Performance Monitoring System, including: International Roughness Index, rutting for asphalt surfaced pavements, faulting for jointed concrete surface pavements, and cracking percent. The method for assessing bridges is based upon elements in the National Bridge Inventory, which examines the condition of the bridge deck, superstructure, substructure, and culverts. The data to support these measures is provided by NDOT, which assess pavement and bridge infrastructure on a periodic basis. Targets have been determined in collaboration with NDOT, and are as follows, according to the FFY 2022-2025 targets. The RTC has adopted a local performance management target for the



pavement condition of regional roads, which is expressed as an overall pavement condition index of 80. It should be noted that the statewide targets for the first performance period (FFY 2018-2021) were achieved, albeit somewhat conservative. The four-year targets for the current period have been established to be more aggressive in achieving higher standards for pavement conditions.

**Table 1-2  
National PM2 Performance Measures**

<b>Performance Measures</b>	<b>Baseline</b>	<b>2-Year Target</b>	<b>4-Year Target</b>
Percentage of Pavements of the Interstate System in Good Condition	81.2%	81.0%	81.0%
Percentage of Pavements of the Interstate System in Poor Condition	0.3%	0.5%	0.5%
Percentage of Pavements of the Non-Interstate NHS in Good Condition	67.4%	67.0%	67.0%
Percentage of Pavements of the Non-Interstate NHS in Poor Condition	0.3%	0.5%	0.5%
Percentage of NHS Bridges Classified as in Good Condition	42.2%	35.0%	35.0%
Percentage of NHS Bridges Classified as in Poor Condition	0.5%	7.0%	7.0%

The national performance measures developed under PM3 include reliability measures related to reliability, delay/congestion, and emissions. RTC adopted NDOT’s performance targets for the reliability measures on the Interstate System, as shown in Table 1-3. Level of Travel Time Reliability is defined as the ratio of the longer travel times (80th percentile) to a “normal” travel time (50th percentile), using data from FHWA’s National Performance Management Research Data Set or equivalent. Data to reflect the users can include bus, auto, and truck occupancy levels. Table 1-4 shows the CMAQ PM3 measure for the most recent reporting year (report submitted 2023).

The Reno-Sparks and Las Vegas urbanized areas are the only two areas in the state that are in maintenance or non-attainment of NAAQS for specific criteria air pollutants. The state targets for these performance measures are simply the sum of the total emissions reductions for both Reno-Sparks and Las Vegas.

**Table 1-3  
National PM3 Performance Measures (System Performance/Freight)**

RTP Goal	Performance Measures	Performance Target	2021 Performance Measure Status	2021 Performance Target Status
Congestion Reduction	Percentage of person-miles traveled that are reliable on the Interstate System	87% or higher	88.9%	Met goal
	Percentage of person-miles traveled that are reliable on the Non-Interstate National Highway System (NHS)	87% or higher	93.1%	Met goal
	Truck Travel Time Reliability (TTTR) Index	1.26% or less	1.26%	Met goal

**Table 1-4  
National PM3 Performance Measures (CMAQ)**

Performance Measure (CMAQ-funded projects)	Emissions Reductions 2023 Report Year
Total emission reduction of NOx	5.515 kg/day
Total emission reduction of VOC	1.384 kg/day
Total emission reduction of PM <sub>10</sub>	0.548 kg/day
Total emission reduction of CO	121.984 kg/day

### FFY 2023-2027 Regional Transportation Improvement Program Summary

The table on the following page is a summary of the highway, transit capital and operating, bikeway and other projects in the RTIP. The 5-year program has a total cost of approximately \$1.75 billion. The complete RTIP project listing is contained in Appendix A, and represents the status of projects at the time of the adoption of this document. The listing shows the project description, the project limits where applicable, the project phase (preliminary engineering/design, right-of-way, construction, or “other” – operations or equipment purchases), the year programmed, the project costs and the federal, state and/or local contributions to the project cost. It should be noted this project listing excludes projects for which funding obligation was completed during FFY 2023 and for which no funding is programmed in FFYs 2024 – 2027. Over the lifetime of the current RTIP, it is anticipated that there will be amendments or administrative modifications, as it is a living document. To ensure portrayal of the most current status of a given project, those interested are encouraged to search for projects in the electronic Statewide Transportation Improvement Program (eSTIP). The eSTIP can be accessed online at <https://estip.nevadadot.com/default.asp>, and contains a searchable/sortable database for projects within the entire State of Nevada.

**Table 1-5  
FFY 2023-2027 RTIP Summary**

<b>Project by Mode/Program</b>	<b>Total Cost</b>	<b>% of Total Program</b>
Capacity Improvements	\$353,300,000	20%
Multimodal Corridor Improvements	\$291,100,000	17%
Freeway System Improvements	\$646,300,000	37%
Pavement Preservation	\$245,600,000	14%
Transit Operating and Capital	\$106,100,000	6%
Intelligent Transportation Systems (ITS)/Traffic Management	\$51,000,000	3%
Other Projects/Programs*	\$51,600,000	3%
<b>TOTAL</b>	<b>\$1,745,000,000</b>	<b>100%</b>

\* Includes Safe Routes to School, trip reduction, Park and Ride, bicycle/pedestrian safety education programs, maintenance agreements, and miscellaneous construction projects.

The individual projects in the RTIP were developed through coordination between the RTC, NDOT, Washoe County and the cities of Reno and Sparks and based on the RTP, which was developed through extensive public outreach. The RTC Technical Advisory Committee (TAC) and Citizens Multimodal Advisory Committee (CMAC) also provided input.

**Freeway System and Capacity Projects** The RTIP programs approximately \$1 billion in freeway system and capacity projects. This funding typically comes from federal IJJA programs, RTC Fuel Tax and state gas tax. Approximately \$646 million are programmed by NDOT for freeway projects, such as improvements to US 395 and I-80. Major capacity projects (about \$353 million programmed) include design and construction of RTC improvements for Pyramid Highway, Sparks Boulevard, North Virginia Street, and Mill Street. Another \$246 million is allocated to the ongoing RTC Pavement Preservation program to maximize the useful life of the regional road network.

**Multimodal Corridor Projects** The RTIP contains roughly \$291 million for multimodal corridor projects. Significant projects include multimodal improvements on Keystone Avenue, West 4<sup>th</sup> Street, Lemmon Drive, South Virginia Street, and Sun Valley Boulevard, along with several shorter segments in downtown Reno.

**Transit Programs** The RTIP programs approximately \$106 million on public transportation projects during the five-year period. This includes transit system operations, including the RTC RIDE fixed-route (with the RTC REGIONAL CONNECTOR intercity service) and the RTC ACCESS paratransit services. Additional transit services such as the Trip Reduction Program and U-Pass Program are operated with these funds. Most capital outlays are federal funds from FTA Section 5307, Section 5339 and CMAQ. The primary capital expenditures call for phase one of the Villanova Maintenance Facility replacement design and the replacement of RTC ACCESS vans, but also include equipment purchases and bus stop/station and facilities improvements.

Strategic utilization of these funds has enabled the RTC to systematically replace older diesel RIDE buses with new alternative-fueled buses. As of spring 2022, 100% of the RIDE fleet is hybrid or electric, a goal that was achieved 13 years ahead of schedule.

### Transportation Systems Management and Transportation Demand Management

Transportation system operations improvements maximize the capacity of existing roadways in a highly cost effective way. RTC conducts a traffic operations program cooperatively with Washoe County, the City of Reno and the City of Sparks. Over the next five years, roughly \$51 million is to be used for the traffic management program, intelligent transportation systems (ITS), intersection geometric and capacity improvements, and traffic calming improvements. This amount also includes NDOT expenditures on similar projects.

### **Other RTIP Conformities and Certifications**

Conformity with the Regional Transportation Plan (RTP) The projects in the RTIP are developed from the project list and policies included in the RTP, and therefore conform to the RTP. The RTIP is the principal mechanism for implementing the transportation projects and programs contained in the RTP.

Financial Capacity With federal programs, the RTC is required to evaluate the financial capacity of the agency to conduct and carry forward the financial requirements related to public transportation operations. The financial capacity analysis is prepared annually by the RTC Finance Department with the budget process. The RTC has the financial capacity to continue the street and highway and the public transportation programs through the five-year operating and capital financial model.

Public Involvement Federal legislation requires that each MPO formally adopt a proactive public involvement process. The intent of the process is an early and continuing involvement of the public in developing transportation plans and programs. The RTC most recently updated its Public Participation Plan in May 2022, which is compliant with federal regulations and is a comprehensive plan outlining the public involvement and education process.

Conformity Determination The air quality and regulatory conformity analysis determination associated with the RTIP is included as part of this document (see Chapter 8 for details). A finding of conformity by the RTC is required before approval of federal program funding for individual projects included in the RTIP. The RTIP will be reviewed and updated every two to four years, allowing consideration and revision of project priorities. The resolution adopting this RTIP incorporates the required findings of conformance. The RTC works closely with the WCHD—AQMD and other partner agencies involved in air quality analysis through periodic interagency consultation meetings.

## Chapter 2: Planning Process

The RTIP is developed with the assistance and cooperation of state and local governments, including public works and planning officials, who develop project proposals and review the project listing developed by RTC staff.

### Regional Planning Process

As the MPO for the Reno-Sparks urbanized area, the RTC is responsible for carrying out a "continuing, cooperative, comprehensive" transportation planning process that results in plans and programs consistent with the planned development of the urbanized area. The RTC develops the RTP, RTIP, and other planning documents in close cooperation with several federal, state and local transportation and environmental agencies as described below.

[Truckee Meadows Regional Planning Agency \(TMRPA\)](#) The TMRPA, created by state legislation in 1989, is responsible for preparation and implementation of the Truckee Meadows Regional Plan (referred to as the Regional Plan). The TMRPA is comprised of the Regional Planning Governing Board, the Regional Planning Commission, and staff. The Regional Plan addresses regional urban form, natural resource management, infrastructure, and service provision within Washoe County. The agency implements the Regional Plan by ensuring that master plans of local governments and affected entities conform to adopted policies. The RTC is considered an affected entity and as such the RTP must be in conformance with the Regional Plan.

[Nevada Department of Transportation \(NDOT\)](#) NDOT is responsible for planning, programming, construction, and maintenance activities involving federal aid and state gas tax funding. Planning and programming of these projects are coordinated with the RTC through the RTIP and RTP processes. NDOT also provides funds to the RTC for transportation planning and transit operations and provides technical data and analysis to support the regional transportation planning process.

[Washoe County Health District \(WCHD\)](#) The WCHD has statutory responsibility for developing and implementing air quality plans and programs in Washoe County. The WCHD is a strong partner with the RTC in promoting a healthy community. The WCHD—AQMD and Chronic Disease Prevention Program actively support transportation investments that improve community health.

[Reno-Tahoe Airport Authority \(RTAA\)](#) The RTAA, created in 1977 by the State Legislature, has responsibility for county-wide airport operations and planning. It is the owner and operator of the Reno-Tahoe International and Reno-Stead Airports.

## RTC Planning Process

The RTC planning process is intended to provide decision makers with plans and projects that effectively meet community needs. The measure of any planning program is the extent to which planned projects are implemented and the extent to which the desired objectives are achieved. Transportation planning in Washoe County has been successful due in large part to the unique structure of the RTC as both a planning and an implementing agency.

**Regional Transportation Plan (RTP)** The central component or foundation of the RTC planning process is the RTP. The RTP includes transportation policies encompassing multimodal travel by vehicles, transit, bicycles, and pedestrians and also addresses transportation management strategies. The RTP identifies the facilities, services and programs necessary to meet increasing travel demands through a minimum of a 20-year planning horizon.

The RTP includes guiding principles that are the overarching themes that recur throughout the plan and provide the basis for the goals and selection of transportation investments. The principles are:

- Safe and Healthy Communities
- Economic Prosperity, Equity and Innovation
- Sustainability and Climate Action
- Increase Travel Choices

The goals that were developed to support the guiding principles include:

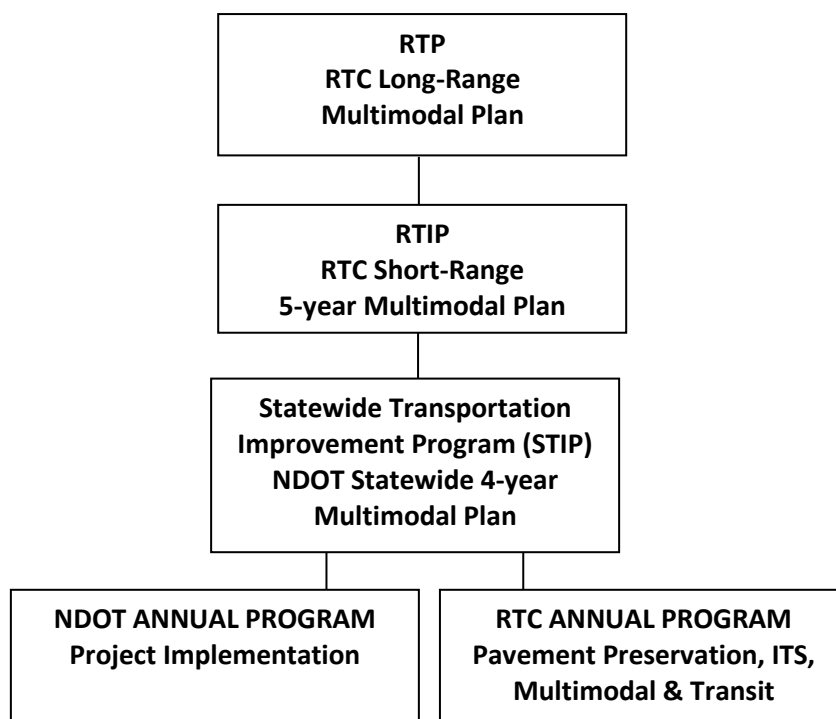
- Improve and Promote Safety
- Integrate All Types of Transportation
- Promote Healthy Communities and Sustainability
- Promote and Foster Equity and Environmental Justice
- Integrate Land Use and Economic Development
- Manage Existing Systems Efficiently
- Enhance Regional Connectivity
- Improve Freight and Goods Movement
- Invest Strategically
- Engage the Public and Encourage Community Involvement

The guiding principles and goals are discussed in detail in Chapter 1 of the 2050 RTP.

**Regional Transportation Improvement Program (RTIP)** The RTIP is a five-year, multimodal transportation plan for implementation of projects in Washoe County. It includes transit, paratransit, major street and highway capital projects and transportation system and demand management programs. The RTIP is the RTC's principal means of implementing long-term transportation planning objectives through annual programming of specific projects. Public

transportation projects are incorporated into the RTP. The implementation of the RTP guiding principles and goals occurs mainly through the RTIP, as shown in Figure 2-1.

**Figure 2-1  
Regional Transportation Commission  
Planning Process**



### RTC Advisory Committees

The RTC has established two standing advisory committees that participate actively in the transportation planning process that provide additional forums for discussion of regional transportation issues. The Citizens Multimodal Advisory Committee (CMAC) is a self-governing committee that meets once a month and has responsibility for reviewing agency plans and projects, evaluating plan conclusions and recommendations and providing general public input into the planning process. The CMAC consists of citizens from various jurisdictions of Washoe County appointed by the RTC Board to provide public input to RTC staff in the conduct of transportation planning activities. CMAC membership is geographically diverse and maintains a balance of members with an interest in or experience with one of the following emphases: RTC RIDE (fixed route transit), RTC ACCESS (paratransit), bicycle/pedestrian, and general multimodal transportation.

The Technical Advisory Committee (TAC) is composed of planning and public works personnel from each of the local governmental entities including the Cities of Reno and Sparks and Washoe County. In addition, representatives from NDOT, FHWA, TMRPA, RTAA, WCHD—

AQMD, Reno-Sparks Indian Colony and Washoe County School District provide input on transportation and air quality planning issues. It is the responsibility of this committee to review and comment on plans developed by the RTC from a technical standpoint. It also advises and assists the RTC planning staff with methods and procedures and recommends technical standards.

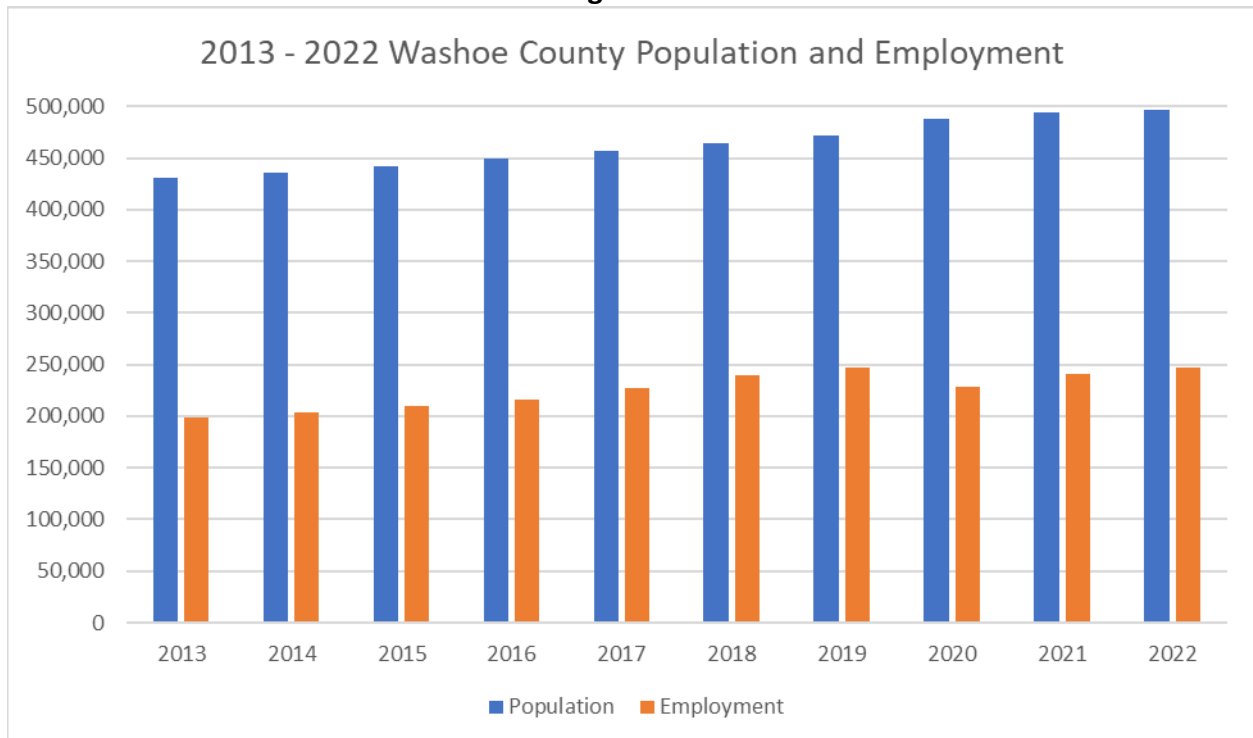


## Chapter 3: Current and Future Trends

### Socioeconomic Trends

As shown in Figure 3-1, both population and employment in Washoe County have been on an upward trend for the past decade. Between 2013 and 2022, the U.S. Census Bureau statistics showed population increased from 421,969 to 477,082. During this same time, employment increased from 191,887 to 227,627. Long term projections indicate a 2050 population of about 600,000. Other socioeconomic trends also affect the amount and type of travel in Washoe County, including the aging of the population.

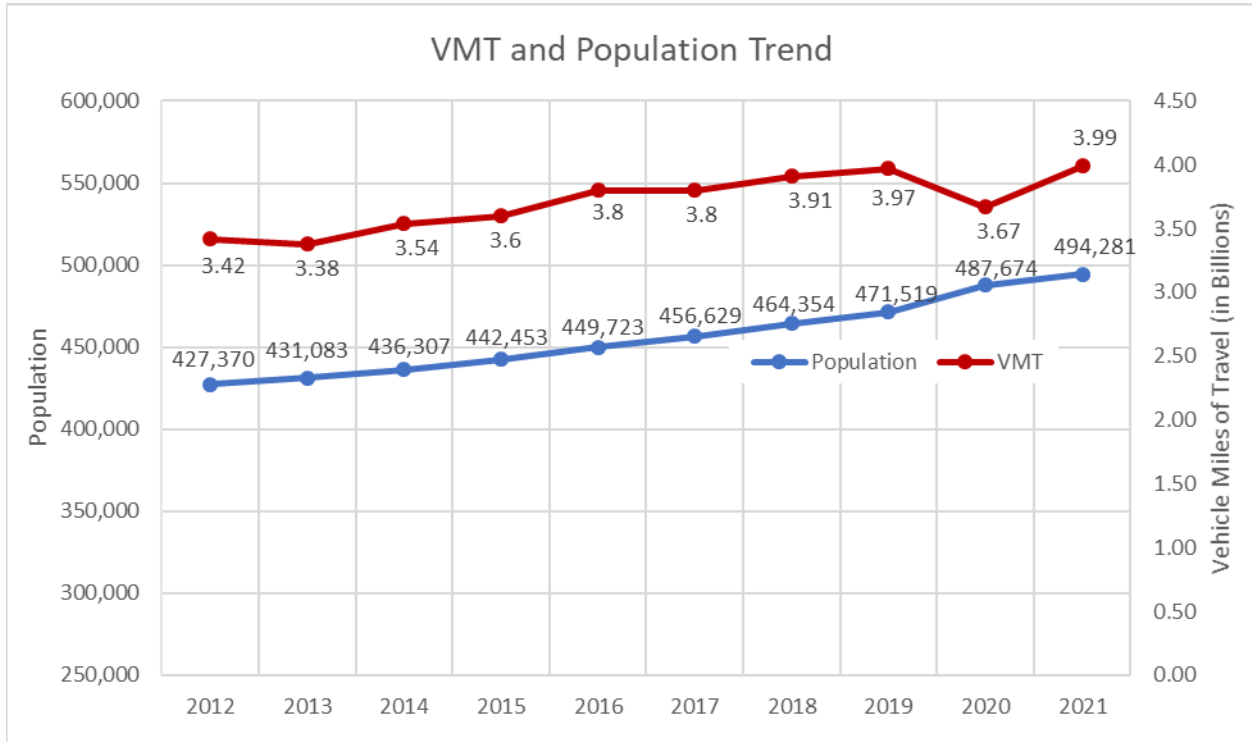
Figure 3-1



### Travel Trends

Annually, NDOT has tracked the growth in motor vehicle travel in Washoe County as part of the Highway Performance Monitoring System (HPMS). While population has been steadily increasing from 2012 to 2021, the HPMS estimate of annual vehicle miles of travel (VMT) experienced an overall increase from about 3.42 billion in 2012 to nearly 4 billion in 2021 (Figure 3-2). While VMT declined from 2019 (3.97 billion) to 2020 due to COVID-19, travel has rebounded as the economy fully reopened.

**Figure 3-2**



### Population and Employment Forecasts

The TMRPA develops the population and employment forecasts used in the regional travel demand model in partnership with the RTC, NDOT, Washoe County, City of Reno, and City of Sparks. The TMRPA uses an allocation-based model to visually display a variety of population growth scenarios. As this RTIP is based on the 2050 RTP, the 2020 Washoe County Consensus Forecast was used to establish the long-range total population projections for Reno, Sparks, and unincorporated Washoe County. Full documentation of the Consensus Forecasts is available on the TMRPA website. The RTC works with the TMRPA through a shared use program which includes support and collaboration in GIS analysis, data collection, online data access, and development of the Consensus Forecast.

**Table 3-1**  
**2020 Consensus Forecast Totals**

Households, Population and Employment					
Model Year	2020	2025	2030	2040	2050
Households	187,558	199,384	209,470	224,738	238,244
Population	473,721	504,914	528,136	565,931	599,684
Employees	291,431	307,510	325,913	360,562	392,228

## Chapter 4: Federal Transportation Programs

### Introduction

The federal Intermodal Surface Transportation Efficiency Act of 1991 (ISTEA) established overall federal transportation program direction and, through the reauthorizations of the bill, a major portion of the FFY 2023-2027 Regional Transportation Improvement Program funding is provided. These programs were, for the most part, continued under the Transportation Equity Act for the 21<sup>st</sup> Century (TEA-21) and the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) transportation legislation. When MAP-21 was enacted, some of the core highway formula programs were restructured and carried through the FAST Act and IIJA. A brief summary of each of the programs is provided below.

**National Highway Performance Program (NHPP)** The NHPP combined former SAFETEA-LU programs including the NHS and Interstate Maintenance and Bridge Programs. The NHPP provides support for the condition and performance of the NHS for the construction of new facilities on the NHS and to ensure that investments of federal-aid funds in highway construction are directed to support progress toward the achievement of performance targets to be established in each state's asset management plan for the NHS. As a new function of this program added by IIJA, NHPP now also provides support for activities to increase the resiliency of the NHS to mitigate the cost of damages caused by natural disasters.

**Surface Transportation Block Grant Program (STBG)** The STBG program provides flexible funding that may be used for projects to preserve or improve conditions and performance on any Federal-aid highway; bridge projects on any public road; facilities for non-motorized transportation; transit capital and public bus terminals and facilities. IIJA increases the flexibility of these funds adding several new project categories to the list, including wildlife crossing structures; installation of electric vehicle charging and vehicle-to-grid infrastructure; projects to enhance travel and tourism; and other project types. These funds provide NDOT and RTC with the opportunity to program funds for new construction, maintenance, transit, ridesharing/employer trip reduction (ETR), centralized traffic signal control systems and traffic management programs. The STBG program is divided into STBG-Statewide and STBG Washoe County Urbanized Area (STBG-Local) funding categories.

**Congestion Mitigation and Air Quality Improvement Program (CMAQ)** The CMAQ funding category is available to air quality non-attainment/maintenance areas and the majority of the Truckee Meadows is an air quality maintenance area for specific criteria pollutants identified under the CAAA. Funding from the CMAQ program can only be used for projects that will have substantial air quality benefits or the type of improvements identified in the State Implementation Plan (SIP). CMAQ cannot be used to fund projects that will result in the construction of new capacity available to single-occupancy vehicles. Programs and projects that CMAQ can fund include programs to improve public transit, ETR programs, intersection improvements, traffic flow improvements that reduce emissions, bicycle/pedestrian facilities,

park-and-ride facilities, and programs to restrict vehicle use in areas of emissions concentration.

**Highway Safety Improvement Program (HSIP)** The HSIP is a core federal-aid program with the purpose to achieve a significant reduction in traffic fatalities and serious injuries on all public roads, including locally (non-state)-owned roads and roads on tribal land. The HSIP requires a data-driven, strategic approach to improving highway safety on all public roads with a focus on performance.

**National Highway Freight Program (NHFP)** The NHFP, first authorized through the FAST Act and continued through IIJA, is focused on improving the efficient movement of freight on the National Highway Freight Network. Funds are distributed to states by formula for eligible activities, such as construction, operational improvements, freight planning, and performance measurement. A state must have a State Freight Plan approved by FHWA in order to obligate NHFP funds. The Nevada State Freight Plan was approved in September 2016.

### **IIJA Planning Provisions**

The passage of IIJA made no significant regulatory changes pertaining to transportation planning. The most important planning requirements that are included in the RTC planning process are summarized below.

**Long-Range Plan** The MPO must prepare a long-range transportation plan that identifies transportation facilities for all modes necessary to serve a 20-year forecast period at a minimum. The long-range plan must include a financial plan that demonstrates how the long-range plan can be implemented, including all public and private revenue sources that are "reasonably expected to be made available to carry out the plan, and recommends any additional financing strategies for needed projects and programs."

**Transportation Management Area (TMA) Designation and Requirement for Congestion Management Process (CMP)** The Secretary of the Department of Transportation is required to designate all urbanized areas with more than 200,000 population as Transportation Management Areas (TMAs). Within a TMA, it is necessary to develop a Congestion Management Process (CMP) that provides for effective management of new and existing transportation facilities eligible for federal funding. The RTC developed a CMP that was updated in the 2050 RTP.

**MPO-State Coordination in RTIP Development** Federal regulation states that all federally funded projects within the boundaries of a metropolitan planning area serving a TMA shall be selected for implementation from the approved RTIP by the MPO in consultation with the state. It specifies that the RTIP project selection is the responsibility of the MPO.

**Project Prioritization and Financial Constraint** Federal regulation also requires that the RTIP prioritize all projects and have a financial plan that demonstrates that funding is available for the projects listed. These requirements are discussed in Chapter 6—Project Prioritization and Chapter 7—Financial Plan of this document.

**Functional Classification** NDOT is required to develop a functional classification of roads and streets and the designation of routes on the Federal-aid highway system "in cooperation with local and regional officials." Functional classification identifies and groups roadways by the character of the service they provide. It was necessary to designate the NHS segments within Nevada by 1993. The NHS in Washoe County was updated in 2016 through a cooperative effort with NDOT and the RTC.

**Certification** The Secretary of the Department of Transportation is required to certify that each MPO is carrying out its responsibilities under the law. The first certification deadline was September 30, 1993. Recertification must occur every four years. The most recent recertification for the RTC was received in 2020. Federal funding may be withheld if the MPO is not certified.

## Chapter 5: Public Transportation Capital and Operating Plan

### Introduction

The FFY 2023-2027 Public Transportation Capital and Operating Plan provides an overview of the current status of public transportation and, more importantly, is a plan for future service delivery. Public transportation is a valuable community asset that helps reduce traffic congestion, improve air quality, and provide essential mobility. This service allows local residents to access jobs, education, and commercial activities. By providing transit service along urban corridors, public transportation also helps promote the higher density, mixed use, and walkable communities envisioned in the Truckee Meadows Regional Plan.

### RTC Public Transportation Services

The RTC provides the following public transportation services to the residents of Washoe County:

- RTC RIDE
- RTC RAPID
- RTC REGIONAL CONNECTOR
- RTC FlexRIDE
- RTC ACCESS
- Tahoe Area Regional Transit and Tahoe Transportation District (through an interlocal cooperative agreement)

**RTC RIDE** – RTC RIDE began operating public transportation services on September 18, 1978 and is RTC's primary fixed-route public transportation system. RTC owns RTC RIDE facilities and equipment. RTC RIDE is operated by a private contractor under a turn-key contract.

RTC RIDE is a public fixed-route transit service owned by RTC. Passage of a 1/4% sales tax referendum by the voters of Washoe County on September 14, 1982, provided the financial resources necessary to expand fixed-route and paratransit service. As federal transit funding steadily declined over the past several years, the sales tax funding has been critical to continued improvement in public transportation. As part of a larger transportation funding package, transit funding was increased by the adoption of an additional 1/8% sales tax effective July 1, 2003, half of which has generally been used for transit and the other half for roadways.

The RTC RIDE system encompasses the Cities of Reno and Sparks and areas of Washoe County, using a fleet of approximately 70 buses on 26 fixed routes. The system operates in a 90 square-mile service area based on a ¾-mile distance from each fixed route (excluding RTC REGIONAL CONECTOR).

**RTC RAPID** – On October 11, 2009, RTC RAPID was introduced. RTC RAPID is a Bus Rapid Transit (BRT) express service, known as the Virginia Line, which serves the Virginia Street corridor from downtown Reno to Meadowood Mall. RTC RAPID includes level-boarding stations with more amenities. The service includes technology that allows the buses to communicate with the traffic signals to extend the green time several seconds for the bus. Design improvements help vehicles move around other traffic. The RAPID currently operates between 4<sup>th</sup> STREET STATION and Meadowood Mall and runs seven days a week. This service was recently extended further north on Virginia Street to provide a stronger connection between the University of Nevada – Reno (UNR), downtown Reno, and the Midtown neighborhood. In addition, a second RAPID route, called the Lincoln Line, went into operation December 2018 after construction was completed to provide RTC RAPID service on the 4<sup>th</sup> Street/Prater Way corridor between CENTENIAL PLAZA STATION in Sparks and 4<sup>th</sup> STREET STATION in Reno for greater connectivity between downtown Reno and downtown Sparks.

**RTC REGIONAL CONNECTOR** – RTC REGIONAL CONNECTOR provides a commuter service between Reno and Carson City. The service operates Monday through Friday through a partnership between RTC and Carson City. This premium service carries over 36,000 passengers per year. This route is 33 miles each way and offers free WiFi. It runs three trips in the morning and three trips in the afternoon.

**RTC FlexRIDE** – RTC FlexRIDE is a curbside-to-curbside transit service available by requesting a ride through an app or by phone. Rides can be scheduled at a desired travel time and can be expected to arrive to the curbside closest to the rider’s location within eight to 15 minutes. Fares are the same as the standard RTC RIDE fares. RTC initiated the first FlexRIDE pilot program in Sparks in 2019 and added additional FlexRIDE zones in the North Valleys, Spanish Springs, and Somersett/Verdi in 2020. The convenience of this service has made it very popular with customers, and resulted in strong increases over previously offered fixed route services in those areas.

**Table 5-1**

<b>FY 2022* RTC RIDE System Performance Indicators</b>	
Total Number of Rides in FY 2022	3.6 million
Average Number of Rides per Day	9,855
Total Service Hours (Vehicle Revenue Hours)	183,049
Average Passengers per Service Hour	19.7
Route w/Highest Passengers per Service Hour	Route 11 (32.6)
Total RTC RAPID Ridership	607,734

\* FY 2022 was an anomaly as a result of multiple employee strikes that caused service to be significantly reduced.

**RTC ACCESS** – In 1988, RTC established RTC ACCESS and assumed direct responsibility for providing door-to-door transportation for people with disabilities in the Reno/Sparks urbanized area. RTC ACCESS operates 24 hours a day, 7 days a week, in compliance with Americans with Disabilities Act (ADA) regulations. In FY 2022, 118,659 rides were provided, with an average of

1.9 rides per service hour. Approximately 2,500 individuals are certified as ADA paratransit eligible in Washoe County. The RTC ACCESS fleet uses compressed natural gas (CNG), a cleaner burning fuel, for better air quality and lower emissions.

RTC ACCESS also services some areas in the community beyond the federally required ADA zone, which is defined by a ¼-mile buffer around all fixed routes (excluding the RTC REGIONAL CONNECTOR). Because RTC does not have the resources to provide fixed-route and paratransit service to all outlying areas of Washoe County, partnerships have been formed with not-for-profit providers that can serve areas beyond the urban core and other specialized transportation needs. With funds from the FTA's Section 5310 Program, RTC is able to offer competitive grant opportunities to organizations that provide enhanced mobility for seniors and persons with disabilities.

**Tahoe Area Regional Transit (TART)** – Tahoe Area Regional Transit (TART) began operation in February 1975 and is operated by Placer County, California. In 1985, RTC signed an interlocal cooperative agreement with Placer County to fund the extension of the TART system into the Incline Village/Crystal Bay area, which has since been amended to include participation with the Tahoe Transportation District (TTD). TART provides fixed-route service to people living in the communities of Tahoma, Homewood, Tahoe City, Kings Beach, Truckee and Incline Village with four fixed routes daily.

### **Prioritization of Public Transportation Improvements**

Federal legislation requires prioritization of projects in the RTIP. This requirement is consistent with existing RTC practices to evaluate the overall benefit of any public transportation project. The following issues are considered before changes in transit service are made:

1. What is the intent of the project and why is it needed?
2. What are the anticipated benefits?
  - a. What user groups or area of the community will benefit from the project?
  - b. What existing services or facilities are available to that group or area?
  - c. Will the project improve productivity?
  - d. Is the project self-sustaining after the initial funding?
3. How will the project improve the availability of public transportation?
  - a. Does the project enhance service level?
4. Does the project improve overall level of service performance standards?
5. Does the project provide air quality benefits?
6. What is the overall cost effectiveness of the project?



7. Does the project leverage other funding sources?

New or expanded service addresses, current and future demand, as well as the cost effectiveness of each service are also analyzed as part of this process. Capital improvements are prioritized by the RTC for inclusion in the RTIP.

### Service and Capital Strategies

Transit is recognized as an essential part of the local economy that helps thousands of Washoe County residents get to work each day. Transit helps shape development patterns and is an economic development tool that supports local zoning and land use policies. Transit also provides a critical public service to residents and visitors that do not drive or do not have an automobile. The environmental benefits of transit service are also well recognized – reducing the number of cars on the road reduces traffic congestion and air pollution.

Attracting new riders and encouraging current riders to take more trips on public transportation requires improving the customers' total transportation experience. It is important not only to expand service to new areas of the community and to make existing service more frequent where passenger loads warrant, but also to consider other factors including:

- How do passengers get to and from their bus stop?
- What is the waiting environment like?
- Do the buses run on-time?
- Are the vehicles and passenger amenities clean?
- Is sufficient information about bus stops, routes and schedules readily available to the public?
- How long does it take to travel from origin to destination?

The RTC must formulate service and capital strategies based on these factors to attract new riders and encourage existing riders to take more trips while balancing financial projections for the system in the future. Because of this, the fiscally constrained transit program maintains the existing service with the following modifications planned for FFY 2023 through FFY 2027:

- Construction and reconfiguration of bus rapid transit stations
- Expansion of FlexRIDE service to the South Meadows area of Reno
- Purchase of hydrogen fuel cell electric buses and associated infrastructure
- Reallocation of service hours to achieve greater efficiency
- Increase subsidy and expand eligibility for taxi bucks/Washoe Senior Ride Program
- Relocation of the Villanova Maintenance Facility and the Meadowood Mall Transfer Station.

The public transportation improvements for FFY 2023-2027 are contained in the project listing in Appendix A.

## Chapter 6: Project Prioritization

The RTP project prioritization framework is a crucial element in the CMP. The projects identified in the 2050 RTP were compiled from a variety of sources, including:

- The 2040 RTP (developed in 2017)
- Corridor plans and studies such as the South Meadows Multimodal Transportation Study, University Area Transportation Study, and other corridor plans
- Road Safety Assessments and Safety Management Plans
- Community workshops and other public comments
- A series of online surveys
- Input from local governing bodies
- Input from the 2050 RTP Agency Working Group, RTC Citizens Multimodal Advisory Committee (CMAC), RTC Technical Advisory Committee, and RTC Regional Road Impact Fee Advisory Committee.

After all project suggestions were reviewed for feasibility and any inconsistencies, each project was evaluated based on a series of criteria developed in support of the RTP guiding principles and CMP. Projects were distributed into one of the following four categories in an effort to establish a basis for comparison amongst similar project types.

- Freeway projects
- Capacity projects (widening or expansion of existing roadways, inclusive of multimodal amenities where feasible and appropriate)
- New roadways
- Multimodal projects (transportation infrastructure improvements exclusive of new capacity)

The framework described in the following sections was developed to assist in the prioritization process for regional roadway projects. It provided input and data for the RTC Board to consider during the project evaluation and selection process. It is important to note that a mathematical formula did not provide the final determination on project rankings and that professional judgement and community/agency staff input was considered by the RTC staff and Board in making final recommendations and decisions. Separate evaluation frameworks were applied to projects on existing roadways and construction of new roads.

### Evaluation Criteria for Projects on Existing Regional Roads

The factors for evaluating projects on existing roadways consists of the criteria below.

- Safety
- Traffic Congestion

- Bicycle & Pedestrian Score
- Project Readiness
- Equity
- Regional Plan Land Use Priority
- Pavement/Bridge Condition
- Flood Mitigation
- Private or Other Agency Funding
- Public Input
- Agency Working Group Input

## Evaluation Criteria for New Road Construction

For analysis of new roads, a different methodology was developed because safety, congestion, pavement condition, and other data used to evaluate projects on existing roads would not be available for new construction. While some of the evaluation criteria remain consistent between existing and new roads, additional criteria were added for a more comprehensive evaluation of new roads projects. For example, RTC developed cost estimates for each proposed new road project, identified the projected average daily traffic (ADT) that would use the road, and developed an estimate for cost per ADT. Below is the complete list of factors used to evaluate new roads projects.

- Project Readiness
- Equity
- Regional Plan Land Use Priority
- Flood Mitigation
- Private or Other Agency Funding
- Public Input
- Agency Working Group Input
- ADT
- Cost per ADT
- Emergency Response/Fire Evacuation
- Projects Identified in a Plan or Study

## Methodology

**Safety** An analysis of all regional roads and freeways was conducted based on the three most recent years of crash data available from NDOT. Projects were scored based on a combination of crash frequency, rate, and severity.

**Traffic Congestion** Traffic congestion is derived from 2020 (existing) traffic level of service as well as from the 2050 "no build" level of service obtained through the RTC Travel Demand Model.

**Bicycle & Pedestrian Score** The bicycle and pedestrian score for each project was provided by the rating identified in the RTC Bicycle and Pedestrian Master Plan when applicable.

**Project Readiness** This criterion is intended to reflect the analysis, community input, and vetting of projects that occurs through other stages of the planning process. It recognizes a commitment to completing a project that has progressed to the design phase, and the level of community support for projects that have been adopted into the Program of Projects or RTIP.

**Equity** Higher priority is given to the extent to which a project improves transportation in an underserved community. Additional emphasis on equity in the 2050 RTP was requested by the CMAC, and the following factors were considered in determining the level of equity a project has.

Is the project located in or in proximity to the following areas:

- Food desert as identified by the U.S. Department of Agriculture (USDA)
- Census tract with higher than Washoe County average proportion of disabled residents
- Census tract with higher than Washoe County average proportion of low-income households
- Census tract with higher than Washoe County average proportion of zero vehicle households
- Census tract with higher than Washoe County average proportion of minority residents
- Census tract with higher than Washoe County average proportion of residents age 65 and older
- Within ¼ mile of a school or hospital

**Regional Plan Land Use Priority** This criterion is based on the tiered land use system identified in the TMRPA Regional Plan. The policies in the Regional Plan support investment in the urban core.

**Pavement/Bridge Condition** This criterion recognizes the benefit of investing in the state of good repair for regional roads and bridges. Projects with a lower pavement condition index or bridge rating receive higher priority.

**Flood Mitigation** Projects that address a critical need for flood mitigation are given a higher priority. An example of this would be road access that has been cut off by flood waters for an extended period. Other projects that are identified as Truckee River Flood Projects are given medium priority.

**Private or Other Agency Funding** The purpose of this criterion is to recognize that the opportunity to maximize RTC revenues through public-private partnerships or financial participation of other agencies is a benefit to the region.

**Emergency Response/Fire Evacuation** This need was identified by both members of the public and the Truckee Meadows Fire Protection District. Proposed roadways that improve regional connectivity or provide a secondary route to isolated areas received higher priority. Project that provide improved access within a neighborhood or community received medium priority.

**Projects Identified in a Plan or Study** Similar to project readiness for projects on existing roads, this criterion is intended to reflect the analysis, community input, and vetting of projects that occurs through other stages of the planning process. It recognizes a commitment to completing a project that has been identified as a recommendation in an individual corridor or area study, apart from the RTP.

Following the project screening, RTC staff developed a draft fiscally constrained project listing for review by the RTC Agency Working Group, RTC advisory committees, and ultimately the RTC Board. The list was also provided for public comment prior to finalizing the RTP.

## Chapter 7: Financial Plan

### Financial Constraint

Federal transportation legislation (currently IIJA) requires that the RTIP include a financial plan that demonstrates how the RTIP can be implemented and indicates the different sources that are reasonably expected to be made available over the term of the document. Since 1991, fiscal constraint has been a key component of the statewide and metropolitan transportation planning processes. Fiscal constraint means that the RTP, RTIP, and STIP include sufficient financial information to demonstrate that the projects included in those documents can be implemented using committed, available, or reasonably available federal, state, local, and private revenues, with the assurance that the federally supported transportation system is being adequately operated and maintained.

The eSTIP is structured in a manner that projects may only be included in the four or five years of the program if funds for those projects are “available” or “committed.” Federal funding reflected in the eSTIP for projects is based on full apportionment by fund source as identified from the FHWA Financial Management Information System (FMIS) 10A report. All funding for projects is required to be identified by federal fund source, required matching source, and by phase by year. The cumulative total of all funds should not exceed the total of funds reasonably available to the RTC or NDOT. Project funding is projected to increase for each year of the RTIP per the estimates of available IIJA funds. However, IIJA funding is apportioned annually and the amount of available funds projected in future years is subject to change. Federal funding amounts are updated at the beginning of each active fiscal year based on the FMIS 10A report.

The RTIP includes all modes of transportation, including transit (both operations and maintenance), street widenings, new streets, operations and maintenance of the street network, and bicycle and pedestrian facilities.

The RTP identifies financial assumptions that were developed in a coordinated effort with the local jurisdictions, state and federal agencies and the other MPOs in the state. Partners in the effort included:

- Federal Highway Administration
- Federal Transit Administration
- Nevada Department of Transportation
- Nevada Department of Motor Vehicles
- City of Reno
- City of Sparks
- Washoe County
- Carson Area MPO
- Tahoe Regional Planning Agency
- Regional Transportation Commission of Southern Nevada

## Financial Assumptions Summary

To comply with IJA requirements, RTC has prepared the following FFY 2023-2027 RTIP financial assumptions summary. This summary is intended to establish and document the levels of funding anticipated to be made available for the implementation of this improvement program with each fund source addressed separately.

### Local Fund Sources

There were several initiatives that made additional local funding available to the RTC. In 1982, voters approved of a 1/4% sales tax dedicated to public transportation. In 2003, with the approval of Washoe County ballot question WC-2, a 1/8% sales tax was added to implement road and transit projects and fuel tax indexing was implemented based on the Consumer Price Index (CPI). The 1/8% sales tax was split evenly between road and transit projects. In 2008, with the approval of Washoe County ballot question RTC-5, the CPI indexing was discontinued in favor of new indexing provisions calculated on the Producer Price Index (PPI).

**Fuel Tax** Following passage of RTC-5, legislation was approved in 2009 to index fuel to PPI and additional bases were added including federal, state, diesel and alternative fuels. Eligible uses for fuel tax include overlays, reconstruction and new construction for regional streets included in the regional road system. The RTC dedicates a portion of this funding source to preservation of the existing regional network.

**Transit Sales Tax** The single most important funding source for transit in Washoe County continues to be the dedicated 5/16 cent sales tax (comprised of the 1/4% and half of the 1/8% sales tax provisions). The revenue generated by this tax provided more than half the funds necessary for RTC to operate RTC RIDE and RTC ACCESS and to contribute to the TART service. Based on historic trends, revenue is expected to grow at an annual rate of 5% for the base year of 2016. The amount of available sales tax revenue will greatly affect the level of public transit service RTC can provide.

**Road Sales Tax** The other half of the 1/8% sales tax is allocated to road projects. This funding source has been used for the pavement preservation program.

**Regional Road Impact Fees (RRIF)** Impact fees are levied on new development to offset the cost of providing specific infrastructure improvements necessary to serve that new development. New development can be required to improve and add facilities necessary to maintain an established policy level of service (LOS). Impact fees are calculated and levied on the new development based on the degree that they contribute to the need for identified improvements. The Regional Road Impact Fee (RRIF) was implemented in October 1995 with the 7<sup>th</sup> Edition effective as of January 31, 2022. With the current growth in development that



the Truckee Meadows is experiencing, the revenue generated by this program is anticipated to be more robust than in recent years.

## State Funding Sources

State funding sources include gas tax, special fuel (diesel) tax, vehicle registration fees, motor carrier fees, driver's license fees and petroleum cleanup funds. For the purposes of this document, funding is generally from State Gas Tax and accounts for roughly \$264 million in funding for FFY 2023-2027.

## Federal Funding Sources

Federal funds for transportation are collected nationally and allocated back to the states through a series of formulas and grants under the existing transportation legislation (IIJA). One of the benefits of IIJA is that it provides long-term funding certainty for surface transportation. Overall, IIJA largely maintains the program structures and funding shares between highways and transit that were introduced under MAP-21. Federal funding programs require a state or local contribution of funds toward the cost of a project which is referred to as matching funds. The typical match for street and highway programs is five percent and for transit programs it is generally 20 percent.

IIJA programs available to the RTC and assumed in this document include:

**National Highway Performance Program (NHPP)** Funds are used to support the condition and performance of the NHS, for the construction of new facilities on the NHS and to ensure that investments of federal-aid funds in highway construction are directed to support progress toward the achievement of performance targets to be established in the state asset management plan.

Within the Truckee Meadows, NDOT has directed NHPP funding to a variety of projects and programs in the I-80 and I-580/US Highway 395 corridors. For planning and programming purposes, the RTIP shows approximately \$257 million in NHPP funding for these projects in the FFY 2023-2027 timeframe. An additional \$2.2 million is programmed under the Railway-Highway Crossings Program.

**Surface Transportation Block Grant Program (STBG)** Flexible funding that may be used for projects to preserve or improve conditions and performance on any federal-aid highway, bridge projects on any public road, facilities for non-motorized transportation, transit capital and public bus terminals and facilities. These funds provide NDOT and the RTC with the opportunity to program funds for new construction, maintenance, transit, ridesharing/employer trip reduction (ETR), centralized traffic signal control systems and traffic management programs. The RTIP includes STBG-Statewide and STBG Washoe County Urbanized Area (STBG-Local) funding categories. STBG-Local funds allocated to the region in the FFY 2023-2027 time period

total approximately \$71.8 million, while STBG-Statewide is around \$84.7 million. There are additional funds being allocated within Washoe County in the form of HSIP (safety) of roughly \$1.4 million.

**Congestion Mitigation and Air Quality Improvement Program (CMAQ)** Flexible funding for transportation projects and programs to help meet the requirements of the Clean Air Act through reduction of congestion and improvement of air quality. This program requires specific analysis related to project selection whereby only projects for which air quality benefits are demonstrated are eligible. To support requirements of the Clean Air Act, the following projects are being funded in the RTIP:

1. Traffic Management Program
2. Replacement of the public transit (RTC RIDE, RTC ACCESS) fleets
3. Intersection Improvement Program
4. Trip Reduction Programs

CMAQ funds can only be expended in areas identified by the U.S. Environmental Protection Agency (EPA) as being in non-attainment of a national air quality standard or in maintenance areas, which subsequently receive an attainment designation from EPA. Within Nevada, these funds are divided between Clark County and Washoe County based upon an approved formula that considers population and the severity of the area's carbon monoxide and ozone air pollution problems. CMAQ funding cannot be used for projects that result in new capacity for single-occupant vehicles. For programming purposes, it has been assumed that CMAQ funding will be nearly \$34.3 million over the five-year term.

All of the RTIP projects considered for CMAQ funding are identified in the 2050 RTP, which identifies several transit projects as a priority for CMAQ funding. For example, the conversion of diesel buses to electric or hybrid-electric vehicles generates a proven reduction in air pollutants. CMAQ has been an important source of funding for the now completed conversion of the RTC bus fleet to cleaner fuels. In addition, the expansion of the RTC Bus RAPID Transit system, which contains the highest ridership of all the fixed route operations, is a high priority for CMAQ funding. The Trip Reduction Program, which helps fund a portion of the RTC VANPOOL program, is also eligible for CMAQ funding. The RTC VANPOOL program is the RTC's fastest growing public transportation program. Further, the RTC has implemented a successful microtransit service (FlexRIDE) in various areas—or “zones”—throughout the region. The program offers mobility options for people who may live or work outside of the RTC fixed-route service area.

**Transportation Alternatives Set-Aside Program (TA Set-Aside)** Funds are for a variety of alternative transportation projects such as bicycle or pedestrian improvements and Safe Routes to School programs. This RTIP assumes \$2.4 million in TA Set-Aside funding over the five-year term.

**FTA Section 5307** Provides grants to urbanized areas with a population of 50,000 or more to support public transportation. The program remained largely unchanged under IIJA. The funds projected to be available each year for urbanized areas with populations more than 200,000 are based on a formula that considers the population and density of the region as well as revenue vehicle miles of service. The federal contribution is up to 80% in capital expenditures with a required local match of at least 20%. Approximately \$44.8 million in 5307 funding has been allocated to the FFY 2023-2027 RTIP.

**FTA Section 5310** The Section 5310 Enhanced Mobility of Seniors and Individuals with Disabilities Program is a federal program that aims to improve mobility for seniors and individuals with disabilities by removing barriers to transportation service and expanding transportation mobility options. This program supports transportation services planned, designed, and carried out to meet the special transportation needs of seniors and individuals with disabilities. Approximately \$0.6 million in 5310 funding has been allocated to the FFY 2023-2027 RTIP.

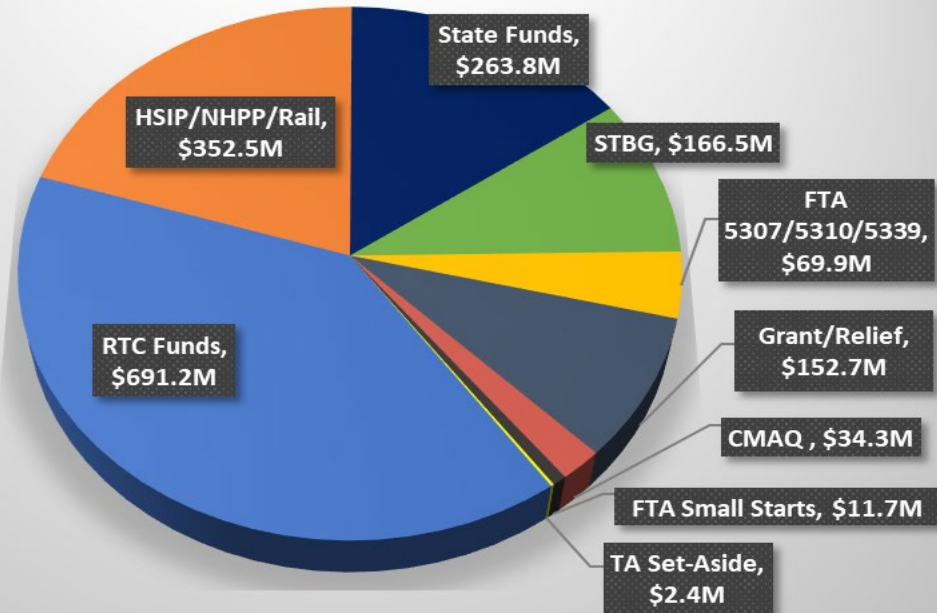
**FTA Section 5339** With the passage of MAP-21, Section 5339 was converted from a discretionary program to a formula-based program, and has remained formula-based under IIJA. Section 5339 provides funding to replace, rehabilitate and purchase buses and related equipment, and to construct bus-related facilities. Approximately \$21.7 million in 5339 funding has been allocated to the FFY 2023-2027 RTIP.

## **Financial Summary**

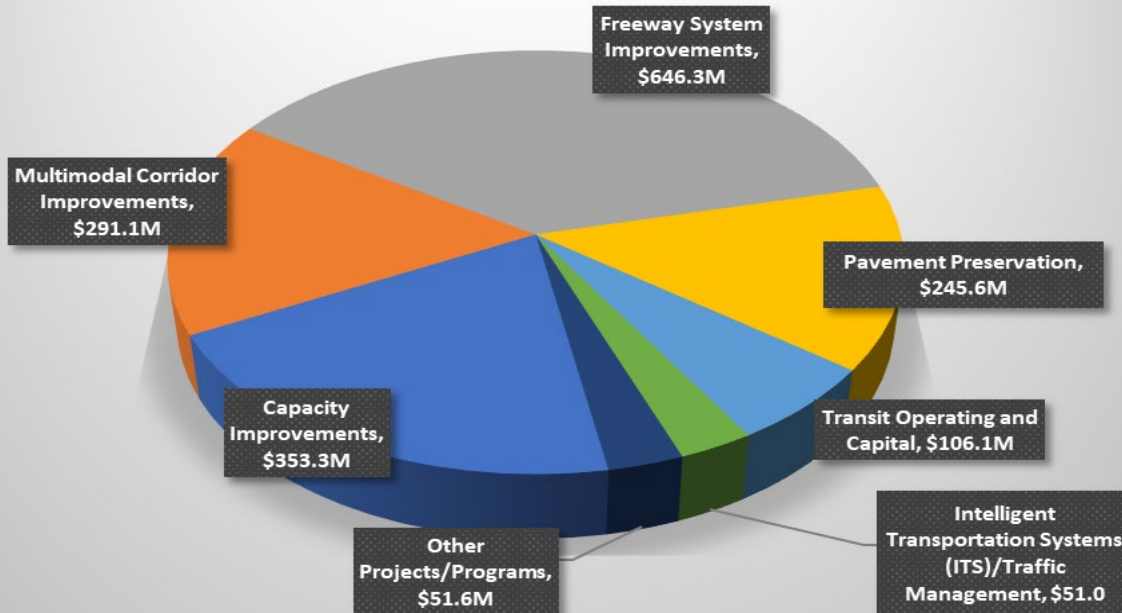
Figure 7-1 represents a summary of revenues and expenditures for the FFY 2023-2027 RTIP. Revenues are categorized by the funding source including STBG (Statewide and Local), CMAQ, NHPP, HSIP, FTA (5307, 5310, 5339), TA Set-Aside funding, Discretionary Funds (FTA Small Starts/other potential grants), RTC Funds, and State Funds. Expenditures are divided into the transportation mode or program in which the funds are to be expended.

**Figure 7-1**  
**FFY 2023-2027 RTIP Revenues and Expenditures**

**FFY 2023-2027 Revenues - \$1.75B**



**FFY 2023-2027 Expenditures - \$1.75B**



## Chapter 8: Air Quality Analysis and Conformity Determination

### Meeting Federal Requirements

The Clean Air Act Amendments (CAAA) of 1990 require that each state environmental agency develop a State Implementation Plan (SIP). The SIP establishes actions designed to improve air quality and meet National Ambient Air Quality Standards (NAAQS) for each criteria air pollutant, according to the schedules included in the CAAA.

Since emissions from motor vehicles make a significant contribution to air pollution, the CAAA also requires that transportation officials make a commitment to programs and projects that will help achieve air quality goals including:

- Providing for greater integration of the transportation and air quality process
- Ensuring that transportation plans, programs and projects conform with the SIP
- Reduction in the growth in vehicle miles traveled (VMT) and congestion in areas that have not attained the EPA's air quality standards.

Conformity for the RTP and this RTIP is demonstrated when projected regional emissions generated by the plan and RTIP do not exceed the region's motor vehicle emissions budgets as established by the SIP. While the MPO is ultimately responsible for making sure a conformity determination is made, the conformity process depends on federal, state and local transportation and air quality agencies working together to meet the transportation conformity requirements. The roles and responsibilities of the partner agencies involved in the air quality conformity analysis are defined in the Washoe County Transportation Conformity Plan.

### Status of Air Quality Pollutants

Criteria pollutants are considered on a county-wide basis if actual pollutant levels are exceeded outside of the core area of the Truckee Meadows. The core area of the Truckee Meadows is designated as the Hydrographic Area #87 (HA87) as shown in Figure 8-1.

**Figure 8-1  
Reno/Sparks Hydrographic Area #87**

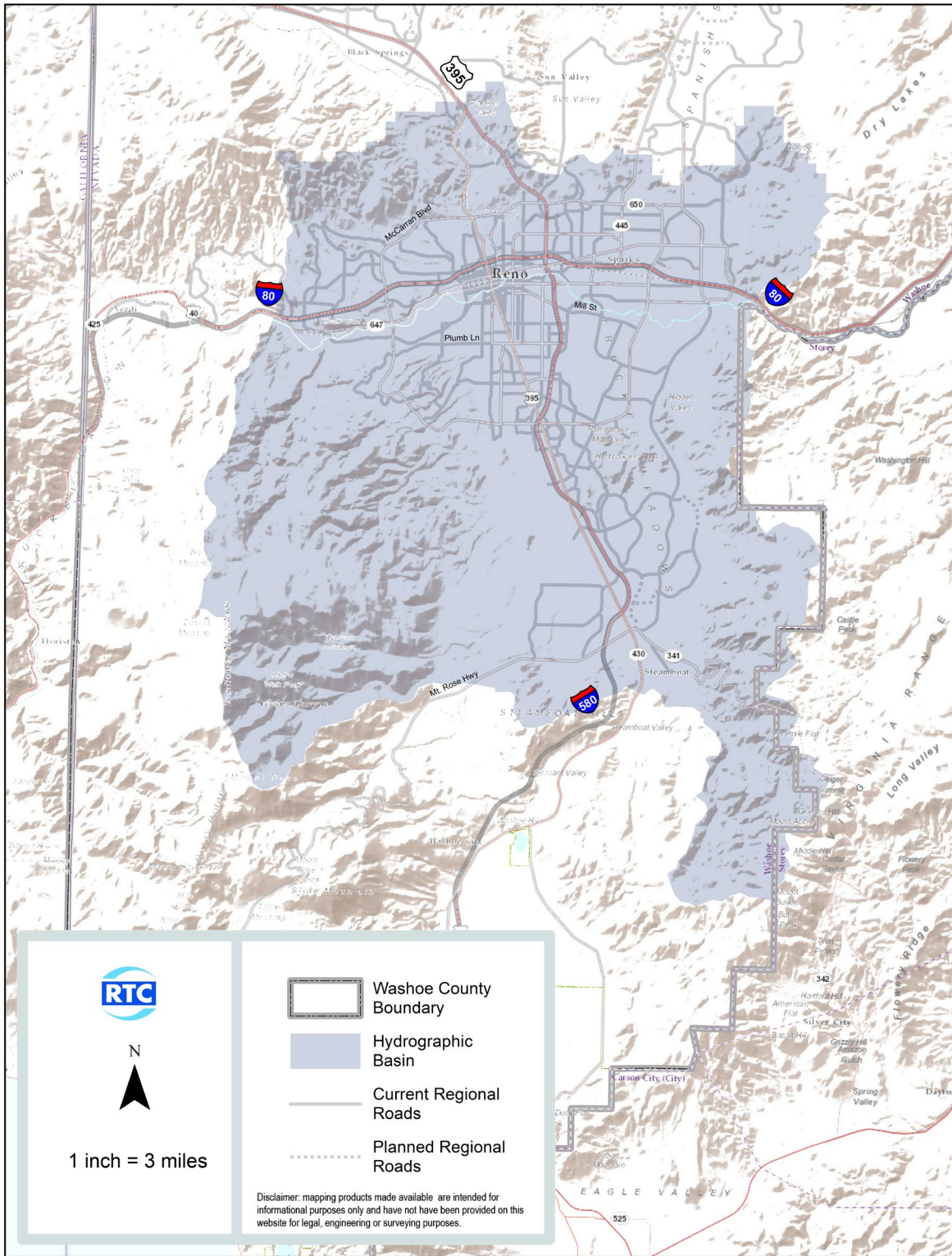


Table 8.1 summarizes Washoe County’s current design values. Design values are the statistic used to compare ambient air monitoring data against the NAAQS to determine designations for each NAAQS.

**Table 8-1  
Design Values and Attainment Status (as of December 31, 2022)**

Pollutant (Averaging Time)	NAAQS		Designations	
	Level	Design Value	Unclassifiable/ Attainment, or Maintenance	Non- Attainment (classification)
O <sub>3</sub> (8-hour)	0.070 ppm	0.073 ppm	All HA’s	---
PM <sub>2.5</sub> (24-hour)	35 µg/m <sup>3</sup>	78 µg/m <sup>3</sup>	All HA’s	---
PM <sub>2.5</sub> (Annual)	12.0 µg/m <sup>3</sup>	11.0 µg/m <sup>3</sup>	All HA’s	---
PM <sub>10</sub> (24-hour)	150 µg/m <sup>3</sup>	5.3 Expected Exceedances	All HA’s <sup>1</sup>	---
CO (1-hour)	35 ppm	2.6 ppm	All HA’s	---
CO (8-hour)	9 ppm	2.1 ppm	All HA’s <sup>2</sup>	---
NO <sub>2</sub> (1-hour)	100 ppb	47 ppb	All HA’s	---
NO <sub>2</sub> (Annual Mean)	53 ppb	12 ppb	All HA’s	---
SO <sub>2</sub> (1-hour)	75 ppb	3 ppb	All HA’s	---
Pb (Rolling 3-month average)	0.15 µg/m <sup>3</sup>	n/a	All HA’s	---

<sup>1</sup>Maintenance Area for PM<sub>10</sub> (1<sup>st</sup> 10-year maintenance plan expires January 6, 2026)

<sup>2</sup>Maintenance Area for CO (2<sup>nd</sup> 10-year maintenance plan expires October 31, 2026)

Source: 2013-22 Washoe County, Nevada Air Quality Trends Report

In 2015, EPA strengthened the 8-hour ozone standard from 75 to 70 ppb. The State of Nevada recommended that Washoe County be designated as attainment of the standard based on recent air monitoring data (2013-15) and EPA’s exclusion of several wildfire-related ozone exceptional events.

Regional emissions analyses were performed for each pollutant to document conformity with the CAAA as part of the RTP. The RTC, in collaboration with local agencies, has also been implementing programs that reduce motor vehicle emissions in the region.

## RTC Travel Demand Model and EPA MOVES Emission Model

The RTC's travel demand model was developed on the TransCAD platform. The model uses the 2020 Consensus Forecast population and employment provided by the TMRPA. EPA's Motor Vehicle Emission Simulator (MOVES) is a state-of-the-science emission modeling system that estimates emissions for mobile sources at the national, county, and project level for criteria air pollutants, greenhouse gases, and air toxics. MOVES3 is now the latest official version of MOVES. The analysis uses MOVES3 to calculate emission data. 2020, 2025, 2030, 2040 and 2050 networks were established for the RTP air quality analysis. The 2020 network consists of the current roadway network and the current transit network. Each of the remaining networks is comprised of the previous model year network with the capacity related projects and transit service changes included in the RTP.

### Air Quality Analysis – Plan Requirements

Federal regulations are specific in defining the level of air quality analysis necessary for incorporation into the RTP. Section 93, Title 40 of the Code of Federal Regulations (CFR) dated August 15, 1997 (effective September 15, 1997), pertains to the criteria and procedures necessary to analyze the air quality impacts of the RTP. For the purposes of an air quality determination, the analysis years are 2020, 2025, 2030, 2040 and 2050. No air quality analysis is required for the street and highway projects identified as unfunded needs. A summary of requirements is listed below:

- A. The RTP must contribute to emission reductions in CO non-attainment/maintenance areas.
- B. Air quality analysis years must be no more than 10 years apart.
- C. In CO and PM<sub>10</sub> non-attainment/maintenance areas, analysis must be performed for both pollutants.
- D. The last year of the RTP (2050) shall also be an analysis year.
- E. An analysis must be performed for each year contained in the motor vehicle emission budget (MVEB) for the HA87 for both CO and PM<sub>10</sub>, as budgets have been established for these pollutants.
- F. For both CO and PM<sub>10</sub>, the analysis of emissions for the required years cannot exceed the MVEB.

### Air Quality Analysis – Crediting Provisions

Federal regulations also allow for crediting procedures over the life of the RTP for the implementation of Transportation Control Measures (TCMs) in which emissions reductions can



be quantified. These TCMs are critical to areas such as Washoe County that have experienced significant growth in population and VMT, and are expected to continue to do so. Several specific TCM measures are in progress or planned in Washoe County that will have quantifiable emissions reductions. These include:

- A. Traffic signal optimization program.
- B. Conversion of the public transit fleet to cleaner fuels.
- C. Implementation of trip reduction programs.

These TCMs have been the focus of studies to quantify the air quality benefit of each. The TCMs are described below. The RTC is not currently taking any credit for reduced emissions associated with these TCMs but may choose to take credit in the future, if conditions warrant.

#### ***A. Traffic Signal Optimization/Timing Upgrade Program***

Traffic signal coordination and improvements seek to achieve two primary objectives: 1) improved traffic flow resulting in improved level of service and 2) mobile source emission reductions through decreased delay, fewer accelerations/decelerations and a decreased number of stops. The RTC has reviewed several studies and federally accepted models to quantify the reduction of mobile emissions from signal coordination programs. These include signal coordination studies conducted by several cities in Southern California and the California Department of Transportation (Caltrans). A comparison of before and after field studies was conducted and the improvements in all three peak periods were noted. Examples included a statewide average reduction of 14 seconds in stop delay and a 12% reduction in the number of stops per mile in the afternoon peak period. Several methodologies were used to take the results of studies to quantify the emission reductions from signal coordination programs.

The pollution reduction results (tons/per day or percentage reduction) from each model vary as some models focus on corridor specific reductions while the others are more of an area-wide reduction projection. Pollutant reductions ranged from 11% along specific corridors to three to four percent on a regional level.

The RTC has initiated a region-wide traffic signal optimization and improvements program to enhance the capacity of the existing system and reduce traffic congestion in the region. This is an ongoing program that will allow nearly 400 intersections in the Truckee Meadows to be coordinated.

#### ***B. Conversion of RTC ACCESS and RTC RIDE Fleets to Alternative or Cleaner Burning Fuels***

Between the RTC RIDE public transit and RTC ACCESS paratransit services, RTC typically provides approximately 8 million passenger trips and 3 million miles of service annually. As noted in Chapter 5, service was drastically reduced due to ongoing strikes, but service expansions are planned in the coming years. While transit, even in full operating conditions, represents a small percentage of total daily travel, it is important in terms of air quality. All RTC RIDE buses are comprised of electric, hybrid diesel-electric and bio-diesel vehicles. RTC ACCESS cut-away vehicles are fueled by Compressed Natural Gas (CNG). These vehicles can reduce mobile

emission totals. Estimates by the California Air Resources Board comparing standard urban diesel and biodiesel (or CNG) determined that NO<sub>x</sub> emissions from vehicles with CNG or cleaner burning diesels were reduced approximately 60%.

RTC currently has 23 zero emission electric buses which are primarily assigned to the Virginia and Lincoln Line RAPID corridors. In addition, RTC is beginning the use of hydrogen fuel cell technology for its next generation of zero emission vehicles with two scheduled for delivery in 2024 and six additional vehicles scheduled for delivery in 2025.

**C. Trip Reduction Programs**

The RTC’s trip reduction program, RTC SMART TRIPS, encourages the use of sustainable travel modes and trip reductions strategies such as telecommuting, compressed work weeks, and trip chaining. Major components of the program include a bus pass subsidy program in which the RTC matches an employer’s contribution to their employees’ 31-day transit passes up to 20%; a subsidized vanpool program, RTC VANPOOL; and an online trip matching program, RTC TRIP MATCH, that makes it quick, easy, and convenient to look for carpool partners as well as bus, bike, and walking buddies for either recurring or one-time trips. One of the most common deterrents to ridesharing is the fear of being “stranded.” Consequently, people who either carpool or vanpool to work can sign up for the Guaranteed Ride Home program and be reimbursed for a taxi ride home up to four times a year if an unexpected event prevents normal ridesharing arrangements from working. Making trips safely on foot and by bicycle are also promoted by the RTC SMART TRIPS program throughout the year.

The goals of these programs are to promote trip reduction on a region-wide level, improve air quality, and reduce vehicle miles of travel and traffic congestion. During FY 2022 the air quality benefits of the program were substantial, as shown in Table 8-2.

**Table 8-2  
RTC VANPOOL Air Pollution Reductions (FY 2022)**

<b>RTC VANPOOL Air Pollution Reductions (kg/day)</b>	
Volatile organic compounds (VOC)	0.955
Nitrogen Oxide (NO <sub>x</sub> )	3.684
Carbon Monoxide (CO)	107.818
Particulate Matter (PM <sub>10</sub> )	0.419
Particulate Matter (PM <sub>2.5</sub> )	0.146
Carbon Dioxide (CO <sub>2</sub> )	20.437

The data include the number of people in each vanpool and the average daily trip mileage. The air pollution calculation was obtained by multiplying the number of passenger trips for each vanpool per month by the average daily trip mileage for each vanpool per month and totaling those results to estimate the total VMT eliminated through the program due to the vanpool passengers not driving alone to work. The reduction in VMT was then multiplied by the pollutant factors per mile with those results outlined in the chart below. The emissions factors per mile for each pollutant were provided by WCHD-AQMD.

### Air Quality Analysis

An emission test on both CO and PM<sub>10</sub> must be successfully completed to make a finding of conformity. The area of analysis for these pollutants is the HA87. As stated previously, the CO and PM<sub>10</sub> emissions for the required analysis years cannot exceed the established motor vehicle emissions budget. Analysis is performed for 2020, 2025, 2030, 2040 and 2050 for both pollutants.

To initiate the air quality conformity determination, the emission levels for the pollutants in each analysis year are generated. The VMT for each facility type is derived from the RTC’s travel demand model. Many local roads are approximated as centroid connectors in the model network. Since centroid connectors are not actual roads, the VMTs for local roads are estimated as 11.67% (urban) and 6.57% (rural) of the total VMTs based on NDOT’s 2019 Annual Vehicle Miles of Travel Report (August 2020). Average speed by facility type from RTC’s travel demand model is provided as an input to the MOVES model. Total emissions for each facility type are then added to get a daily emission total for the roadway system in the analysis area. Emission totals are shown in pounds per day (lbs. /day). The Interagency Air Quality Consultation Team recommended approval of the air quality analysis on February 23, 2021.

### CO Analysis

The MVEB for carbon monoxide (CO), effective October 31, 2016, is shown in Table 8-3, which also includes the CO emissions for all analysis years of the RTP. All RTP analysis years are within the MVEB.

**Table 8-3  
CO Emissions Analysis  
(lbs/day)**

Analysis Year	MVEB	RTP Analysis
2020	172,670	64,477
2025	171,509	55,872
2030	169,959	47,192
2040	169,959	40,391
2050	169,959	44,143

### PM<sub>10</sub> Analysis

The MVEB for PM<sub>10</sub>, effective January 6, 2016, is shown in Table 8-4, which also includes the PM<sub>10</sub> emissions for all analysis years of the RTP. All RTP analysis years are within the MVEB.

**Table 8-4**  
**PM<sub>10</sub> Emissions Analysis**  
**(lbs/day)**

Analysis Year	MVEB	RTP Analysis
2020	6,088	3,514
2025	6,473	3,555
2030	6,927	3,751
2040	6,927	4,030
2050	6,927	4,501

### Summary

A strong commitment to fund and implement feasible TCM measures must be made if acceptable air quality standards are to be sustained. The local jurisdictions and NDOT, through the RTP process, have made the commitment to fund TCMs such as ridesharing, traffic flow improvements, signal coordination, and conversion of public transit fleets to cleaner burning fuels. The 2050 RTP includes significant investments in bicycle and pedestrian infrastructure, consistent with the Complete Streets Master Plan adopted by RTC in 2016. Based on existing and planned commitments, the air quality analysis conducted in this chapter demonstrates that the required air quality conformity determination can be made and the RTP is shown to be in conformance with federal air quality regulations.

## **Appendix A: FFY 2023-2027 Regional Transportation Improvement Program Project Listing**

The FFY 2023-2027 Regional Transportation Improvement Program project listing is provided on the following pages. The list has been sorted by project type and shows the project description, the project limits (where applicable), and includes the funding source by project phase (engineering/design, right-of-way, construction, or “other” for capital acquisition), the federal fiscal year each phase has been programmed, and the total project cost as well as the federal, state or local contributions. Projects were identified through outreach and coordination with the public and agency stakeholders through the development of the RTP.

Project cost estimates were derived from the RTP and the FY 2024 RTC Street and Highway Program of Projects. Planning level estimates are developed for each project based on the type of improvement to be implemented. As a project progresses into the design phase, a more detailed cost estimate is prepared to ensure adequate funding is available to construct the project.

**WA20190030** (Ver 6) 23-00 **LOCAL**

**Name:** Purchase Multiuse Path Maintenance Equipment

**Scope:** Purchase multiuse path maintenance equipment for each of the local jurisdictions (Washoe County and the Cities of Reno and Sparks) to remove debris and snow on multiuse/offstreet paths throughout the region.

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Other - Non construction related activities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$190,000	\$190,000
2024	Local Fund	\$0	\$0	\$0	\$10,000	\$10,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$200,000</b>	<b>\$200,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$200,000</b>	<b>\$200,000</b>

MPO **RTC Washoe**

Lead Agency **City of Reno**

**WA20230011** (Ver 1) 23-00 **FEDERAL**

**Name:** Rosewood Trailhead Project

**Scope:** To provide a safe pedestrian and bicycle access point to the Veterans multi-use pathway, that will further connect users to nearby trail systems. To commence a trailhead design process with a private-sector firm that will initiate a survey of the site, develop design opportunities, and culminate with the selection of facility options. Deliverables will also include a bid-ready set of plans, engineers estimate, and construction documents for the next phase of the project, or the Rosewood Trailhead Phase Two Construction Project (not funded as part of this project).

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Nearest Crossstreet:

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fund	\$8,684	\$0	\$0	\$0	\$8,684
2024	TAP WA STBG	\$165,000	\$0	\$0	\$0	\$165,000
<b>2023-2027 TOTAL</b>		<b>\$173,684</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$173,684</b>
<b>ALL YEARS TOTAL</b>		<b>\$173,684</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$173,684</b>

MPO **RTC Washoe**

Lead Agency **City of Reno**

**WA20230010** (Ver 1) 23-00 **FEDERAL**

**Name:** Broadhead Park Restoration Project

**Scope:** The Broadhead Park Restoration Project is a collaborative effort by the City of Reno and One Truckee River (OTR). The goal is to complete a riverbank restoration and vegetation management project at Brodhead Memorial Park along the Tahoe-Pyramid Trail. The project will address critical environmental issues facing the Truckee River including impaired water quality, erosion, aquatic wildlife habitat needs, and vegetation loss. The project is also engaging with the local neighborhood to create an invested stakeholder base to participate in the planning, implementation, and creation of a local neighborhood support system to provide ongoing care of the riverbank restoration.

**Project Type:** Landscape & Aesthetics **AQ:** Exempt, Other - Plantings, landscaping, etc. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Nearest Crossstreet:

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fund	\$0	\$0	\$36,182	\$0	\$36,182
2024	TAP WA STBG	\$91,930	\$0	\$595,523	\$0	\$687,453
<b>2023-2027 TOTAL</b>		<b>\$91,930</b>	<b>\$0</b>	<b>\$631,705</b>	<b>\$0</b>	<b>\$723,635</b>
<b>ALL YEARS TOTAL</b>		<b>\$91,930</b>	<b>\$0</b>	<b>\$631,705</b>	<b>\$0</b>	<b>\$723,635</b>

MPO **RTC Washoe**

Lead Agency **City of Reno**

**WA20230008** (Ver 1) 23-00 **FEDERAL**

**Name:** Plumas Street Sidewalk - Phase 2

**Scope:** Install sidewalk, curb and gutter on the west side of Plumas Street from Mountain View Drive to Brinkby Ave. This will improve safety, encourage non-motorized travel, improve access to transit, and reduce emissions and energy use by encouraging non-motorized travel.

**Project Type:** Road Improvement **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Mountain View Dr to Brinkby Ave of Distance (mile) .26

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fund	\$0	\$0	\$20,108	\$0	\$20,108
2024	TAP WA STBG	\$0	\$0	\$382,047	\$0	\$382,047
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$402,155</b>	<b>\$0</b>	<b>\$402,155</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$402,155</b>	<b>\$0</b>	<b>\$402,155</b>

MPO **RTC Washoe**

Lead Agency **City of Reno**

<b>WA20230009</b> (Ver 1) 23-00							<b>FEDERAL</b>
<b>Name:</b> Pedestrian Ramp Project - Phase 1							
<b>Scope:</b> This project will update several pedestrian ramps within various school zoning areas that are currently non-ADA compliant. These ramps provide access to a safe place to walk and a safe place to navigate intersections.							
<b>Project Type:</b> ADA		<b>AQ:</b> Exempt, Air Quality - Bicycle and pedestrian facilities.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> Various Locations					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	Local Fund	\$0	\$0	\$19,500	\$0	\$19,500	
2024	TAP WA STBG	\$0	\$0	\$370,500	\$0	\$370,500	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$390,000</b>	<b>\$0</b>	<b>\$390,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$390,000</b>	<b>\$0</b>	<b>\$390,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>City of Sparks</b>				

<b>WA20200019</b> (Ver 5) 23-00							<b>LOCAL</b>
<b>Name:</b> Purchase Multiuse Path Maintenance Equipment							
<b>Scope:</b> Purchase multiuse path maintenance equipment for each of the local jurisdictions (Washoe County and the Cities of Reno and Sparks) to remove debris and snow on multiuse/off-street paths throughout the region							
<b>Project Type:</b> Active Transportation (Bi		<b>AQ:</b> Exempt, Other - Non construction related activities.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> Not Location Specific					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$190,000	\$190,000	
2024	Local Fund	\$0	\$0	\$0	\$10,000	\$10,000	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$200,000</b>	<b>\$200,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$200,000</b>	<b>\$200,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>City of Sparks</b>				



**WA20200013** (Ver 3) 23-00 **FEDERAL**

**Name:** I-80 West, Reno - Bridge Replacements

**Scope:** Replace structures, I-770, I-774 E/W, I-775 E/W, H-767 E/W

**Project Type:** Bridge/Structures

**AQ:** Exempt, Safety - Non capacity widening or bridge reconstruction.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Bridge #: I -770

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	State Gas Tax	\$6,655,000	\$0	\$0	\$0	\$6,655,000
2024	BRIDGE FORMULA PROGRAM	\$0	\$0	\$28,500,000	\$0	\$28,500,000
2024	Highway Infra Bridge Replacement	\$0	\$0	\$11,400,000	\$0	\$11,400,000
2024	NHPP	\$0	\$0	\$59,315,625	\$0	\$59,315,625
2024	PROTECT PROGRAM	\$0	\$0	\$3,200,000	\$0	\$3,200,000
2024	STBG State-Wide	\$0	\$0	\$19,771,875	\$0	\$19,771,875
2024	State Gas Tax	\$0	\$550,000	\$0	\$0	\$550,000
2024	State Match - Nv	\$0	\$0	\$7,062,500	\$0	\$7,062,500
<b>2023-2027 TOTAL</b>		<b>\$6,655,000</b>	<b>\$550,000</b>	<b>\$129,250,000</b>	<b>\$0</b>	<b>\$136,455,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$6,655,000</b>	<b>\$550,000</b>	<b>\$129,250,000</b>	<b>\$0</b>	<b>\$136,455,000</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**

**WA20220031** (Ver 2) 23-00 **STATE**

**Name:** FR 09 Lockwood Dr. at Truckee River- replace B-1490 (on system bridge).

**Scope:** FR 09 Lockwood Dr. at Truckee River- replace B-1490 (on system bridge).

**Project Type:** Bridge/Structures

**AQ:** Exempt, Safety - Non capacity widening or bridge reconstruction.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Bridge #: B-1490

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2025	State Gas Tax	\$820,000	\$0	\$0	\$0	\$820,000
<b>2023-2027 TOTAL</b>		<b>\$820,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$820,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$820,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$820,000</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**

**WA20220001** (Ver 3) 23-00 **STATE**

**Name:** SR 659, N. McCarran Intersection Improvements

**Scope:** INTERSECTION AND SAFETY IMPROVEMENTS

**Project Type:** Interchange/Intersection

**AQ:** Exempt, Safety - Safety Improvement Program.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From WA MP 7.60 to WA MP 9.95 of Distance (mile) 2.35 Milepost begins at 7.6 ends at 9.95

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	State Gas Tax	\$207,000	\$0	\$0	\$0	\$207,000
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$784,000	\$0	\$784,000
2024	State Gas Tax	\$0	\$5,000	\$2,450,000	\$0	\$2,455,000
<b>2023-2027 TOTAL</b>		<b>\$207,000</b>	<b>\$5,000</b>	<b>\$3,234,000</b>	<b>\$0</b>	<b>\$3,446,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$207,000</b>	<b>\$5,000</b>	<b>\$3,234,000</b>	<b>\$0</b>	<b>\$3,446,000</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**

**WA20170146** (Ver 10) 23-00

**FEDERAL**

**Name:** US 395, North Valleys, McCarran to Golden Valley, Phase 1B

**Scope:** CONSTRUCT AUX LANE NB AND SB, CONSTRUCT TRAVEL LANE SB, CONSTRUCT NEW BRAIDED RAMP AT PANTHER VALLEY INTERCHANGE, AND REHAB EXISTING PAVEMENT

**Project Type:** Major/New Construction

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From McCarran to Golden Valley Structure of Distance (mile) 4.05 Milepost begins at 27.06 ends at 31.11

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	HIGHWAY INFRA COVID Statewide	\$0	\$0	\$22,771,613	\$0	\$22,771,613
2023	Hghwy Infra Statewide	\$0	\$0	\$13,919,540	\$0	\$13,919,540
2023	NHPP AC	\$0	\$0	\$22,000,000	\$0	\$22,000,000
2023	NHPP Exempt	\$0	\$0	\$16,836,191	\$0	\$16,836,191
2023	PROTECT PROGRAM	\$0	\$0	\$1,500,000	\$0	\$1,500,000
2023	SB 5	\$0	\$0	\$8,000,000	\$0	\$8,000,000
2023	STBG State-Wide AC	\$0	\$0	\$35,303,702	\$0	\$35,303,702
2023	State Gas Tax	\$4,261,550	\$0	\$50,197,810	\$0	\$54,459,360
2023	State Match - Nv	\$0	\$0	\$4,713,654	\$0	\$4,713,654
<2023	Prior	\$1,230,000	\$130,000	\$0	\$0	\$1,360,000
<b>2023-2027 TOTAL</b>		<b>\$4,261,550</b>	<b>\$0</b>	<b>\$175,242,510</b>	<b>\$0</b>	<b>\$179,504,060</b>
<b>ALL YEARS TOTAL</b>		<b>\$5,491,550</b>	<b>\$130,000</b>	<b>\$175,242,510</b>	<b>\$0</b>	<b>\$180,864,060</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**

**WA20200011** (Ver 3) 23-00 **STATE**

**Name:** Reno Spaghetti Bowl - Phase 2 Nugget Viaduct -Preliminary Design  
**Scope:** RECONSTRUCT I-80 FROM JUST EAST OF THE SPAGHETTI BOWL TO MCCARRAN BLVD EAST; REPLACE I-80 BRIDGE H-866 E/W OVER THE NUGGET CASINO; CONSTRUCT NEW INTERCHANGE AT KIETZKE LANE; RECONSTRUCT ROCK BLVD AND PYRAMID WAY INTERCHANGES (PRELIMINARY ENGINEERING)

**Project Type:** Major/New Construction **AQ:** Exempt, Other - Engineering studies. **TCM:**No **NDOT:** District 2

**County:** Washoe **Limits:** From EAST OF I 80/I 580/US 395 (SPAGHETTI BOWL) INTERCHANGE to EAST MCCARRAN BLVD (SR 659) of Distance (mile) 2.1 Milepost begins at 14.9 ends at 17

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	State Gas Tax	\$5,115,000	\$72,000,000	\$0	\$0	\$77,115,000
<b>2023-2027 TOTAL</b>		<b>\$5,115,000</b>	<b>\$72,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$77,115,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$5,115,000</b>	<b>\$72,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$77,115,000</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**

**WA20180057** (Ver 7) 23-00 **FEDERAL**

**Name:** US 395, North Valleys, Golden Valley to Stead, Phase 2 - capacity, operational, and pavement rehab  
**Scope:** GOLDEN VALLEY TO LEMMON VALLEY; CONSTRUCT AN ADDITIONAL TRAVEL LANE, AUX LANE, AND REHABILITATE EXISTING LANES NB & SB. LEMMON VALLEY TO STEAD; CONSTRUCT AN ADDITIONAL TRAVEL LANE AND REHABILITATE EXISTING LANES

**Project Type:** Preservation **AQ:** Non-Exempt **TCM:**No **NDOT:** District 2

**County:** Washoe **Limits:** Primary Interchange: Golden Valley, Secondary Interchange: Stead

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2026	INFRA Grant	\$0	\$0	\$88,900,000	\$0	\$88,900,000
2026	NHPP	\$0	\$0	\$57,000,000	\$0	\$57,000,000
2026	State Match - Nv	\$0	\$0	\$3,000,000	\$0	\$3,000,000
<2023	Prior	\$400,000	\$15,000	\$0	\$0	\$415,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$148,900,000</b>	<b>\$0</b>	<b>\$148,900,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$400,000</b>	<b>\$15,000</b>	<b>\$148,900,000</b>	<b>\$0</b>	<b>\$149,315,000</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**

<b>WA20200072</b> (Ver 5) 23-00							<b>FEDERAL</b>
<b>Name:</b> US 395, from Cold Springs to the State Line							
<b>Scope:</b> COLDMILL AND OVERLAY, PLANTMIX BITUMINOUS SURFACE AND OPEN GRADED WEARING COURSE							
<b>Project Type:</b> Preservation		<b>AQ:</b> Exempt, Safety - Pavement resurfacing and/or rehabilitation.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> From WA MP 38.37 to WA MP 42.16 of Distance (mile) 3.79 Milepost begins at 38.37 ends at 42.16					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	NHPP	\$0	\$0	\$9,269,625	\$0	\$9,269,625	
2023	STBG State-Wide	\$0	\$0	\$3,089,875	\$0	\$3,089,875	
2023	State Match - Nv	\$0	\$0	\$650,500	\$0	\$650,500	
<2023	Prior	\$320,000	\$0	\$0	\$0	\$320,000	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$13,010,000</b>	<b>\$0</b>	<b>\$13,010,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$320,000</b>	<b>\$0</b>	<b>\$13,010,000</b>	<b>\$0</b>	<b>\$13,330,000</b>	
MPO <b>RTC Washoe</b>				Lead Agency <b>Nevada DOT</b>			

<b>WA20200073</b> (Ver 5) 23-00							<b>STATE</b>
<b>Name:</b> SR 445, PYRAMID WAY - 3R AND ADA							
<b>Scope:</b> 2-3/4 INCH COLDMILL, 2 INCH PBS WITH 3/4 INCH OG AND 10% PATCHING. UPGRADE EXISTING RAMPS, DRIVEWAYS AND SELECT SIDEWALK LOCATIONS TO MEET ADA							
<b>Project Type:</b> Preservation		<b>AQ:</b> Exempt, Safety - Pavement resurfacing and/or rehabilitation.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> From Nugget Avenue to York Way of Distance (mile) 1.38 Milepost begins at 0 ends at 1.38					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	State Gas Tax	\$0	\$115,000	\$0	\$0	\$115,000	
2025	State Gas Tax	\$0	\$0	\$9,472,000	\$0	\$9,472,000	
<2023	Prior	\$330,000	\$0	\$0	\$0	\$330,000	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$115,000</b>	<b>\$9,472,000</b>	<b>\$0</b>	<b>\$9,587,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$330,000</b>	<b>\$115,000</b>	<b>\$9,472,000</b>	<b>\$0</b>	<b>\$9,917,000</b>	
MPO <b>RTC Washoe</b>				Lead Agency <b>Nevada DOT</b>			

WA20220002 (Ver 3) 23-00							FEDERAL
<b>Name:</b> I-80 Keystone Package 2, Phase 2B							
<b>Scope:</b> WIDEN FOR EB AUXILIARY LANE, SOUNDWALL INSTALLATION, BARRIER RAIL UPGRADE, CULVERT LINING AND REPLACEMENT, HARDWARE UPGRADE FOR ITS DEVICES, AESTHETIC AND FENCING IMPROVEMENTS							
<b>Project Type:</b> Preservation		<b>AQ:</b> Non-Exempt			<b>TCM:</b> No <b>NDOT:</b> District 2		
<b>County:</b> Washoe		<b>Limits:</b> From WA MP 10.68 to WA MP 12.45 of Distance (mile) 1.77 Milepost begins at 10.68 ends at 12.45					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	NHPP	\$0	\$0	\$19,712,500	\$0	\$19,712,500	
2024	State Match - Nv	\$0	\$0	\$1,037,500	\$0	\$1,037,500	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$20,750,000</b>	<b>\$0</b>	<b>\$20,750,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$20,750,000</b>	<b>\$0</b>	<b>\$20,750,000</b>	
<b>MPO</b> RTC Washoe				<b>Lead Agency</b> Nevada DOT			

WA20220016 (Ver 2) 23-00							STATE
<b>Name:</b> SR 659 East McCarran Blvd.- ADA Preservation							
<b>Scope:</b> SR 659 East McCarran Blvd.- ADA Preservation							
<b>Project Type:</b> Preservation		<b>AQ:</b> Exempt, Safety - Pavement resurfacing and/or rehabilitation.			<b>TCM:</b> No <b>NDOT:</b> District 2		
<b>County:</b> Washoe		<b>Limits:</b> From WA MP 17.0 to WA MP 18.0 of Distance (mile) 1 Milepost begins at 17.01 ends at 18.01					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	State Gas Tax	\$0	\$0	\$4,587,500	\$0	\$4,587,500	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$4,587,500</b>	<b>\$0</b>	<b>\$4,587,500</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$4,587,500</b>	<b>\$0</b>	<b>\$4,587,500</b>	
<b>MPO</b> RTC Washoe				<b>Lead Agency</b> Nevada DOT			

<b>WA20220017</b> (Ver 2) 23-00							<b>FEDERAL</b>
<b>Name:</b> US 395 North Valleys- Preservation							
<b>Scope:</b> Mill and overlay with hydraulic, ITS, and ramp improvements							
<b>Project Type:</b> Preservation		<b>AQ:</b> Exempt, Safety - Pavement resurfacing and/or rehabilitation.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> From WA MP 34.1 to WA MP 38.3 of Distance (mile) 4.13 Milepost begins at 34.13 ends at 38.26					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	State Gas Tax	\$225,000	\$0	\$0	\$0	\$225,000	
2026	NHPP	\$0	\$0	\$9,761,250	\$0	\$9,761,250	
2026	STBG State-Wide	\$0	\$0	\$3,253,750	\$0	\$3,253,750	
2026	State Match - Nv	\$0	\$0	\$685,000	\$0	\$685,000	
<b>2023-2027 TOTAL</b>		<b>\$225,000</b>	<b>\$0</b>	<b>\$13,700,000</b>	<b>\$0</b>	<b>\$13,925,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$225,000</b>	<b>\$0</b>	<b>\$13,700,000</b>	<b>\$0</b>	<b>\$13,925,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>Nevada DOT</b>				

<b>WA20220019</b> (Ver 3) 23-00							<b>STATE</b>
<b>Name:</b> I 580 Bowers to Mount Rose Highway- Preservation							
<b>Scope:</b> Mill and fill w/ OG, profile grind concrete sections							
<b>Project Type:</b> Preservation		<b>AQ:</b> Exempt, Safety - Pavement resurfacing and/or rehabilitation.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> From WA MP 5.4 to WA MP 14.95 of Distance (mile) 9.59 Milepost begins at 5.36 ends at 14.95					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	State Gas Tax	\$300,000	\$0	\$0	\$0	\$300,000	
2024	State Gas Tax	\$0	\$0	\$21,603,000	\$0	\$21,603,000	
<b>2023-2027 TOTAL</b>		<b>\$300,000</b>	<b>\$0</b>	<b>\$21,603,000</b>	<b>\$0</b>	<b>\$21,903,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$300,000</b>	<b>\$0</b>	<b>\$21,603,000</b>	<b>\$0</b>	<b>\$21,903,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>Nevada DOT</b>				

**WA20220020** (Ver 2) 23-00 **STATE**

**Name:** SR 659 North McCarran Blvd- Preservation

**Scope:** SR 659 North McCarran Blvd- Preservation

**Project Type:** Preservation

**AQ:** Exempt, Safety - Pavement resurfacing and/or rehabilitation.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From WA MP 10.6 to WA MP 12.9 of Distance (mile) 2.27 Milepost begins at 10.58 ends at 12.85

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2026	State Gas Tax	\$0	\$0	\$9,475,400	\$0	\$9,475,400
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$9,475,400</b>	<b>\$0</b>	<b>\$9,475,400</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$9,475,400</b>	<b>\$0</b>	<b>\$9,475,400</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**

**WA20220021** (Ver 2) 23-00 **FEDERAL**

**Name:** SR 445 Spanish Springs, Golden View Drive to Egyptian Drive- Preservation

**Scope:** 2.75" cold mill, 2" dense grade with 0.75" open grade

**Project Type:** Preservation

**AQ:** Exempt, Safety - Pavement resurfacing and/or rehabilitation.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From WA MP 4.5 to WA MP 9.0 of Distance (mile) 4.53 Milepost begins at 4.5 ends at 9.03

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	State Gas Tax	\$230,000	\$0	\$0	\$0	\$230,000
2025	NHPP	\$0	\$0	\$7,980,000	\$0	\$7,980,000
2025	STBG State-Wide	\$0	\$0	\$2,660,000	\$0	\$2,660,000
2025	State Match - Nv	\$0	\$0	\$560,000	\$0	\$560,000
<b>2023-2027 TOTAL</b>		<b>\$230,000</b>	<b>\$0</b>	<b>\$11,200,000</b>	<b>\$0</b>	<b>\$11,430,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$230,000</b>	<b>\$0</b>	<b>\$11,200,000</b>	<b>\$0</b>	<b>\$11,430,000</b>

MPO **RTC Washoe**

Lead Agency **Nevada DOT**



<b>XS20220034</b> (Ver 2) 23-00							<b>FEDERAL</b>
<b>Name:</b> SR 439 USA Parkway- Preservation							
<b>Scope:</b> Mill and fill W/OG, hydraulic improvements, safety improvements, and lighting.							
<b>Project Type:</b> Preservation		<b>AQ:</b> Exempt, Safety - Pavement resurfacing and/or rehabilitation.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Storey, Washoe		<b>Limits:</b> From ST MP 4.3 to WA MP 0.1 of Distance (mile) -4.14 Milepost begins at 4.26 ends at .12					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	State Gas Tax	\$300,000	\$0	\$0	\$0	\$300,000	
2027	NHPP	\$0	\$0	\$23,512,500	\$0	\$23,512,500	
2027	STBG State-Wide	\$0	\$0	\$7,837,500	\$0	\$7,837,500	
2027	State Match - Nv	\$0	\$0	\$1,650,000	\$0	\$1,650,000	
<b>2023-2027 TOTAL</b>		<b>\$300,000</b>	<b>\$0</b>	<b>\$33,000,000</b>	<b>\$0</b>	<b>\$33,300,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$300,000</b>	<b>\$0</b>	<b>\$33,000,000</b>	<b>\$0</b>	<b>\$33,300,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>Nevada DOT</b>				

<b>XS20220035</b> (Ver 2) 23-00							<b>FEDERAL</b>
<b>Name:</b> I-80, Fernley, Painted Rock Interchange to East Fernley Grade Separation - Preservation							
<b>Scope:</b> 2" cold mill, 2" dense grade with 0.75" open grade and various patching locations							
<b>Project Type:</b> Preservation		<b>AQ:</b> Exempt, Safety - Pavement resurfacing and/or rehabilitation.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Lyon, Washoe		<b>Limits:</b> From WA MP 41.5 to LY MP 5.8 of Distance (mile) -35.65 Milepost begins at 41.49 ends at 5.84					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2025	NHPP	\$0	\$0	\$14,250,000	\$0	\$14,250,000	
2025	STBG State-Wide	\$0	\$0	\$7,125,000	\$0	\$7,125,000	
2025	State Gas Tax	\$0	\$0	\$2,500,000	\$0	\$2,500,000	
2025	State Match - Nv	\$0	\$0	\$1,125,000	\$0	\$1,125,000	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$25,000,000</b>	<b>\$0</b>	<b>\$25,000,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$25,000,000</b>	<b>\$0</b>	<b>\$25,000,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>Nevada DOT</b>				

<b>WA20170130</b> (Ver 8) 23-00							<b>FEDERAL</b>
<b>Name:</b> Highland Avenue Railroad Crossing							
<b>Scope:</b> Install crossing surface improvements and adjustment to crossing signal arms							
<b>Project Type:</b> Railroad		<b>AQ:</b> Exempt, Safety - Railroad/highway crossing.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> Nearest Crossstreet: Highland Avenue					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	Local Fund	\$0	\$0	\$102,000	\$0	\$102,000	
2024	RAIL	\$0	\$0	\$918,000	\$0	\$918,000	
<2023	Prior	\$28,950	\$0	\$0	\$0	\$28,950	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$1,020,000</b>	<b>\$0</b>	<b>\$1,020,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$28,950</b>	<b>\$0</b>	<b>\$1,020,000</b>	<b>\$0</b>	<b>\$1,048,950</b>	
MPO <b>RTC Washoe</b>				Lead Agency <b>Nevada DOT</b>			

<b>WA20170132</b> (Ver 9) 23-00							<b>FEDERAL</b>
<b>Name:</b> Silver Lake Drive Railroad Crossing							
<b>Scope:</b> INSTALL CROSSING SIGNAL AND CROSSING SURFACE IMPROVEMENTS INCLUDING VEHICLE ASSEMBLY GATES, ADDITION OF PEDESTRIAN GATES, RAILROAD CABINET REPLACEMENT, RAILROAD TRAFFIC PREEMPTION AND OTHER IMPROVEMENTS.							
<b>Project Type:</b> Railroad		<b>AQ:</b> Exempt, Safety - Railroad/hwy crossing warning devices.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> Nearest Crossstreet: Silver Lake Drive					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	Local Fund	\$0	\$0	\$259,000	\$0	\$259,000	
2023	RAIL	\$0	\$0	\$1,240,500	\$0	\$1,240,500	
2023	State Match - Nv	\$0	\$0	\$500	\$0	\$500	
<2023	Prior	\$17,000	\$0	\$0	\$0	\$17,000	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$1,500,000</b>	<b>\$0</b>	<b>\$1,500,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$17,000</b>	<b>\$0</b>	<b>\$1,500,000</b>	<b>\$0</b>	<b>\$1,517,000</b>	
MPO <b>RTC Washoe</b>				Lead Agency <b>Nevada DOT</b>			

WA20210008 (Ver 4) 23-00							FEDERAL	
<b>Name:</b> FRWA51, Enhanced Safety Improvements								
<b>Scope:</b> INSTALL NEW SIGNAGE, ADJUST CONCRETE BARRIER RAIL AND NEW STRIPING FOR ENHANCED SAFETY IMPROVEMENTS ,FROM VILLANOVA DR, I 580 EXIT 65A, TO PLUMB LANE; MP WA 0.00 TO MP WA 0.246								
<b>Project Type:</b> Road Improvement		<b>AQ:</b> Exempt, Safety - Safety Improvement Program.					<b>TCM:</b> No	<b>NDOT:</b> District 2
<b>County:</b> Washoe		<b>Limits:</b> From Villanova Drive to Plumb Lane of Distance (mile) 0.25 Milepost begins at 0 ends at .25						
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL		
2023	HSIP	\$0	\$0	\$475,000	\$0	\$475,000		
2023	State Match - Nv	\$0	\$0	\$25,000	\$0	\$25,000		
<2023	Prior	\$0	\$5,000	\$0	\$0	\$5,000		
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$500,000</b>	<b>\$0</b>	<b>\$500,000</b>		
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$5,000</b>	<b>\$500,000</b>	<b>\$0</b>	<b>\$505,000</b>		
MPO <b>RTC Washoe</b>				Lead Agency <b>Nevada DOT</b>				

WA2012128 (Ver 10) 23-00							FEDERAL	
<b>Name:</b> Reno Freeway Service Patrol FFY23-26								
<b>Scope:</b> Provide cost-free assistance to motorists and first responders for a variety of roadway incidents.								
<b>Project Type:</b> Traffic Operations		<b>AQ:</b> Exempt					<b>TCM:</b> No	<b>NDOT:</b> District 2
<b>County:</b> Washoe		<b>Limits:</b> Various Locations						
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL		
<2023	Prior	\$0	\$0	\$0	\$6,638,716	\$6,638,716		
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$6,638,716</b>	<b>\$6,638,716</b>		
MPO <b>RTC Washoe</b>				Lead Agency <b>Nevada DOT</b>				

**WA20110314** (Ver 12) 23-00 **FEDERAL**

**Name:** Bicycle, Pedestrian, and ADA Improvements  
**Scope:** Implementation of prioritized bus stop improvements, bicycle and pedestrian projects from the Bicycle-Pedestrian Master Plan and ADA improvement projects from the ADA Transition Plan - Annual Program

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** Yes **NDOT:** District 2

**County:** Washoe **Limits:** Various Locations

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$0	\$0
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$3,000,000	\$0	\$3,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$3,000,000	\$0	\$3,000,000
<2023	Prior	\$400,000	\$0	\$13,600,000	\$0	\$14,000,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$6,000,000</b>	<b>\$0</b>	<b>\$6,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$400,000</b>	<b>\$0</b>	<b>\$19,600,000</b>	<b>\$0</b>	<b>\$20,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20150003** (Ver 10) 23-00 **FEDERAL**

**Name:** Truckee River Shared Use Path  
**Scope:** Construct Pathway

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** Yes **NDOT:** District 2

**County:** Washoe **Limits:** Truckee River Shared Use Path from John Champion Memorial Park to East side of the US 395/IR580 overpass of Distance (mile) .25

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	State Match - Nv	\$0	\$0	\$1	\$0	\$1
<2023	Prior	\$212,754	\$564,700	\$0	\$0	\$777,454
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$1</b>	<b>\$0</b>	<b>\$1</b>
<b>ALL YEARS TOTAL</b>		<b>\$212,754</b>	<b>\$564,700</b>	<b>\$1</b>	<b>\$0</b>	<b>\$777,455</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20170123** (Ver 9) 23-00 **LOCAL**

**Name:** Center Street Multimodal Improvements  
**Scope:** Construct two-way cycle track on the west side of Center Street and spot sidewalk improvements.

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** Yes **NDOT:** District 2

**County:** Washoe **Limits:** From S. Virginia Street to I-80 of Distance (mile) 1.4

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$10,000,000	\$0	\$10,000,000
<2023	Prior	\$1,080,000	\$0	\$0	\$0	\$1,080,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$10,000,000</b>	<b>\$0</b>	<b>\$10,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$1,080,000</b>	<b>\$0</b>	<b>\$10,000,000</b>	<b>\$0</b>	<b>\$11,080,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20170126** (Ver 9) 23-00 **FEDERAL**

**Name:** Vassar Street Multimodal Improvements  
**Scope:** Bicycle and sidewalk improvements.

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** Yes **NDOT:** District 2

**County:** Washoe **Limits:** From Kietzke Lane to Terminal Way of Distance (mile) .45

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	CMAQ - Washoe County	\$641,250	\$0	\$0	\$0	\$641,250
2024	Local Fuel Tax - RTCWA	\$33,750	\$0	\$0	\$0	\$33,750
2025	CMAQ - Washoe County	\$0	\$0	\$3,633,750	\$0	\$3,633,750
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$191,250	\$0	\$191,250
<b>2023-2027 TOTAL</b>		<b>\$675,000</b>	<b>\$0</b>	<b>\$3,825,000</b>	<b>\$0</b>	<b>\$4,500,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$675,000</b>	<b>\$0</b>	<b>\$3,825,000</b>	<b>\$0</b>	<b>\$4,500,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20170128** (Ver 8) 23-00 **LOCAL**

**Name:** Vine Street Bike Facility

**Scope:** Roadway reconfiguration and bicycle facilities.

**Project Type:** Active Transportation (Bi **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities.

**TCM:** Yes **NDOT:** District 2

**County:** Washoe

**Limits:** From Riverside Drive to University Terrace of Distance (mile) 0.85

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$1,130,000	\$0	\$0	\$0	\$1,130,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$10,170,000	\$0	\$10,170,000
<b>2023-2027 TOTAL</b>		<b>\$1,130,000</b>	<b>\$0</b>	<b>\$10,170,000</b>	<b>\$0</b>	<b>\$11,300,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$1,130,000</b>	<b>\$0</b>	<b>\$10,170,000</b>	<b>\$0</b>	<b>\$11,300,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20180059** (Ver 7) 23-00 **LOCAL**

**Name:** West 4th Street (Reno) Improvements

**Scope:** Construct enhanced sidewalks, bus/bike lanes, lighting, and intersection improvements.

**Project Type:** Active Transportation (Bi **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Vine St to Evans Ave of Distance (mile) 0.8

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$1,000,000	\$0	\$0	\$0	\$1,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$7,500,000	\$0	\$7,500,000
<b>2023-2027 TOTAL</b>		<b>\$1,000,000</b>	<b>\$0</b>	<b>\$7,500,000</b>	<b>\$0</b>	<b>\$8,500,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$1,000,000</b>	<b>\$0</b>	<b>\$7,500,000</b>	<b>\$0</b>	<b>\$8,500,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20190042** (Ver 5) 23-00 **LOCAL**

**Name:** Sun Valley Boulevard Corridor Improvements - Phase 2  
**Scope:** Multimodal improvements along the Sun Valley Boulevard corridor.

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Scottsdale Road to 7th Avenue of Distance (mile) 2.6 Milepost begins at 1 ends at 3.6

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$2,000,000	\$500,000	\$0	\$0	\$2,500,000
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$24,500,000	\$0	\$24,500,000
<b>2023-2027 TOTAL</b>		<b>\$2,000,000</b>	<b>\$500,000</b>	<b>\$24,500,000</b>	<b>\$0</b>	<b>\$27,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$2,000,000</b>	<b>\$500,000</b>	<b>\$24,500,000</b>	<b>\$0</b>	<b>\$27,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210009** (Ver 4) 23-00 **LOCAL**

**Name:** 3rd Street Bicycle Facility  
**Scope:** Construct bicycle facilities between Vine Street and Evans Street.

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Vine Street to Evans Street of Distance (mile) .85

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$750,000	\$0	\$0	\$0	\$750,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$6,750,000	\$0	\$6,750,000
<b>2023-2027 TOTAL</b>		<b>\$750,000</b>	<b>\$0</b>	<b>\$6,750,000</b>	<b>\$0</b>	<b>\$7,500,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$750,000</b>	<b>\$0</b>	<b>\$6,750,000</b>	<b>\$0</b>	<b>\$7,500,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210010** (Ver 4) 23-00 **LOCAL**

**Name:** 5th Street Multimodal Improvements  
**Scope:** Multimodal improvements from Keystone Ave to Evans Ave.

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Keystone Ave to Evans Ave of Distance (mile) 1

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$800,000	\$0	\$0	\$0	\$800,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$7,200,000	\$0	\$7,200,000
<b>2023-2027 TOTAL</b>		<b>\$800,000</b>	<b>\$0</b>	<b>\$7,200,000</b>	<b>\$0</b>	<b>\$8,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$800,000</b>	<b>\$0</b>	<b>\$7,200,000</b>	<b>\$0</b>	<b>\$8,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210012** (Ver 3) 23-00 **LOCAL**

**Name:** E 6th Street Bicycle Facility and Safety Improvements  
**Scope:** Construct bicycle facilities and safety improvements from Virginia Street to 4th Street

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Virginia Street to 4th Street of Distance (mile) 1.2

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$600,000	\$0	\$0	\$0	\$600,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$5,400,000	\$0	\$5,400,000
<b>2023-2027 TOTAL</b>		<b>\$600,000</b>	<b>\$0</b>	<b>\$5,400,000</b>	<b>\$0</b>	<b>\$6,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$600,000</b>	<b>\$0</b>	<b>\$5,400,000</b>	<b>\$0</b>	<b>\$6,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**



**WA20210013** (Ver 3) 23-00 **LOCAL**

**Name:** Moana Lane Multimodal and ADA Improvements  
**Scope:** Construct multimodal and ADA improvements from Skyline Blvd to Plumas Street  
**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2  
**County:** Washoe **Limits:** From Skyline Blvd to Plumas Street of Distance (mile) .85

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$560,000	\$0	\$0	\$0	\$560,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$5,040,000	\$0	\$5,040,000
<b>2023-2027 TOTAL</b>		<b>\$560,000</b>	<b>\$0</b>	<b>\$5,040,000</b>	<b>\$0</b>	<b>\$5,600,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$560,000</b>	<b>\$0</b>	<b>\$5,040,000</b>	<b>\$0</b>	<b>\$5,600,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210014** (Ver 3) 23-00 **FEDERAL**

**Name:** Virginia Line BRT Improvements Project  
**Scope:** Improve Virginia Line BRT operations and construct multimodal and ADA improvements from Plumb Lane to Meadowood Mall Circle  
**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities. **TCM:** No **NDOT:** District 2  
**County:** Washoe **Limits:** From Plumb Lane to Meadowood Mall Circle of Distance (mile) 2.54

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	FTA 5309 Small Starts	\$0	\$0	\$11,655,000	\$0	\$11,655,000
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$11,655,000	\$0	\$11,655,000
<2023	Prior	\$2,590,000	\$0	\$0	\$0	\$2,590,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$23,310,000</b>	<b>\$0</b>	<b>\$23,310,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$2,590,000</b>	<b>\$0</b>	<b>\$23,310,000</b>	<b>\$0</b>	<b>\$25,900,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

WA20220035 (Ver 2) 23-00							FEDERAL
<b>Name:</b> West 4th St Improvements - West McCarran Blvd to Vine St							
<b>Scope:</b> Construct enhanced sidewalks, bus/bike lanes, and intersection improvements.							
<b>Project Type:</b> Active Transportation (Bi			<b>AQ:</b> Exempt, Air Quality - Bicycle and pedestrian facilities.			<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> From W. McCarran Blvd to Vine St of Distance (mile) 2.15 mi					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	Local Fuel Tax - RTCWA	\$3,500,000	\$0	\$0	\$0	\$3,500,000	
2025	HSIP	\$0	\$0	\$13,828,000	\$0	\$13,828,000	
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$10,107,000	\$0	\$10,107,000	
<b>2023-2027 TOTAL</b>		<b>\$3,500,000</b>	<b>\$0</b>	<b>\$23,935,000</b>	<b>\$0</b>	<b>\$27,435,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$3,500,000</b>	<b>\$0</b>	<b>\$23,935,000</b>	<b>\$0</b>	<b>\$27,435,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>RTC Washoe</b>				

XS20220010 (Ver 3) 23-00							FEDERAL
<b>Name:</b> North Virginia Street Multimodal Project- Design							
<b>Scope:</b> Multimodal improvements on N. Virginia Street from McCarran Boulevard to US 395. The design portion of the project.							
<b>Project Type:</b> Active Transportation (Bi			<b>AQ:</b> Exempt, Air Quality - Bicycle and pedestrian facilities.			<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> From McCarran to US 395 of Distance (mile) 2.43 Milepost begins at 27.57 ends at 30					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	Local Fuel Tax - RTCWA	\$75,000	\$0	\$0	\$0	\$75,000	
2024	STBG WA	\$1,425,000	\$0	\$0	\$0	\$1,425,000	
<b>2023-2027 TOTAL</b>		<b>\$1,500,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,500,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$1,500,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,500,000</b>	
MPO <b>RTC Washoe</b>			Lead Agency <b>RTC Washoe</b>				

**WA20170122** (Ver 10) 23-00 **FEDERAL**

**Name:** Arlington Avenue Bridges Replacement

**Scope:** Replace the Arlington Avenue Bridges at the Truckee River

**Project Type:** Bridge/Structures

**AQ:** Exempt, Safety - Non capacity widening or bridge reconstruction.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Bridge #: B1531, B1532

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$0	\$300,000	\$0	\$0	\$300,000
2024	Congressionally Directed Spending	\$0	\$0	\$2,000,000	\$0	\$2,000,000
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$9,800,000	\$0	\$9,800,000
2024	RAISE Grant	\$0	\$0	\$7,000,000	\$0	\$7,000,000
2024	STBG WA	\$0	\$0	\$5,900,000	\$0	\$5,900,000
<2023	Prior	\$3,300,944	\$0	\$0	\$0	\$3,300,944
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$300,000</b>	<b>\$24,700,000</b>	<b>\$0</b>	<b>\$25,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$3,300,944</b>	<b>\$300,000</b>	<b>\$24,700,000</b>	<b>\$0</b>	<b>\$28,300,944</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20190044** (Ver 8) 23-00 **FEDERAL**

**Name:** Keystone Avenue Improvements - Package 1

**Scope:** Multimodal improvements and Truckee River bridge replacement.

**Project Type:** Bridge/Structures

**AQ:** Exempt, Air Quality - Bicycle and pedestrian facilities.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From California Avenue to 1st Street of Distance (mile) 0.5

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	BRIDGE FORMULA PGM OFF-SYS	\$5,000,000	\$0	\$0	\$0	\$5,000,000
<b>2023-2027 TOTAL</b>		<b>\$5,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$5,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

<b>WA20220025</b> (Ver 4) 23-00							<b>FEDERAL</b>
<b>Name:</b> Sierra St Bridge Replacement							
<b>Scope:</b> Design, ROW, and construction for the Sierra St Bridge Replacement project.							
<b>Project Type:</b> Bridge/Structures		<b>AQ:</b> Exempt, Safety - Non capacity widening or bridge reconstruction.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> Bridge #: 1					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	BRIDGE FORMULA PGM OFF-SYS	\$3,500,000	\$0	\$0	\$0	\$3,500,000	
2026	BRIDGE FORMULA PGM OFF-SYS	\$0	\$0	\$12,500,000	\$0	\$12,500,000	
2026	Local Fuel Tax - RTCWA	\$0	\$100,000	\$6,700,000	\$0	\$6,800,000	
2026	STBG Bridge	\$0	\$0	\$10,800,000	\$0	\$10,800,000	
<b>2023-2027 TOTAL</b>		<b>\$3,500,000</b>	<b>\$100,000</b>	<b>\$30,000,000</b>	<b>\$0</b>	<b>\$33,600,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$3,500,000</b>	<b>\$100,000</b>	<b>\$30,000,000</b>	<b>\$0</b>	<b>\$33,600,000</b>	
MPO <b>RTC Washoe</b>				Lead Agency <b>RTC Washoe</b>			

<b>WA20220026</b> (Ver 4) 23-00							<b>LOCAL</b>
<b>Name:</b> Keystone Avenue Improvements - Package 2							
<b>Scope:</b> Multimodal improvements and Truckee River bridge replacement.							
<b>Project Type:</b> Bridge/Structures		<b>AQ:</b> Exempt, Air Quality - Bicycle and pedestrian facilities.				<b>TCM:</b> No <b>NDOT:</b> District 2	
<b>County:</b> Washoe		<b>Limits:</b> From California Avenue to 1st Street of Distance (mile) 0.5					
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$30,000,000	\$0	\$30,000,000	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$30,000,000</b>	<b>\$0</b>	<b>\$30,000,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$30,000,000</b>	<b>\$0</b>	<b>\$30,000,000</b>	
MPO <b>RTC Washoe</b>				Lead Agency <b>RTC Washoe</b>			

**WA20190038** (Ver 5) 23-00 **LOCAL**

**Name:** Mill Street Capacity & Safety Project

**Scope:** Multimodal, operational, safety, and capacity improvements.

**Project Type:** Capacity

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Kietzke Lane to Terminal Way of Distance (mile) 1.5

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$0	\$15,000,000	\$0	\$0	\$15,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$20,000,000	\$0	\$20,000,000
<2023	Prior	\$5,000,000	\$0	\$0	\$0	\$5,000,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$15,000,000</b>	<b>\$20,000,000</b>	<b>\$0</b>	<b>\$35,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$5,000,000</b>	<b>\$15,000,000</b>	<b>\$20,000,000</b>	<b>\$0</b>	<b>\$40,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20190041** (Ver 7) 23-00 **FEDERAL**

**Name:** Sparks Boulevard Corridor - Phase 2

**Scope:** Widen roadway from 4 to 6 lanes and construct multimodal improvements.

**Project Type:** Capacity

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Lincoln Way to Baring Boulevard of Distance (mile) 1.35

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$0	\$3,000,000	\$0	\$0	\$3,000,000
2025	STBG WA	\$0	\$0	\$57,800,000	\$0	\$57,800,000
<2023	Prior	\$6,000,000	\$0	\$0	\$0	\$6,000,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$3,000,000</b>	<b>\$57,800,000</b>	<b>\$0</b>	<b>\$60,800,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$6,000,000</b>	<b>\$3,000,000</b>	<b>\$57,800,000</b>	<b>\$0</b>	<b>\$66,800,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210005** (Ver 3) 23-00 **LOCAL**

**Name:** North Virginia Street Widening

**Scope:** Panther Drive to Stead Blvd

**Project Type:** Capacity

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Panther Drive to Stead Blvd of Distance (mile) 3.75

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$5,000,000	\$0	\$0	\$0	\$5,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$50,000,000	\$0	\$50,000,000
<b>2023-2027 TOTAL</b>		<b>\$5,000,000</b>	<b>\$0</b>	<b>\$50,000,000</b>	<b>\$0</b>	<b>\$55,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$5,000,000</b>	<b>\$0</b>	<b>\$50,000,000</b>	<b>\$0</b>	<b>\$55,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210006** (Ver 4) 23-00 **LOCAL**

**Name:** Pembroke Drive Safety Project

**Scope:** Widen roadway with two-way left turn lane from McCarran Blvd to Veterans Pkwy; includes associated safety improvements.

**Project Type:** Capacity

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From McCarran Blvd to Veterans Pkwy of Distance (mile) 1

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$2,000,000	\$0	\$0	\$0	\$2,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$17,000,000	\$0	\$17,000,000
<b>2023-2027 TOTAL</b>		<b>\$2,000,000</b>	<b>\$0</b>	<b>\$17,000,000</b>	<b>\$0</b>	<b>\$19,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$2,000,000</b>	<b>\$0</b>	<b>\$17,000,000</b>	<b>\$0</b>	<b>\$19,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210007** (Ver 3) 23-00 **LOCAL**

**Name:** South Virginia Street & I-580 Exit 29 Capacity & Safety Project  
**Scope:** Addition of northbound lane between I-580 off ramp and Longley Lane with associated safety improvements.

**Project Type:** Capacity **AQ:** Non-Exempt **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From I-580 off ramp to Longley Lane of Distance (mile) .35

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$1,288,024	\$50,000	\$0	\$0	\$1,338,024
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$7,707,000	\$0	\$7,707,000
<b>2023-2027 TOTAL</b>		<b>\$1,288,024</b>	<b>\$50,000</b>	<b>\$7,707,000</b>	<b>\$0</b>	<b>\$9,045,024</b>
<b>ALL YEARS TOTAL</b>		<b>\$1,288,024</b>	<b>\$50,000</b>	<b>\$7,707,000</b>	<b>\$0</b>	<b>\$9,045,024</b>

MPO **RTC Washoe** Lead Agency **RTC Washoe**

**WA20210011** (Ver 4) 23-00 **LOCAL**

**Name:** Steamboat Pkwy and Damonte Ranch Pkwy Capacity Project  
**Scope:** Widen Steamboat Pkwy and Damonte Ranch Pkwy at various roadway sections from Veterans Pkwy to Promenade Way.

**Project Type:** Capacity **AQ:** Non-Exempt **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Veterans Pkwy to Damonte Ranch Pkwy of Distance (mile) .45

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	RRIF (Regional Road Impact Fees) - RTCWA	\$0	\$0	\$6,500,000	\$0	\$6,500,000
<2023	Prior	\$400,000	\$0	\$0	\$0	\$400,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$6,500,000</b>	<b>\$0</b>	<b>\$6,500,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$400,000</b>	<b>\$0</b>	<b>\$6,500,000</b>	<b>\$0</b>	<b>\$6,900,000</b>

MPO **RTC Washoe** Lead Agency **RTC Washoe**

**WA20210021** (Ver 4) 23-00 **LOCAL**

**Name:** Pyramid Way Lane Addition - Design

**Scope:** Add southbound lane from Ingenuity Avenue to Egyptian Drive.

**Project Type:** Capacity

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Ingenuity Avenue to Egyptian Drive of Distance (mile) 1.82 Milepost begins at 8.85 ends at 10.67

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$1,200,000	\$0	\$0	\$0	\$1,200,000
2025	Local Fuel Tax - RTCWA	\$0	\$300,000	\$10,000,000	\$0	\$10,300,000
<b>2023-2027 TOTAL</b>		<b>\$1,200,000</b>	<b>\$300,000</b>	<b>\$10,000,000</b>	<b>\$0</b>	<b>\$11,500,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$1,200,000</b>	<b>\$300,000</b>	<b>\$10,000,000</b>	<b>\$0</b>	<b>\$11,500,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20230006** (Ver 1) 23-00 **LOCAL**

**Name:** Buck Drive Circulation Project

**Scope:** Widening of Buck Drive between Lemmon Drive and North Hills Boulevard

**Project Type:** Capacity

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Lemmon Dr to North Hills Blvd of Distance (mile) .18 mi

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$511,164	\$0	\$0	\$0	\$511,164
2025	Local Fuel Tax - RTCWA	\$0	\$488,836	\$0	\$0	\$488,836
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$3,000,000	\$0	\$3,000,000
<b>2023-2027 TOTAL</b>		<b>\$511,164</b>	<b>\$488,836</b>	<b>\$3,000,000</b>	<b>\$0</b>	<b>\$4,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$511,164</b>	<b>\$488,836</b>	<b>\$3,000,000</b>	<b>\$0</b>	<b>\$4,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**



**WA20230007** (Ver 1) 23-00 **LOCAL**

**Name:** Military Road Capacity & Safety Project

**Scope:** Widen roadway from Lemmon Dr to Echo Ave; includes associated safety improvements.

**Project Type:** Capacity

**AQ:** Non-Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Lemmon Dr to Echo Ave of Distance (mile) 2.45

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$5,000,000	\$0	\$0	\$0	\$5,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$5,000,000	\$0	\$0	\$5,000,000
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$25,412,000	\$0	\$25,412,000
<b>2023-2027 TOTAL</b>		<b>\$5,000,000</b>	<b>\$5,000,000</b>	<b>\$25,412,000</b>	<b>\$0</b>	<b>\$35,412,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$5,000,000</b>	<b>\$5,000,000</b>	<b>\$25,412,000</b>	<b>\$0</b>	<b>\$35,412,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20110215** (Ver 13) 23-00 **FEDERAL**

**Name:** Traffic Management Program - Annual Traffic Signal, ITS Operations, & Intersection Improvements

**Scope:** Ongoing cycle of retiming of 1/3 of the regional traffic signals - approximately 150 per year, traffic engineering spot/intersection improvements, Intelligent Transportation Systems (ITS) operations.

**Project Type:** ITS

**AQ:** Exempt, Other - Traffic signal synchronization projects.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$10,000,000	\$10,000,000
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$10,000,000	\$10,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$10,000,000	\$10,000,000
<2023	Prior	\$0	\$0	\$0	\$22,500,000	\$22,500,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$30,000,000</b>	<b>\$30,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$52,500,000</b>	<b>\$52,500,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210004** (Ver 3) 23-00 **LOCAL**

**Name:** McCarran Blvd Intersection and Operations  
**Scope:** Intersection and operations improvements from Kietzke Lane to Greensboro Drive.

**Project Type:** Interchange/Intersection **AQ:** Exempt, All Projects - Intersection channelization projects. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Kietzke Lane to Greensboro Drive of Distance (mile) 0.8 Milepost begins at 0 ends at .8

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$10,000,000	\$0	\$10,000,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$10,000,000</b>	<b>\$0</b>	<b>\$10,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$10,000,000</b>	<b>\$0</b>	<b>\$10,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20220011** (Ver 4) 23-00 **FEDERAL**

**Name:** Pyramid Hwy/Sparks Blvd Interchange and Connector Improvements - Design  
**Scope:** Improvements to the Pyramid Way and Sparks Blvd/Highland Ranch intersection, including converting existing intersection to a new grade-separated interchange. Preliminary design of the Pyramid/US 395 Connector (new road) between Pyramid Way and US 395.

**Project Type:** Interchange/Intersection **AQ:** Exempt, All Projects - Interchange reconfiguration projects. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Kiley Pkwy to Wingfield Hills Rd of Distance (mile) 1 Milepost begins at 5.05 ends at 6.05

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$500,000	\$0	\$0	\$0	\$500,000
2023	STBG WA	\$4,500,000	\$0	\$0	\$0	\$4,500,000
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$0	\$0
2027	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$0	\$0
<b>2023-2027 TOTAL</b>		<b>\$5,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$5,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20230002** (Ver 1) 23-00 **LOCAL**

**Name:** Pyramid Hwy/Sparks Blvd Interchange and Connector Improvements - Construction  
**Scope:** Improvements to the Pyramid Way and Sparks Blvd/Highland Ranch intersection, including converting existing intersection to a new grade-separated interchange. Right-of-way and construction phases of project.

**Project Type:** Interchange/Intersection **AQ:** Exempt, All Projects - Interchange reconfiguration projects. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Kiley Pkwy to Wingfield Hills Rd of Distance (mile) 1 Milepost begins at 5.05 ends at 6.05

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2026	Local Fuel Tax - RTCWA	\$0	\$2,000,000	\$0	\$0	\$2,000,000
2027	Local Fuel Tax - RTCWA	\$0	\$0	\$30,000,000	\$0	\$30,000,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$2,000,000</b>	<b>\$30,000,000</b>	<b>\$0</b>	<b>\$32,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$2,000,000</b>	<b>\$30,000,000</b>	<b>\$0</b>	<b>\$32,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210003** (Ver 3) 23-00 **FEDERAL**

**Name:** Geiger Grade Realignment  
**Scope:** Construct new roadway alignment on Geiger Grade from Virginia Street to Toll Road

**Project Type:** Major/New Construction **AQ:** Non-Exempt **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Virginia Street to Toll Road of Distance (mile) 0.4 Milepost begins at 0 ends at .4

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$12,000,000	\$0	\$0	\$0	\$12,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$12,000,000	\$0	\$0	\$12,000,000
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$59,000,000	\$0	\$59,000,000
2026	STBG WA	\$0	\$0	\$1,000,000	\$0	\$1,000,000
<b>2023-2027 TOTAL</b>		<b>\$12,000,000</b>	<b>\$12,000,000</b>	<b>\$60,000,000</b>	<b>\$0</b>	<b>\$84,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$12,000,000</b>	<b>\$12,000,000</b>	<b>\$60,000,000</b>	<b>\$0</b>	<b>\$84,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20220027** (Ver 2) 23-00

**STATE**

**Name:** Mt. Rose Corridor Study Recommendations Phase 1 Improvements

**Scope:** Mt. Rose Corridor Study Recommendations Phase 1 Improvements

**Project Type:** Major/New Construction

**AQ:** Exempt, Safety - Pavement resurfacing and/or rehabilitation.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Douglas Fir Dr to Veterans Pkwy of Distance (mile) 7.5 Milepost begins at 17.2 ends at 24.7

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$3,000,000	\$0	\$0	\$0	\$3,000,000
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$20,000,000	\$0	\$20,000,000
2026	State Gas Tax	\$0	\$0	\$13,000,000	\$0	\$13,000,000
<b>2023-2027 TOTAL</b>		<b>\$3,000,000</b>	<b>\$0</b>	<b>\$33,000,000</b>	<b>\$0</b>	<b>\$36,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$3,000,000</b>	<b>\$0</b>	<b>\$33,000,000</b>	<b>\$0</b>	<b>\$36,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20140044** (Ver 12) 23-00

**FEDERAL**

**Name:** Safe Routes to School

**Scope:** County wide safe routes to school program

**Project Type:** Other Type

**AQ:** Exempt, Other - Transportation enhancement activities.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fund	\$0	\$0	\$0	\$12,800	\$12,800
2023	STBG WA	\$0	\$0	\$0	\$243,200	\$243,200
2024	Local Fund	\$0	\$0	\$0	\$12,800	\$12,800
2024	STBG WA	\$0	\$0	\$0	\$243,200	\$243,200
2025	Local Fund	\$0	\$0	\$0	\$12,800	\$12,800
2025	STBG WA	\$0	\$0	\$0	\$243,200	\$243,200
2026	Local Fund	\$0	\$0	\$0	\$12,800	\$12,800
2026	STBG WA	\$0	\$0	\$0	\$243,200	\$243,200
2027	Local Fund	\$0	\$0	\$0	\$12,800	\$12,800
2027	STBG WA	\$0	\$0	\$0	\$243,200	\$243,200
<2023	Prior	\$0	\$0	\$0	\$1,267,000	\$1,267,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,280,000</b>	<b>\$1,280,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,547,000</b>	<b>\$2,547,000</b>

**WA20150011** (Ver 9) 23-00 **LOCAL**

**Name:** Pavement Preservation Program  
**Scope:** Annual Pavement Preservation Program - Roadway Reconstruction and Preventive Maintenance  
**Project Type:** Preservation **AQ:** Exempt, Safety - Pavement resurfacing and/or rehabilitation. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Various Locations

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Local Fuel Tax - RTCWA	\$0	\$0	\$22,500,000	\$0	\$22,500,000
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$22,500,000	\$0	\$22,500,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$22,500,000	\$0	\$22,500,000
2026	Local Fuel Tax - RTCWA	\$0	\$0	\$22,500,000	\$0	\$22,500,000
2027	Local Fuel Tax - RTCWA	\$0	\$0	\$22,500,000	\$0	\$22,500,000
<2023	Prior	\$0	\$0	\$61,600,000	\$0	\$61,600,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$112,500,000</b>	<b>\$0</b>	<b>\$112,500,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$174,100,000</b>	<b>\$0</b>	<b>\$174,100,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20200070** (Ver 7) 23-00 **FEDERAL**

**Name:** Lemmon Drive Segment 2 Traffic Improvements and Resiliency Project  
**Scope:** Project will improve traffic operations and add resiliency through stormwater and other improvements.  
**Project Type:** Road Improvement **AQ:** Non-Exempt **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** From Fleetwood Drive to Ramsey Way of Distance (mile) 3.1

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	Congressionally Directed Spending	\$4,000,000	\$0	\$0	\$0	\$4,000,000
2023	Local Fuel Tax - RTCWA	\$210,526	\$0	\$0	\$0	\$210,526
2023	RAISE Grant	\$0	\$0	\$25,000,000	\$0	\$25,000,000
2024	Local Fuel Tax - RTCWA	\$0	\$2,000,000	\$0	\$0	\$2,000,000
2025	Local Fuel Tax - RTCWA	\$0	\$0	\$18,200,000	\$0	\$18,200,000
<b>2023-2027 TOTAL</b>		<b>\$4,210,526</b>	<b>\$2,000,000</b>	<b>\$43,200,000</b>	<b>\$0</b>	<b>\$49,410,526</b>
<b>ALL YEARS TOTAL</b>		<b>\$4,210,526</b>	<b>\$2,000,000</b>	<b>\$43,200,000</b>	<b>\$0</b>	<b>\$49,410,526</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA2012101** (Ver 15) 23-00

**FEDERAL**

**Name:** Trip Reduction Program

**Scope:** Administrative Activities and Vanpool Program

**Project Type:** TDM

**AQ:** Exempt, Air Quality - Ride-sharing and van-pooling program.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	CMAQ - Washoe County	\$0	\$0	\$0	\$2,280,000	\$2,280,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$120,000	\$120,000
2024	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$2,280,000	\$2,280,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$120,000	\$120,000
2025	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$2,280,000	\$2,280,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$120,000	\$120,000
2026	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$2,280,000	\$2,280,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$120,000	\$120,000
2027	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$2,280,000	\$2,280,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$120,000	\$120,000
<2023	Prior	\$0	\$0	\$0	\$7,226,316	\$7,226,316
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$12,000,000</b>	<b>\$12,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$19,226,316</b>	<b>\$19,226,316</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20210016** (Ver 3) 23-00

**FEDERAL**

**Name:** RTC Park and Ride Program

**Scope:** On-going program to establish strategic park and ride lots throughout the region.

**Project Type:** TDM

**AQ:** Exempt, Air Quality - Ride-sharing and van-pooling program.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Various Locations

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$120,000	\$0	\$120,000
2023	RTC Sales Tax	\$0	\$0	\$30,000	\$0	\$30,000
2024	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$130,000	\$0	\$130,000
2024	RTC Sales Tax	\$0	\$0	\$32,500	\$0	\$32,500
2025	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$140,000	\$0	\$140,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$35,000	\$0	\$35,000
2026	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$140,000	\$0	\$140,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$35,000	\$0	\$35,000
2027	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$140,000	\$0	\$140,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$35,000	\$0	\$35,000
<2023	Prior	\$0	\$0	\$125,000	\$0	\$125,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$837,500</b>	<b>\$0</b>	<b>\$837,500</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$962,500</b>	<b>\$0</b>	<b>\$962,500</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**



**NV20110303** (Ver 10) 23-00

**FEDERAL**

**Name:** RIDE Capital - Shelters

**Scope:** Transit Enhancements/Shelters

**Project Type:** Transit - Capital & Rehat

**AQ:** Exempt, Mass Transit - Reconstruction or renovation of transit structures.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$50,000	\$50,000
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$50,000	\$50,000
2025	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$50,000	\$50,000
2026	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$50,000	\$50,000
2027	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$50,000	\$50,000
<2023	Prior	\$0	\$0	\$0	\$2,850,025	\$2,850,025
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,250,000</b>	<b>\$1,250,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$4,100,025</b>	<b>\$4,100,025</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**NV20110304** (Ver 10) 23-00

**FEDERAL**

**Name:** RIDE Capital - Support Vehicles/Equipment

**Scope:** Purchase Support Vehicles/Equipment (RTC Administration) - Annual Program

**Project Type:** Transit - Capital & Rehat

**AQ:** Exempt, Mass Transit - Purchase of support vehicles.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$60,000	\$60,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$15,000	\$15,000
2025	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$60,000	\$60,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$15,000	\$15,000
2026	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$60,000	\$60,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$15,000	\$15,000
2027	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$60,000	\$60,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$15,000	\$15,000
<2023	Prior	\$0	\$0	\$0	\$430,000	\$430,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$300,000</b>	<b>\$300,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$730,000</b>	<b>\$730,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**NV20110305** (Ver 10) 23-00

**FEDERAL**

**Name:** RIDE Capital - Equipment

**Scope:** Purchase Shop Equipment/Other Equipment - Annual Program

**Project Type:** Transit - Capital & Rehat

**AQ:** Exempt, Mass Transit - Purchase of office, shop and operating equipment for existing facilities  
**CM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2023	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$50,000	\$50,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$62,500	\$62,500
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2024	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$50,000	\$50,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$62,500	\$62,500
2025	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2025	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$50,000	\$50,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$62,500	\$62,500
2026	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$62,500	\$62,500
2027	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$200,000	\$200,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$62,500	\$62,500
<2023	Prior	\$0	\$0	\$0	\$2,007,500	\$2,007,500
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,462,500</b>	<b>\$1,462,500</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$3,470,000</b>	<b>\$3,470,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**NV20110307** (Ver 9) 23-00

**FEDERAL**

**Name:** RIDE Capital - Communication/Computer Equipment

**Scope:** Purchase Vehicle/Communication/Computer Equipment - Annual Program

**Project Type:** Transit - Capital & Rehat **AQ:** Exempt, Mass Transit - Purchase of office, shop and operating equipment for existing facilities **CM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$350,000	\$350,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$87,500	\$87,500
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$350,000	\$350,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$87,500	\$87,500
2025	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$350,000	\$350,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$87,500	\$87,500
2026	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$350,000	\$350,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$87,500	\$87,500
2027	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$350,000	\$350,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$87,500	\$87,500
<2023	Prior	\$0	\$0	\$0	\$2,051,000	\$2,051,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,187,500</b>	<b>\$2,187,500</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$4,238,500</b>	<b>\$4,238,500</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**NV20110309** (Ver 12) 23-00

**FEDERAL**

**Name:** RIDE Capital - Bldg. Renovations

**Scope:** Building Renovations and Upgrades - Annual Program

**Project Type:** Transit - Capital & Rehat

**AQ:** Exempt, Mass Transit - Reconstruction or renovation of transit structures.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5307 Lrg Urb Capital	\$0	\$0	\$90,000	\$0	\$90,000
2023	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$90,000	\$0	\$90,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$45,000	\$0	\$45,000
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$90,000	\$0	\$90,000
2024	FTA 5337 Good Repair	\$0	\$0	\$75,000	\$0	\$75,000
2024	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$90,000	\$0	\$90,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$63,750	\$0	\$63,750
2025	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$90,000	\$90,000
2025	FTA 5337 Good Repair	\$0	\$0	\$0	\$75,000	\$75,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$63,750	\$63,750
2026	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$90,000	\$90,000
2026	FTA 5337 Good Repair	\$0	\$0	\$0	\$75,000	\$75,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$63,750	\$63,750
2027	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$90,000	\$90,000
2027	FTA 5337 Good Repair	\$0	\$0	\$0	\$75,000	\$75,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$63,750	\$63,750
<2023	Prior	\$0	\$0	\$10,173,059	\$0	\$10,173,059
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$543,750</b>	<b>\$686,250</b>	<b>\$1,230,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$10,716,809</b>	<b>\$686,250</b>	<b>\$11,403,059</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**NV20110312** (Ver 10) 23-00

**FEDERAL**

**Name:** ACCESS Capital - Sutro

**Scope:** Facilities (Operations & Maintenance)/Equipment - ACCESS Buildings at Sutro Street

**Project Type:** Transit - Capital & Rehat

**AQ:** Exempt, Mass Transit - Reconstruction or renovation of transit structures.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Nearest Crossstreet: 600 Sutro Street

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$64,000	\$64,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$16,000	\$16,000
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$64,000	\$64,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$16,000	\$16,000
2025	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$64,000	\$64,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$16,000	\$16,000
2026	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$64,000	\$64,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$16,000	\$16,000
2027	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$64,000	\$64,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$16,000	\$16,000
<2023	Prior	\$0	\$0	\$0	\$590,000	\$590,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$400,000</b>	<b>\$400,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$990,000</b>	<b>\$990,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20130078** (Ver 15) 23-00

**FEDERAL**

**Name:** RIDE Replacement Vehicles

**Scope:** Purchase RIDE replacement vehicles. Ongoing vehicle replacement schedule. FHWA funds to be transferred to FTA.

**Project Type:** Transit - Capital & Rehat **AQ:** Exempt, Mass Transit - Purchase new buses and rail cars to replace existing vehicles or **rTCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2025	CMAQ - Washoe County	\$0	\$0	\$0	\$4,500,000	\$4,500,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$236,842	\$236,842
2026	CMAQ - Washoe County	\$0	\$0	\$0	\$4,500,000	\$4,500,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$236,842	\$236,842
2027	CMAQ - Washoe County	\$0	\$0	\$0	\$4,500,000	\$4,500,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$236,842	\$236,842
<2023	Prior	\$0	\$0	\$0	\$47,620,411	\$47,620,411
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$14,210,526</b>	<b>\$14,210,526</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$61,830,937</b>	<b>\$61,830,937</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

WA20140046 (Ver 13) 23-00							FEDERAL
<b>Name:</b> ACCESS Replacement Vehicles							
<b>Scope:</b> Purchase ACCESS Replacement Vehicles - Ongoing Vehicle Replacement Schedule							
<b>Project Type:</b> Transit - Capital & Rehat <span style="float: right;"><b>AQ:</b> Exempt, Mass Transit - Purchase new buses and rail cars to replace existing vehicles or <b>rTCM:</b> Yes <b>NDOT:</b> District 2</span>							
<b>County:</b> Washoe <span style="float: right;"><b>Limits:</b> Not Location Specific</span>							
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2023	CMAQ - Washoe County	\$0	\$0	\$0	\$2,100,000	\$2,100,000	
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$110,526	\$110,526	
2026	CMAQ - Washoe County	\$0	\$0	\$0	\$2,100,000	\$2,100,000	
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$110,526	\$110,526	
2027	CMAQ - Washoe County	\$0	\$0	\$0	\$2,100,000	\$2,100,000	
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$110,526	\$110,526	
<2023	Prior	\$0	\$0	\$0	\$7,426,000	\$7,426,000	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$6,631,578</b>	<b>\$6,631,578</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$14,057,578</b>	<b>\$14,057,578</b>	
<b>MPO</b> RTC Washoe				<b>Lead Agency</b> RTC Washoe			

WA20210015 (Ver 3) 23-00							FEDERAL
<b>Name:</b> RTC 4TH STREET STATION Expansion							
<b>Scope:</b> Expand footprint and structure of RTC 4TH STREET STATION							
<b>Project Type:</b> Transit - Capital & Rehat <span style="float: right;"><b>AQ:</b> Exempt, Mass Transit - Reconstruction or renovation of transit structures. <b>TCM:</b> No <b>NDOT:</b> District 2</span>							
<b>County:</b> Washoe <span style="float: right;"><b>Limits:</b> Nearest Crossstreet:</span>							
FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL	
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$5,619,999	\$0	\$5,619,999	
2024	Local Sales Tax - RTCWA	\$0	\$0	\$1,405,001	\$0	\$1,405,001	
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$7,025,000</b>	<b>\$0</b>	<b>\$7,025,000</b>	
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$7,025,000</b>	<b>\$0</b>	<b>\$7,025,000</b>	
<b>MPO</b> RTC Washoe				<b>Lead Agency</b> RTC Washoe			



**WA20210017** (Ver 3) 23-00 **FEDERAL**

**Name:** FlexRIDE Replacement Vehicles

**Scope:** Vehicle replacement program for the RTC FlexRIDE (microtransit) service.

**Project Type:** Transit - Capital & Rehat **AQ:** Exempt, Mass Transit - Purchase new buses and rail cars to replace existing vehicles or **rTCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$494,000	\$494,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$26,000	\$26,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$520,000</b>	<b>\$520,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$520,000</b>	<b>\$520,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20230003** (Ver 1) 23-00 **FEDERAL**

**Name:** Hydrogen Fuel Cell Buses

**Scope:** Purchase of hydrogen fuel cell electric buses to replace aging diesel buses; purchase through award of discretionary funds (5339(c) Low or No Vehicle Emission Program).

**Project Type:** Transit - Capital & Rehat **AQ:** Exempt, Mass Transit - Purchase new buses and rail cars to replace existing vehicles or **rTCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$7,029,884	\$7,029,884
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$1,240,568	\$1,240,568
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$8,270,452</b>	<b>\$8,270,452</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$8,270,452</b>	<b>\$8,270,452</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20230004** (Ver 1) 23-00 **FEDERAL**

**Name:** Hydrogen Fuel Cell Bus Support Equipment/Facilities  
**Scope:** Purchase of hydrogen fuel tank and station upgrades; purchase through award of discretionary funds (5339(c) Low or No Vehicle Emission Program).

**Project Type:** Transit - Capital & Rehat **AQ:** Exempt, Mass Transit - Purchase new buses and rail cars to replace existing vehicles or rTCM: No **NDOT:** District 2

**County:** Washoe **Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$1,143,122	\$1,143,122
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$127,014	\$127,014
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,270,136</b>	<b>\$1,270,136</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,270,136</b>	<b>\$1,270,136</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20230005** (Ver 1) 23-00 **FEDERAL**

**Name:** Hydrogen Fuel Cell Other Capital Items  
**Scope:** Project management and training for hydrogen fuel cell bus and station acquisition/construction; funding through award of discretionary funds (5339(c) Low or No Vehicle Emission Program).

**Project Type:** Transit - Capital & Rehat **AQ:** Exempt, Mass Transit - Const of new bus or rail storage/maint facilities excluded in 23 CF TCM: No **NDOT:** District 2

**County:** Washoe **Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$611,600	\$611,600
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$152,900	\$152,900
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$764,500</b>	<b>\$764,500</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$764,500</b>	<b>\$764,500</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**XS20220031** (Ver 3) 23-00

**FEDERAL**

**Name:** Villanova Maintenance Facility Replacement - Phase 1

**Scope:** The maintenance and operations facility located on Villanova Drive will be relocated. ACCESS fleet operations facility and storage yard on Sutro will be expanded and redesigned to accommodate the combined RIDE and ACCESS fleets along with other improvements such as solar, electric bus chargers, workforce training site, expanded parking and maintenance bays, etc. Phase 1 will involve PE only.

**Project Type:** Transit - Capital & Rehat

**AQ:** Exempt, Mass Transit - Reconstruction or renovation of transit structures.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Nearest Crossstreet: 600 Sutro Street

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Congressionally Directed Spending	\$3,000,000	\$0	\$0	\$0	\$3,000,000
2024	FTA 5307 Lrg Urb Capital	\$2,600,000	\$0	\$0	\$0	\$2,600,000
2024	Local Sales Tax - RTCWA	\$1,400,000	\$0	\$0	\$0	\$1,400,000
<b>2023-2027 TOTAL</b>		<b>\$7,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$7,000,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$7,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$7,000,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**NV20110301** (Ver 10) 23-00

**FEDERAL**

**Name:** RTC RIDE and ACCESS - PM

**Scope:** Preventive Maintenance - Mechanical

**Project Type:** Transit - Maintenance

**AQ:** Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$3,500,000	\$3,500,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$875,000	\$875,000
2024	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$3,500,000	\$3,500,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$875,000	\$875,000
2025	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$3,500,000	\$3,500,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$875,000	\$875,000
2026	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$3,500,000	\$3,500,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$875,000	\$875,000
2027	FTA 5307 Lrg Urb Capital	\$0	\$0	\$0	\$3,500,000	\$3,500,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$875,000	\$875,000
<2023	Prior	\$0	\$0	\$0	\$28,750,000	\$28,750,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$21,875,000</b>	<b>\$21,875,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$50,625,000</b>	<b>\$50,625,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20150006** (Ver 9) 23-00

**FEDERAL**

**Name:** ACCESS Capital - Facilities

**Scope:** Annual Facilities (Operations and Maintenance) Program

**Project Type:** Transit - Maintenance

**AQ:** Exempt

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$50,000	\$50,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$12,500	\$12,500
2024	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$50,000	\$50,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$12,500	\$12,500
2025	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$50,000	\$50,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$12,500	\$12,500
2026	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$50,000	\$50,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$12,500	\$12,500
2027	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$0	\$50,000	\$50,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$12,500	\$12,500
<2023	Prior	\$0	\$0	\$0	\$325,000	\$325,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$312,500</b>	<b>\$312,500</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$637,500</b>	<b>\$637,500</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20200023** (Ver 6) 23-00

**FEDERAL**

**Name:** FlexRIDE Service

**Scope:** New FlexRIDE (microtransit) service in the South Meadows, Spanish Springs and the Verdi/Somerset areas.

**Project Type:** Transit - Operating

**AQ:** Exempt, Mass Transit - Transit operating assistance.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Various Locations

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	CMAQ - Washoe County	\$0	\$0	\$0	\$2,090,000	\$2,090,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$110,000	\$110,000
2026	CMAQ - Washoe County	\$0	\$0	\$0	\$2,090,000	\$2,090,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$110,000	\$110,000
<2023	Prior	\$0	\$0	\$0	\$2,105,264	\$2,105,264
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$4,400,000</b>	<b>\$4,400,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$6,505,264</b>	<b>\$6,505,264</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20200025** (Ver 6) 23-00

**FEDERAL**

**Name:** Virginia Street Bus RAPID Transit Extension Operation

**Scope:** Bus RAPID transit extension from the RTC 4th STREET STATION in downtown Reno to the University of Nevada, Reno; improves safety, traffic/bus operations, constructs 3 RAPID stations, adds bus only lane segment.

**Project Type:** Transit - Operating

**AQ:** Exempt, Mass Transit - Transit operating assistance.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** From Plumb Lane to 15th Street of Distance (mile) 2.9

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	CMAQ - Washoe County	\$0	\$0	\$0	\$850,000	\$850,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$44,737	\$44,737
2024	CMAQ - Washoe County	\$0	\$0	\$0	\$850,000	\$850,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$44,737	\$44,737
2025	CMAQ - Washoe County	\$0	\$0	\$0	\$850,000	\$850,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$44,737	\$44,737
2026	CMAQ - Washoe County	\$0	\$0	\$0	\$850,000	\$850,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$44,737	\$44,737
2027	CMAQ - Washoe County	\$0	\$0	\$0	\$850,000	\$850,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$44,737	\$44,737
<2023	Prior	\$0	\$0	\$0	\$1,105,263	\$1,105,263
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$4,473,685</b>	<b>\$4,473,685</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,578,948</b>	<b>\$5,578,948</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**NV20110311** (Ver 9) 23-00

**FEDERAL**

**Name:** ACCESS Operations - ADA

**Scope:** ADA-related operating expenses

**Project Type:** Transit - Other

**AQ:** Exempt, Mass Transit - Transit operating assistance.

**TCM:** Yes **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$700,000	\$700,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$175,000	\$175,000
2024	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$700,000	\$700,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$175,000	\$175,000
2025	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$700,000	\$700,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$175,000	\$175,000
2026	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$700,000	\$700,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$175,000	\$175,000
2027	FTA 5307 Lrg Urb Operating	\$0	\$0	\$0	\$700,000	\$700,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$175,000	\$175,000
<2023	Prior	\$0	\$0	\$0	\$5,700,600	\$5,700,600
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$4,375,000</b>	<b>\$4,375,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$10,075,600</b>	<b>\$10,075,600</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**



**WA20190002** (Ver 8) 23-00

**FEDERAL**

**Name:** U-Pass (Ed-Pass) Start-up Program

**Scope:** Seed money to establish a Universal Access Transit Pass (U-Pass) program with the University of Nevada, Reno (UNR) and Truckee Meadows Community College (TMCC) to provide unlimited access to RTC RIDE transit routes.

**Project Type:** Transit - Other **AQ:** Exempt, Mass Transit - Transit operating assistance.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	CMAQ - Washoe County	\$0	\$0	\$0	\$350,000	\$350,000
2023	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$18,421	\$18,421
2024	CMAQ - Washoe County	\$0	\$0	\$0	\$350,000	\$350,000
2024	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$18,420	\$18,420
2025	CMAQ - Washoe County	\$0	\$0	\$0	\$350,000	\$350,000
2025	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$18,421	\$18,421
2026	CMAQ - Washoe County	\$0	\$0	\$0	\$350,000	\$350,000
2026	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$18,421	\$18,421
2027	CMAQ - Washoe County	\$0	\$0	\$0	\$350,000	\$350,000
2027	Local Sales Tax - RTCWA	\$0	\$0	\$0	\$18,421	\$18,421
<2023	Prior	\$0	\$0	\$0	\$210,527	\$210,527
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,842,104</b>	<b>\$1,842,104</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,052,631</b>	<b>\$2,052,631</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20190027** (Ver 5) 23-00 **FEDERAL**

**Name:** Meadowood Mall Transfer Station Relocation

**Scope:** Relocate the RTC RIDE Meadowood Mall Transfer Station within the Meadowood Mall property and implement potential FTA joint development with affordable housing and possible retail.

**Project Type:** Transit - Other **AQ:** Exempt, Mass Transit - Reconstruction or renovation of transit structures. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Nearest Crossstreet:

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2023	FTA 5339 Bus/Fac Lrg Urb Capital	\$500,000	\$0	\$0	\$0	\$500,000
2023	Local Sales Tax - RTCWA	\$125,000	\$0	\$0	\$0	\$125,000
2024	FTA 5339 Bus/Fac Lrg Urb Capital	\$0	\$0	\$4,800,000	\$0	\$4,800,000
2024	Local Sales Tax - RTCWA	\$0	\$95,000	\$1,200,000	\$0	\$1,295,000
<b>2023-2027 TOTAL</b>		<b>\$625,000</b>	<b>\$95,000</b>	<b>\$6,000,000</b>	<b>\$0</b>	<b>\$6,720,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$625,000</b>	<b>\$95,000</b>	<b>\$6,000,000</b>	<b>\$0</b>	<b>\$6,720,000</b>

MPO **RTC Washoe**

Lead Agency **RTC Washoe**

**WA20200020** (Ver 5) 23-00 **LOCAL**

**Name:** Purchase Multiuse Path Maintenance Equipment

**Scope:** Purchase multiuse path maintenance equipment for each of the local jurisdictions (Washoe County and the Cities of Reno and Sparks) to remove debris and snow on multiuse/off-street paths throughout the region

**Project Type:** Active Transportation (Bi) **AQ:** Exempt, Other - Non construction related activities. **TCM:** No **NDOT:** District 2

**County:** Washoe **Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fuel Tax - RTCWA	\$0	\$0	\$0	\$190,000	\$190,000
2024	Local Fund	\$0	\$0	\$0	\$10,000	\$10,000
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$200,000</b>	<b>\$200,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$200,000</b>	<b>\$200,000</b>

MPO **RTC Washoe**

Lead Agency **Washoe County**

**WA20190035** (Ver 6) 23-00 **FEDERAL**

**Name:** Traffic Calming Improvements

**Scope:** Purchase speed radar signs and Rectangular Rapid Flashing Beacons (RRFBs).

**Project Type:** Signals & Lighting **AQ:** Exempt, Safety - Non signalization traffic control and operating.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Various Locations

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fund	\$350	\$0	\$0	\$1,440	\$1,790
2024	TAP WA STBG	\$6,650	\$0	\$0	\$27,350	\$34,000
<b>2023-2027 TOTAL</b>		<b>\$7,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$28,790</b>	<b>\$35,790</b>
<b>ALL YEARS TOTAL</b>		<b>\$7,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$28,790</b>	<b>\$35,790</b>

MPO **RTC Washoe**

Lead Agency **Washoe County**

**WA20220004** (Ver 2) 23-00 **FEDERAL**

**Name:** WCSD Safe Routes To School

**Scope:** SRTS will purchase solar powered radar signs and rapid flash beacons that will act as traffic calming devices.

**Project Type:** Safety **AQ:** Exempt, Safety - Safety Improvement Program.

**TCM:** No **NDOT:** District 2

**County:** Washoe

**Limits:** Not Location Specific

FED FY	Revenue Source	PE	ROW	CON	OTHER	TOTAL
2024	Local Fund	\$0	\$0	\$0	\$4,100	\$4,100
2024	TAP WA STBG	\$0	\$0	\$0	\$77,900	\$77,900
<b>2023-2027 TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$82,000</b>	<b>\$82,000</b>
<b>ALL YEARS TOTAL</b>		<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$82,000</b>	<b>\$82,000</b>

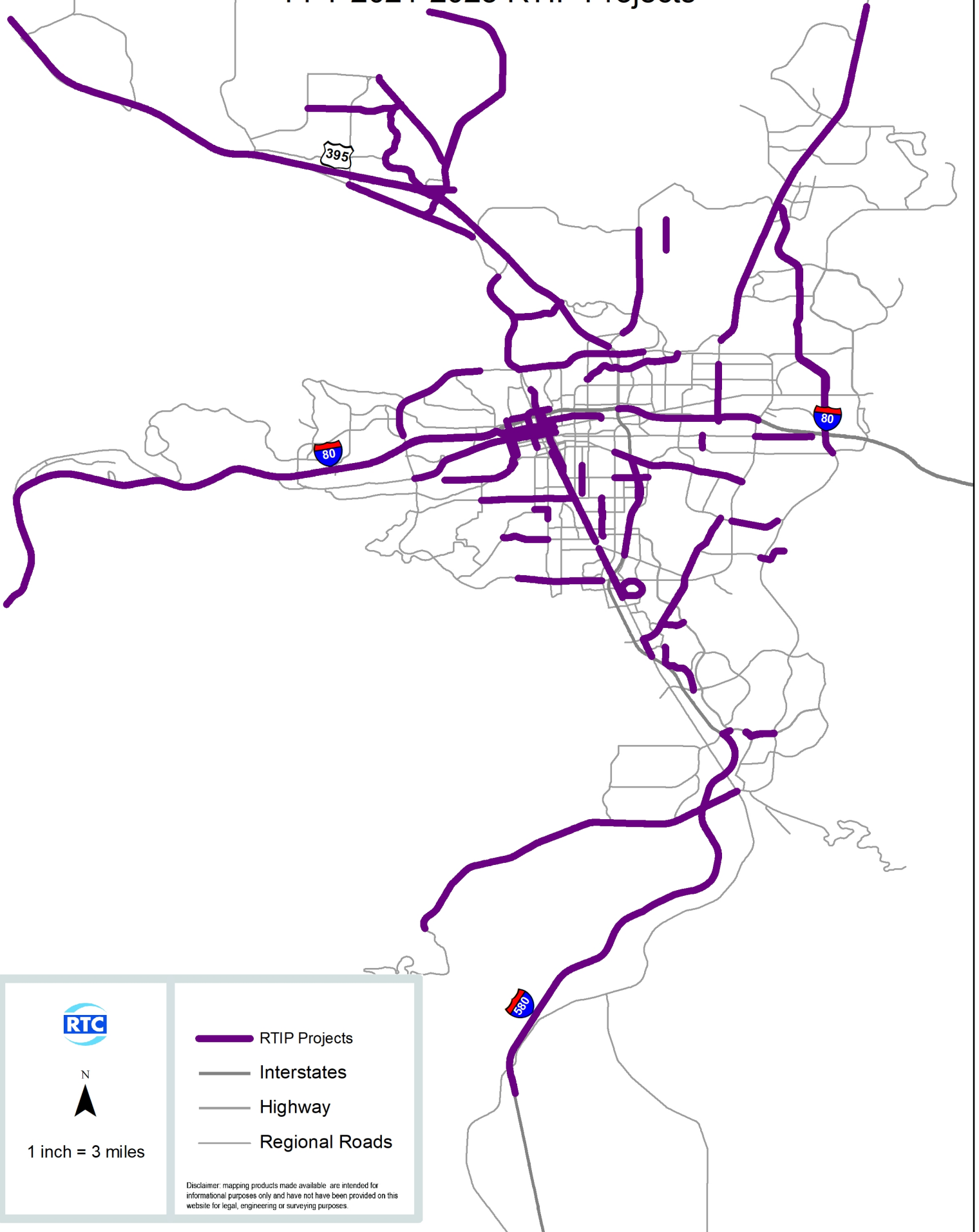
MPO **RTC Washoe**

Lead Agency **Washoe County School District**





## **Appendix B: FFY 2023-2027 Regional Transportation Improvement Program Project Location Map**

A project location map for the FFY 2023-2027 Regional Transportation Improvement Program is provided on the following page. It should be noted that many of the projects in the RTIP are not able to be mapped, such as the purchase of vehicles or equipment and the administration of programs or operation of services. However, detailed information, including a map for location-based projects, can be found online for any of the projects through the electronic Statewide Transportation Improvement Program (eSTIP) at <https://estip.nevadadot.com>.

# FFY 2021-2025 RTIP Projects



1 inch = 3 miles

-  RTIP Projects
-  Interstates
-  Highway
-  Regional Roads

Disclaimer: mapping products made available are intended for informational purposes only and have not been provided on this website for legal, engineering or surveying purposes.

## Appendix C: RTIP Amendments/Administrative Modifications

The Regional Transportation Improvement Program (RTIP) is required to be updated at least every four years. There are changes that occur during the course of the document such as adjustments in project schedules, funding amounts/sources or project descriptions. These changes require either an amendment or an administrative modification. Any changes in the RTIP must be included in the RTP.

### Amendments

Amendments are major or substantive changes to the RTIP and must follow the requirements contained in the RTC's Public Participation Plan and the Washoe County Transportation Conformity Plan (for air quality analysis if necessary). Amendments are applicable when:

- There are substantial changes to the scope of an existing project that would result in an air quality conformity determination
- A federally-funded project is added or deleted
- There is a change in a funding source for a project from non-federal to federal
- There is an increase of more than 25% of the federally-funded portion of a project

### Administrative Modifications

Administrative Modifications are non-substantive changes to the RTIP and include the following:

- Addition or deletion of a phase of an existing project without a major change to the scope
- Addition of a regionally significant project that does not require air quality conformity and is programmed with 100% non-federal funds
- Minor adjustments to project limits
- Changes made to an existing project's non-federal funding amounts
- An increase of less than 25%, or any decrease or removal, of the federally-funded portion of a project
- Movement of projects between fiscal years included in the RTIP
- Minor changes to funding sources

Administrative modifications are typically processed through letter or email between the Nevada Department of Transportation and RTC.

## **Appendix D: Acronyms**

**ADA**—Americans with Disabilities Act of 1990

**ADT**—Average daily traffic

**CMAC**—Citizens Multimodal Advisory Committee

**CEA**— Critical Emphasis Areas

**CFR**—Code of Federal Regulations

**CMAQ**—Congestion Mitigation and Air Quality Improvement Program

**CMP**—Congestion Management Process

**CNG**—Compressed natural gas

**CO**—Carbon monoxide

**EPA**—U.S. Environmental Protection Agency

**eSTIP**—electronic State Transportation Improvement Program

**ETR**—Employer Trip Reduction

**FAST Act**—Fixing America’s Surface Transportation Act

**FFY**—Federal Fiscal Year

**FHWA**—Federal Highway Administration

**FTA**—Federal Transit Administration

**HA87**—Hydrographic Area #87

**HPMS**—Highway Performance monitoring system

**HSIP**—Highway Safety Improvement Program

**IJA**—Infrastructure Investment and Jobs Act

**ISTEA**—Intermodal Surface Transportation Efficiency Act of 1991

**ITS**—Intelligent Transportation Systems

**LOS**—Level of service

**MAP-21**—Moving Ahead for Progress in the 21<sup>st</sup> Century Act

**MOVES**—Air quality model

**MPO**—Metropolitan Planning Organization

**MVEB**—Motor vehicle emission budget

**NAAQS**—National Ambient Air Quality Standards

**NDOT**—Nevada Department of Transportation

**NHPP**—National Highway Performance Program

**NHS**—National Highway System

**NO<sub>x</sub>**—Nitrogen oxides

**PM**—Performance management

**PM1**—FHWA’s first Transportation Performance Management Final Rule

**PM2**—FHWA’s second Transportation Performance Management Final Rule

**PM<sub>2.5</sub>**—Particulate matter of less than 2.5 microns

**PM3**—FHWA’s third Transportation Performance Management Final Rule

**PM<sub>10</sub>**—Particulate matter of less than 10 microns

**RRIF**—Regional Road Impact Fee

**RTAA**—Reno-Tahoe Airport Authority

**RTC**—Regional Transportation Commission of Washoe County

**RTIP**—Regional Transportation Improvement Program



**RTP**—Regional Transportation Plan

**SIP**—State Implementation Plan

**STIP**—State Transportation Improvement Program

**STBG**—Surface Transportation Block Grant Program

**TAC**—RTC Technical Advisory Committee

**TA Set-Aside** —Transportation Alternatives Set-Aside Program

**TART**—Tahoe Area Regional Transit

**TCM**—Transportation Control Measure

**TMA**—Transportation Management Area

**TMRPA**—Truckee Meadows Regional Planning Agency

**TTD**—Tahoe Transportation District

**VMT**—Vehicle Miles Traveled or Vehicle Miles of Travel

**VOC**—Volatile organic compounds

**WCHD—AQMD**—Washoe County Health District, Air Quality Management Division



**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item: 6.1.**

To: Regional Transportation Commission

From: Bill Thomas, AICP, Executive Director

**SUBJECT: Board Request - Truckee River Path (Schieve)**

**RECOMMENDED ACTION**

Discuss paving and lighting on the Truckee River Path as requested by Mayor Schieve, and provide input and direction to the Executive Director. (For Possible Action).

**BACKGROUND AND DISCUSSION**

At the July 2023 Board Meeting, Mayor Schieve requested an agenda item be added to a future meeting to discuss the paving and lighting on the Truckee River Path.

**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken.

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**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item: 6.2.**

To: Regional Transportation Commission

From: Bill Thomas, AICP, Executive Director

**SUBJECT: Board Request - Public Transit and Lake Tahoe**

**RECOMMENDED ACTION**

Discuss the history and future direction of public transit in relation to Lake Tahoe as requested by Mayor Schieve, and provide input and direction to the Executive Director.

**BACKGROUND AND DISCUSSION**

At the July 2023 Board Meeting, Mayor Schieve requested an agenda item be added to a future meeting on the history and future direction of public transit in relation to Lake Tahoe.

**FISCAL IMPACT**

There is no fiscal impact related to this action.

**PREVIOUS BOARD ACTION**

There has been no previous Board action taken

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**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item: 7.1.**

To: Regional Transportation Commission

From: Bill Thomas, Executive Director

**SUBJECT: Executive Director Report**

**RECOMMENDED ACTION**

Monthly verbal update/messages from RTC Executive Director Bill Thomas - no action will be taken.

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**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item:** 7.2.

To: Regional Transportation Commission

From: Paul Nelson, Government Affairs Officer

**SUBJECT: Federal Report Discussion**

**RECOMMENDED ACTION**

Monthly verbal update/messages from Paul Nelson, RTC Government Affairs Officer on federal matters related to the RTC - no action will be taken.

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**REGIONAL TRANSPORTATION COMMISSION**

*Metropolitan Planning • Public Transportation & Operations • Engineering & Construction*

Metropolitan Planning Organization of Washoe County, Nevada

**Meeting Date:** 8/18/2023

**Agenda Item: 7.3.**

To: Regional Transportation Commission

From: Tracy Larkin Thomason, NDOT Director

**SUBJECT: NDOT Report**

**RECOMMENDED ACTION**

Monthly verbal update of activities from NDOT - no action will be taken.

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